

LOT 1 - ERIE AIR PARK REPLAT D FINAL CONSTRUCTION PLANS

LOCATED IN THE NORTHWEST QUARTER OF SECTION 31, TOWNSHIP 1 NORTH RANGE 68 WEST OF THE 6TH P.M.
TOWN OF ERIE, COUNTY OF WELD, STATE OF COLORADO

AREA = 10.3405 ACRES
SITE PLAN - SP-001595-2023

For
Submittal

Lot 1
Erie Air Park
Replat D
3020-3100 Airport Drive
Erie, Colorado 80516

| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

Project No: 23126A
By: JAS/DPA
File: 23126A-1

Cover Sheet

Sheet

C-001

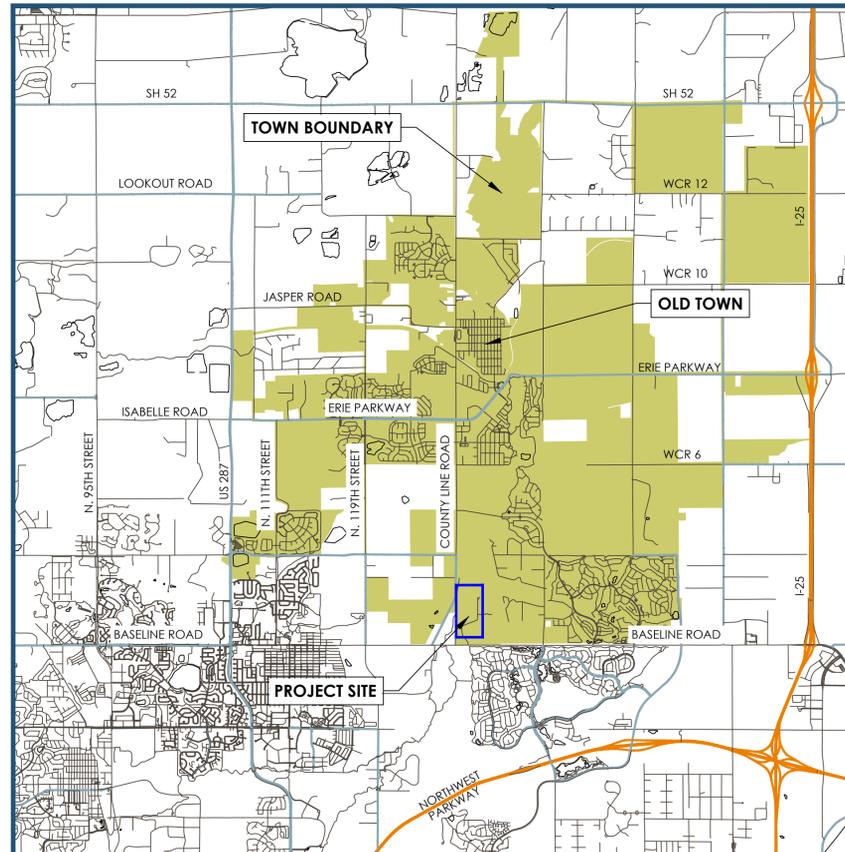
| GENERAL NOTES | |
|---------------|--|
| 1. | THESE PLANS ARE BASED ON THE ALTA/NSPS LAND TITLE SURVEY PREPARED BY SCOTT, COX & ASSOCIATES, INC., DATED 04/20/18, PROJECT NO. 17510. REFERENCE THE ERIE AIRPARK TRACT E MINOR SUBDIVISION PLAT, RECORDED 03/22/19 AT RECEPTION NO. 4475348. |
| 2. | THE BASIS OF BEARINGS IS THE NORTH LINE OF THE SW 1/4 OF SECTION 31, T1N, R68W OF THE 6TH P.M. BETWEEN THE FOUND MONUMENTS SHOWN HEREON AND BEARS S89°35'27"E, PER ERIE AIR PARK SUBDIVISION PLAT. |
| 3. | LAND TITLE GUARANTY COMPANY/OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY ORDER NO. FCC25152287-10 EFFECTIVE DATE 03/29/2019 AT 5:00 P.M. WAS RELIED UPON FOR INFORMATION REGARDING RIGHTS-OF-WAY, EASEMENTS AND ENCUMBRANCES IN PREPARATION OF THIS PLAT. |
| 4. | PORTIONS OF THE PROPERTY LIE WITHIN SPECIAL FLOOD HAZARD AREAS (SFHAS) SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD. THE 1% ANNUAL CHANCE FLOOD (100-YEAR FLOOD), ALSO KNOWN AS THE BASE FLOOD, IS THE FLOOD THAT HAS A 1% CHANCE OF BEING EQUALLED OR EXCEEDED IN ANY GIVEN YEAR. THE SPECIAL FLOOD HAZARD AREA IS THE AREA SUBJECT TO FLOODING BY THE 1% ANNUAL CHANCE FLOOD. ZONE AE =BASE FLOOD ELEVATIONS DETERMINED. THE BASE FLOOD IS THE WATER-SURFACE ELEVATION OF THE 1% CHANCE FLOOD. ZONE X (SHADED) =AREAS OF 0.2% ANNUAL CHANCE FLOOD. SFHAS ARE SHOWN PER FLOOD INSURANCE RATE MAP (FIRM) MAP NO. 08013C0443K DATED 08/15/19. |
| 5. | DUE TO THE PROXIMITY OF THE PROPERTY TO THE ERIE MUNICIPAL AIRPORT, THERE WILL BE AIRCRAFT PASSING OVER THE PROPERTY. AIRCRAFT PASSAGE MAY RESULT IN NOISE AND OTHER IMPACTS ON THE PROPERTY. AIRCRAFT MAY CROSS PROPERTY AT LOW ALTITUDE IN ACCORDANCE WITH FAA REGULATIONS. THE FREQUENCY OF AIRCRAFT PASSING OVER THE PROPERTY MAY INCREASE IN THE FUTURE. THE OWNERS, THEIR HEIRS, SUCCESSORS AND ASSIGNS SPECIFICALLY ACKNOWLEDGE THE RIGHT OF PASSAGE OVER THE PROPERTY FOR AIRCRAFT AND AGREE TO HOLD HARMLESS THE TOWN OF ERIE FOR AIRCRAFT OPERATIONS. |
| 6. | PORTIONS OF ERIE AIR PARK REPLAT D ARE UNDERMINED. TO REDUCE RISK OF SUBSIDENCE-RELATED DAMAGE, NO BUILDING OR BUILDING SEGMENT MAY EXCEED 140 FEET IN LENGTH OR WIDTH. THE MINE SUBSIDENCE HAZARD AND MITIGATION RECOMMENDATIONS ARE DESCRIBED IN MINE SUBSIDENCE ASSESSMENT, TRACT E-2, ERIE AIR PARK SUBDIVISION (WESTERN ENVIRONMENT AND ECOLOGY, INC., SEPTEMBER 24, 2019). THIS REPORT IS ON FILE WITH THE TOWN OF ERIE PLANNING AND DEVELOPMENT DEPARTMENT, FILES. MS-001121-2019 AND PUD-001122-2019. AT THE TIME OF BUILDING PERMIT SUBMITTAL, ENGINEERING PLANS WILL BE SUBMITTED TO THE BUILDING DEPARTMENT (FOR REVIEW AND APPROVAL) WHICH WILL SHOW DETAILS OF THE BUILDING SEPARATION AND ISOLATION SYSTEMS. THIS WILL ENSURE THAT THE MAXIMUM BUILDING LENGTH REQUIREMENTS WOULD BE MET WITH AN ENGINEERED SOLUTION FOR THE FOUNDATION AND SUPERSTRUCTURE DESIGN. |

| LEGAL DESCRIPTION |
|--|
| LOT 1, ERIE AIR PARK REPLAT D, A PLAT RECORDED ON 12/05/22 AT RECEPTION NO. 4870475, TOWN OF ERIE, COUNTY OF WELD, STATE OF COLORADO |

| SITE DATA SUMMARY CHART | | | |
|--|----------------|-----------------|--|
| ITEM | SQUARE FOOTAGE | % OF GROSS SITE | |
| GROSS SITE AREA | 450,431 | 100.00% | |
| • BUILDING FOOTPRINT | 139,319 | 30.93% | |
| • PARKING/ROADS (INCLUDING PLANTED INTERIOR PARKING ISLANDS) | 216,412 | 48.05% | |
| HARDSCAPE TOTAL | 355,731 | 78.98% | |
| • PLANTED AREA (EXCLUDING PLANTED INTERIOR PARKING ISLANDS) | 94,700 | 21.02% | |
| • EXISTING VEGETATION | - | 0.00% | |
| • TRAILS AND SIDEWALKS | - | 0.00% | |
| LANDSCAPE TOTAL | 94,700 | 21.02% | |

| PARKING TABLE | | | | | | | |
|---------------|---------------------|----------------|----------------|------------------------|------------------------|---------------------------|---------------------------|
| BLDG | USE | REQUIRED RATIO | BUILDING AREA | TOTAL PARKING REQUIRED | TOTAL PARKING PROVIDED | HANDICAP PARKING REQUIRED | HANDICAP PARKING PROVIDED |
| 100 | Light Industrial | 1: 1500 | 37,500 | 25 | 29 | 1 | 2 |
| 200 | Light Industrial | 1: 1500 | 37,500 | 25 | 28 | 1 | 1 |
| 300 | Light Industrial | 1: 1500 | 21,875 | 15 | 15 | 1 | 1 |
| 400 | Light Industrial | 1: 1500 | 37,500 | 25 | 28 | 1 | 2 |
| 500 | Light Industrial | 1: 1500 | 37,500 | 25 | 29 | 1 | 1 |
| | Shared Parking Area | | | | 56 | 1 | |
| | TOTAL | | 171,875 | 115 | 185 | 6 | 7 |

NOTE: THE SITE HAS BEEN PARKED AT THE LIGHT INDUSTRIAL RATIO OF 1:1,500. THIS ASSUMES THAT THE FLOOR AREA (FIRST AND SECOND FLOORS) OF EACH UNIT IS PERMITTED AS LIGHT INDUSTRIAL, INCLUDING OFFICE USES THAT ARE ANCILLARY TO THE LIGHT INDUSTRIAL USE. IF OTHER INDIVIDUAL USES ARE PROPOSED FOR ANY OF THE UNITS, THEN THOSE USES WOULD BE PARKED AT THE RATIOS OUTLINED IN THE UDC. THERE ARE CURRENTLY SEVENTY (70) EXTRA SPACES AT THE SITE. ADDITIONAL USES WOULD BE DEDUCTED FROM THE EXTRA SPACES ALLOCATED TO THE PROJECT.



1 LOCATION MAP
SCALE: 1" = 5000'

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| | |
|-------|--------------------------|
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| C-703 | CIVIL DETAILS |
| C-704 | CIVIL DETAILS |
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| C-712 | CIVIL DETAILS |
| C-713 | CIVIL DETAILS |
| C-714 | CIVIL DETAILS |
| C-715 | CIVIL DETAILS |
| C-716 | CIVIL DETAILS |

| LEGEND | |
|---------|---|
| SS | EX. SANITARY SEWER |
| W | EX. WATER |
| G | EX. UNDERGROUND GAS LINE |
| PO | EX. OVERHEAD ELECTRIC LINE |
| 25 | PROPOSED CONTOURS |
| 55.25 | EXISTING CONTOUR |
| 25.8 | POINT WHERE PROPOSED GRADE MEETS EXISTING GRADE |
| 25.8 | PROPOSED SPOT ELEVATION |
| 25.8 | EXISTING SPOT ELEVATION |
| → | HISTORIC SHEET FLOW |
| → | PROPOSED SHEET FLOW |
| --- | PROPOSED BASIN BOUNDARY |
| H 10.34 | HISTORIC SUB-BASIN DESIGNATION AREA IN ACRES |
| A 2.36 | PROPOSED SUB-BASIN DESIGNATION AREA IN ACRES |
| SS | PROPOSED SANITARY SEWER |
| W | PROPOSED WATER |
| ST | PROPOSED STORM SEWER |
| G | PROPOSED UNDERGROUND GAS LINE |
| PO | PROPOSED UNDERGROUND ELECTRIC LINE |
| W | PROPOSED WATER VALVE |
| ⊕ | PROPOSED FIRE HYDRANT |
| ⊙ | PROPOSED STORM MANHOLE |
| ⊙ | PROPOSED STORM INLET |
| T | PROPOSED TRANSFORMER |
| SF | SILT FENCE (SF) |
| ▨ | SEDIMENT CONTROL LOG (SCL) |
| + | STRAW BALE BARRIER (SB) |
| ⊙ | INLET PROTECTION (IP) |
| ▨ | STABILIZED STAGING AREA (SSA) |
| ▨ | PERMANENT SEEDING (PS) |
| ▨ | VEHICLE TRACKING CONTROL (VTC) |
| □ | CONCRETE WASHOUT AREA (CWA) |

TOWN OF ERIE ACCEPTANCE

ALL WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH CURRENT TOWN OF ERIE STANDARDS AND SPECIFICATIONS, AS AMENDED. THIS DRAWING HAS BEEN REVIEWED AND FOUND TO BE IN GENERAL COMPLIANCE WITH THESE STANDARDS AND SPECIFICATIONS AND OTHER TOWN REQUIREMENTS. THIS ACCEPTANCE SHALL NOT BE CONSTRUED TO RELIEVE ANY REQUIREMENT TO THE STANDARDS AND SPECIFICATIONS NOT SPECIFICALLY ADDRESSED IN THESE PLANS. IN ADDITION, THE ENGINEERING DESIGN AND CONCEPT REMAINS THE RESPONSIBILITY OF THE PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE APPEAR HEREON.

ACCEPTED BY:

TOWN ENGINEER _____ DATE _____

GENERAL NOTES - CONSTRUCTION

- ALL CONSTRUCTION SHALL CONFORM TO THE LATEST "STANDARDS AND SPECIFICATIONS FOR DESIGN AND CONSTRUCTION OF PUBLIC IMPROVEMENTS" BY THE TOWN OF ERIE. COPIES OF THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS MAY BE OBTAINED FROM THE TOWN OF ERIE WEB SITE. CONTRACTOR SHALL HAVE A SET ON SITE AT ALL TIMES.
- A PRE-CONSTRUCTION MEETING WITH THE TOWN OF ERIE ENGINEERING STAFF PRIOR TO THE START OF CONSTRUCTION, THOSE IN ATTENDANCE SHALL INCLUDE THE OWNER, HIS ENGINEER, THE TOWN OF ERIE ENGINEERING STAFF, REPRESENTATIVES OF THE CONTRACTORS AND OTHER AFFECTED AGENCIES, PLANS SIGNED AND ACCEPTED BY THE TOWN OF ERIE WILL BE DISTRIBUTED AT THE PRE- CONSTRUCTION MEETING. CONTRACTOR SHALL HAVE (1) COPY OF THE SIGNED PLANS ON SITE AT ALL TIMES.
- THE TOWN OF ERIE, THROUGH ACCEPTANCE OF THIS DOCUMENT, ASSUMES NO RESPONSIBILITY FOR THE COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT, THE OWNER AND DESIGN ENGINEER UNDERSTAND THAT THE RESPONSIBILITY FOR THE ENGINEERING ADEQUACY OF THE FACILITIES DEPICTED IN THIS DOCUMENT LIES SOLELY WITH THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT. REPORT ALL DISCREPANCIES TO THE DESIGN ENGINEER IMMEDIATELY.
- PRIOR TO BEGINNING THE WORK, THE CONTRACTOR SHALL OBTAIN ANY/ALL WRITTEN AGREEMENTS FOR INGRESS AND EGRESS TO THE WORK SITE FROM ADJACENT PRIVATE PROPERTY OWNERS. A COPY OF ALL AGREEMENTS SHALL BE PROVIDED TO THE TOWN. ACCESS TO ANY ADJACENT PRIVATE PROPERTY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF ERIE ENGINEERING STAFF. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO TOWN STANDARDS AND SPECIFICATIONS. INSPECTIONS AND ONSITE VISITS ARE NOT TO BE CONSTRUED AS A GUARANTEE BY THE TOWN ENGINEERING STAFF OF THE CONTRACTORS' CONTRACTUAL COMMITMENT. REQUESTS FOR INSPECTION BY THE TOWN OF ERIE SHALL BE MADE BY THE CONTRACTOR A MINIMUM OF TWENTY-FOUR (24) HOURS IN ADVANCE.
- CONSTRUCTION WATER IS AVAILABLE TO THE CONTRACTOR AS ESTABLISHED IN THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT THE TOWN OF ERIE REGARDING CURRENT REGULATIONS, FEES AND REQUIRED AGREEMENTS RELATED TO THE PROVISION OF CONSTRUCTION WATER.
- THE CONTRACTOR SHALL COORDINATE HIS ACTIVITIES WITH THE AFFECTED UTILITY COMPANIES AND SHALL NOTIFY THE UTILITY NOTIFICATION CENTER, PHONE NUMBER 811, THREE (3) BUSINESS DAYS PRIOR TO THE START OF CONSTRUCTION.
- UTILITIES IN THE AREA OF CONSTRUCTION ARE APPROXIMATE ONLY. THEY HAVE BEEN LOCATED FROM FIELD INVESTIGATION AND THE BEST AVAILABLE UTILITY RECORDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION, PROTECTION AND REPAIR OF ALL UTILITIES ENCOUNTERED DURING CONSTRUCTION WHETHER SHOWN ON THESE PLANS OR NOT. THE CONTRACTOR SHALL CONTACT ALL RESPECTIVE UTILITIES AND HAVE ALL UTILITIES FIELD-LOCATED PRIOR TO CONSTRUCTION. IF ANY UNKNOWN SUBSURFACE STRUCTURES ARE ENCOUNTERED DURING CONSTRUCTION, IT SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE TOWN OF ERIE ENGINEERING STAFF AND DESIGN ENGINEER PRIOR TO PROCEEDING.
- THE CONTRACTOR SHALL NOTIFY TOWN OF ERIE ENGINEERING STAFF OF ANY PROBLEM IMPACTING WATER AND WASTE WATER FACILITIES THAT WOULD POTENTIALLY REQUIRE A VARIANCE FROM THE APPROVED PLANS AND SPECIFICATIONS, ANY VARIANCE FROM THE APPROVED DOCUMENTS SHALL BE AT THE SOLE DISCRETION OF THE TOWN OF ERIE ENGINEERING STAFF.
- CONTRACTOR SHALL OBTAIN, AT HIS OWN EXPENSE, ALL APPLICABLE SPECIFICATIONS AND PERMITS NECESSARY TO PERFORM THE PROPOSED WORK.
- AS-BUILT DRAWINGS AS REQUIRED IN THE SPECIFICATIONS, ARE TO BE SUBMITTED BY THE OWNER/DEVELOPER PRIOR TO INITIAL ACCEPTANCE OF THE CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING AND REPLACING ANY EXISTING SIGNS, STRUCTURES, FENCES, ETC., ENCOUNTERED ON THE JOB AND RESTORING THEM TO THEIR ORIGINAL CONDITION.
- THE CONTRACTOR IS RESPONSIBLE FOR:
 - NOTIFYING THE TOWN OF ERIE UTILITY CUSTOMERS OF POTENTIAL SERVICE OUTAGES, AND COORDINATE WITH THE TOWN OF ERIE FOR DETERMINATION OF MINIMUM TIME REQUIREMENT.
 - NOTIFYING THE TOWN OF ERIE ENGINEERING STAFF IF WORK IS SUSPENDED FOR ANY PERIOD OF TIME AFTER INITIAL START-UP. THE CONTRACTOR SHALL NOTIFY THE TOWN OF ERIE FORTY-EIGHT (48) HOURS PRIOR TO RESTART.
 - IN THE EVENT OF AN AFTER HOURS EMERGENCY, CALL 303-441-4444.
 - NOTIFYING THE MOUNTAIN VIEW FIRE PROTECTION DISTRICT OF ALL STREET CLOSURES AND EXISTING FIRE HYDRANTS TAKEN OUT OF SERVICE A MINIMUM OF FORTY-EIGHT (48) HOURS PRIOR TO THE START OF CONSTRUCTION.
- PRIOR TO INSTALLATION OF UTILITY MAINS, ROAD CONSTRUCTION MUST HAVE COMPLETED THE OVER LOT GRADING STAGE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ANY GROUNDWATER ENCOUNTERED DURING THE CONSTRUCTION OF ANY PORTION OF THIS PROJECT. A CONSTRUCTION DEWATERING PERMIT MUST BE OBTAINED FROM THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE). GROUNDWATER SHALL BE PUMPED, PIPED, REMOVED AND DISPOSED OF IN A MANNER WHICH DOES NOT CAUSE FLOODING OF EXISTING STREETS OR EROSION OF ABUTTING PROPERTIES IN ORDER TO CONSTRUCT THE IMPROVEMENTS SHOWN ON THESE PLANS. THE USE OF ANY SANITARY SEWER TO DISPOSE OF TRENCH WATER WILL NOT BE PERMITTED. NO CONCRETE SHALL BE PLACED WHERE GROUNDWATER IS VISIBLE OR UNTIL THE GROUNDWATER TABLE HAS BEEN LOWERED BELOW THE PROPOSED IMPROVEMENTS, ANY UNSTABLE AREAS, AS A RESULT OF GROUNDWATER, ENCOUNTERED DURING THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS SHALL BE STABILIZED AS AGREED UPON BY THE CONTRACTOR, THE TOWN OF ERIE, AND THE DESIGN ENGINEER AT THE TIME OF THE OCCURRENCE
- IT SHALL BE THE RESPONSIBILITY OF THE DESIGN ENGINEER TO RESOLVE CONSTRUCTION PROBLEMS WITH THE TOWN OF ERIE DUE TO CHANGED CONDITIONS ENCOUNTERED BY THE CONTRACTOR DURING THE PROGRESS OF ANY PORTION OF THE PROPOSED WORK. IF, IN THE OPINION OF THE TOWN OF ERIE, PROPOSED ALTERATIONS TO THE SIGNED CONSTRUCTION PLANS INVOLVES SIGNIFICANT CHANGES TO THE CHARACTER OF THE WORK, OR TO THE FUTURE CONTIGUOUS PUBLIC OR PRIVATE IMPROVEMENTS, THE DESIGN ENGINEER SHALL BE RESPONSIBLE FOR SUBMITTING REVISED PLANS TO THE TOWN OF ERIE FOR REVIEW, PRIOR TO ANY FURTHER CONSTRUCTION RELATED TO THAT PORTION OF THE WORK.
- DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, THE CONTRACTOR SHALL BE SOLELY

AND COMPLETELY RESPONSIBLE FOR CONDITIONS AT AND ADJACENT TO THE JOB INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THE CONTRACTOR SHALL PROVIDE ALL LIGHTS, SIGNS, BARRICADES, FLAGMEN, OR OTHER DEVICES NECESSARY TO PROVIDE FOR PUBLIC SAFETY. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND IS NOT LIMITED TO NORMAL WORKING HOURS. THE TOWN OF ERIE OR THE DESIGN ENGINEER EXERCISE NO CONTROLS OVER THE SAFETY OR ADEQUACY OF ANY EQUIPMENT, BUILDING COMPONENTS, SCAFFOLDING, FORMS OR OTHER WORK AIDS USED IN OR ABOUT THE PROJECT, OR IN THE SUPERINTENDING OF THE SAME. THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD HARMLESS FROM ANY AND ALL LIABILITY, REAL AND ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER, THE DESIGN ENGINEER OR THE TOWN. THE TOWN OF ERIE ENGINEERING STAFF, OR ANY CONTRACTED ENGINEER, ARE NOT RESPONSIBLE FOR SAFETY IN, ON OR ABOUT THE PROJECT SITE, NOR FOR COMPLIANCE BY THE APPROPRIATE PARTY OF ANY REGULATIONS RELATING THERETO.

- WORK IN PUBLIC STREETS, ONCE BEGUN, SHALL BE PROSECUTED TO COMPLETION WITHOUT DELAY SO AS TO PROVIDE MINIMUM INCONVENIENCE TO ADJACENT PROPERTY OWNERS AND TO THE TRAVELING PUBLIC.
- REGULAR WORK HOURS ARE SEVEN (7) A.M. UNTIL SEVEN (7) P.M. OR DUSK (WHICHEVER OCCURS FIRST) OF THE SAME DAY, MONDAY THROUGH FRIDAY. THE CONTRACTOR WILL NOT PERMIT OVERTIME WORK OUTSIDE OF REGULAR WORKING HOURS OR THE PERFORMANCE OF WORK ON SATURDAY, SUNDAY OR ANY LEGAL HOLIDAY WITHOUT RECEIVING WRITTEN CONSENT FROM THE PUBLIC WORKS DIRECTOR. REQUESTS FOR WEEKEND WORK APPROVAL MUST BE SUBMITTED, IN WRITING TO THE TOWN OF ERIE NO LATER THAN WEDNESDAYS AT 3:30PM FOR SUBSEQUENT WEEKEND AND REQUESTS FOR HOLIDAY WORK APPROVAL MUST BE SUBMITTED, IN WRITING TO THE TOWN OF ERIE NO LATER THAN 7:00AM-2 BUSINESS DAYS PRIOR TO THE HOLIDAY. ALL EXPENSES INCURRED BY THE TOWN SHALL BE REIMBURSED AT A RATE TO BE DETERMINED BY DIRECTOR OF FINANCE.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY AND PROPER PRECAUTIONS TO PROTECT ADJACENT PROPERTIES FROM ANY AND ALL DAMAGE THAT MAY OCCUR FROM STORM WATER RUNOFF AND/OR DEPOSITION OF DEBRIS RESULTING FROM ANY AND ALL WORK. THE OWNER/CONTRACTOR IS RESPONSIBLE FOR OBTAINING A STORMWATER DISCHARGE PERMIT FOR CONSTRUCTION ACTIVITIES FOR ANY PROJECT DISTURBING OVER ONE ACRE FROM BOTH THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND THE TOWN OF ERIE.
- EACH TYPE OF CONSTRUCTION SHALL BE COMPLETED BY A CONTRACTOR THAT HAS DEMONSTRATED ACCEPTABLE QUALIFICATIONS TO THE TOWN AND IS A LICENSED CONTRACTOR IN THE TOWN OF ERIE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL DURING CONSTRUCTION. ALL TRAFFIC CONTROLS SHALL CONFORM TO THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (MUTCD) LATEST EDITIONS. A PLAN SHALL BE SUBMITTED TO THE TOWN FOR REVIEW AND ACCEPTANCE PRIOR TO CONSTRUCTION.
- ALL BACKFILL SHALL CONFORM TO THE TRENCH DETAIL LOCATED IN THE TOWN OF ERIE STANDARDS & SPECIFICATIONS.
- THE CONTRACTOR SHALL IMMEDIATELY REMOVE ANY CONSTRUCTION DEBRIS OR MUD TRACKED ONTO EXISTING ROADWAYS.
- THE CONTRACTOR SHALL REPAIR ANY EXCAVATION OR PAVEMENT FAILURES CAUSED BY HIS CONSTRUCTION.
- THE CONTRACTOR SHALL RENEW OR REPLACE ANY EXISTING TRAFFIC STRIPING AND/OR PAVEMENT MARKINGS, WHICH HAVE BEEN EITHER REMOVED OR THE EFFECTIVENESS OF WHICH HAS BEEN REDUCED DURING HIS OPERATION. RENEWAL OF PAVEMENT STRIPING AND MARKING SHALL BE DONE IN CONFORMANCE WITH THE TOWN OF ERIE STANDARD SPECIFICATIONS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO TAKE EVERY MEASURE NECESSARY TO COMPLY WITH ANY STATE, COUNTY OR TOWN DUST CONTROL ORDINANCE.
- CONSTRUCTION VEHICLES SHALL USE TRUCK ROUTES DESIGNATED BY THE TOWN.
- THE OWNER/DEVELOPER WILL BE HELD RESPONSIBLE FOR THE PROPER FUNCTIONING OF THE IMPROVEMENTS FOR A MINIMUM OF TWO (2) YEARS FROM THE DATE OF INITIAL ACCEPTANCE OF THE IMPROVEMENTS BY THE TOWN OF ERIE. ANY FAILURE DURING THIS PERIOD OF GUARANTEE SHALL BE REMEDIED BY THE OWNER/CONTRACTOR TO THE SATISFACTION OF THE TOWN OF ERIE AT NO EXPENSE TO THE TOWN.
- THE SOILS ENGINEER SHALL PERFORM SUFFICIENT INSPECTIONS DURING GRADING AND CONSTRUCTION SO THAT AN OPINION CAN BE RENDERED AND VERIFIED IN WRITING AS TO COMPLIANCE WITH THE PLANS AND CODES WITHIN THE SOILS ENGINEER'S PURVIEW.

GENERAL NOTES - GRADING

- ALL CONSTRUCTION ACTIVITIES THAT DISTURBS ONE OR MORE ACRES OF LAND, AS WELL AS ACTIVITIES THAT DISTURB LESS THAN ONE ACRE OF LAND, BUT IS PART OF A LARGER COMMON PLAN OF DEVELOPMENT, MUST COMPLY WITH BOTH LOCAL AND STATE REGULATIONS REGARDING STORMWATER DRAINAGE ON CONSTRUCTION SITES. OWNERS OR CONTRACTORS MUST OBTAIN A COLORADO STORMWATER DISCHARGE PERMIT FOR CONSTRUCTION ACTIVITIES FROM THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE) AND EITHER A PUBLIC IMPROVEMENT PERMIT OR A GRADING AND STORMWATER QUALITY PERMIT FROM THE TOWN OF ERIE. CONTRACTOR SHALL:
 - MAINTAIN A COPY OF THE STORM WATER MANAGEMENT PLAN (SWMP) ONSITE AT ALL TIMES. THE SWMP MUST BE MAINTAINED AND MADE AVAILABLE TO TOWN OF ERIE INSPECTORS UPON REQUEST.
 - INSTALL AND MAINTAIN EROSION, SEDIMENT, AND MATERIALS MANAGEMENT CONTROL BMPS AS SPECIFIED IN THE SWMP.
 - INSPECT ALL BEST MANAGEMENT PRACTICES (BMPs) AT LEAST EVERY FOURTEEN (14) DAYS AND WITHIN TWENTY FOUR (24) HOURS AFTER ANY PRECIPITATION OR SNOWMELT EVENT THAT CAUSES SURFACE RUNOFF.
 - MAINTAIN INSPECTION AND MAINTENANCE RECORDS OF BMPs ONSITE WITH THE SWMP. COPIES OF THESE REPORTS SHALL BE PROVIDED TO THE TOWN OF ERIE ENGINEERING STAFF.
 - BASED ON INSPECTIONS PERFORMED BY THE PERMIT HOLDER OR BY TOWN PERSONNEL, MODIFICATIONS TO THE SWMP WILL BE NECESSARY IF AT ANY TIME THE SPECIFIED BMPs DO NOT MEET THE OBJECTIVES OF THE PERMIT. ALL MODIFICATIONS SHALL BE COMPLETED AS SOON AS PRACTICABLE AFTER THE REFERENCED INSPECTION, AND SHALL BE RECORDED ON THE OWNER'S COPY OF THE SWMP.
 - THE OPERATOR SHALL AMEND THE SWMP WHENEVER THERE IS A SIGNIFICANT CHANGE IN DESIGN, CONSTRUCTION, OPERATION, OR MAINTENANCE, WHICH HAS A SIGNIFICANT EFFECT ON THE POTENTIAL FOR DISCHARGE OF POLLUTANTS TO THE RECEIVING WATERS, OR IF THE SWMP PROVES TO BE INEFFECTIVE IN ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
 - INSTALLATION AND MAINTENANCE OF BMPs SHALL BE SUPERVISED BY PERSONNEL CERTIFIED IN EROSION AND SEDIMENT CONTROL.
- ALL SITE GRADING (EXCAVATION, EMBANKMENT, AND COMPACTION) SHALL CONFORM TO THE RECOMMENDATIONS OF THE LATEST SOILS INVESTIGATION FOR THIS PROPERTY AND SHALL FURTHER BE IN CONFORMANCE WITH THE TOWN OF ERIE "STANDARDS AND SPECIFICATIONS FOR THE DESIGN AND CONSTRUCTION OF PUBLIC IMPROVEMENTS", LATEST EDITION.
- ALL GRADING AND FILLING OPERATIONS SHALL BE OBSERVED, INSPECTED AND TESTED BY A LICENSED SOILS ENGINEER. ALL TEST RESULTS SHALL BE SUBMITTED TO THE TOWN OF ERIE ENGINEERING STAFF.
- NATURAL VEGETATION SHALL BE RETAINED AND PROTECTED WHEREVER POSSIBLE. EXPOSURE OF SOIL TO EROSION BY REMOVAL OR DISTURBANCE OF VEGETATION SHALL BE LIMITED TO THE AREA REQUIRED FOR IMMEDIATE CONSTRUCTION OPERATION AND FOR THE SHORTEST PRACTICAL PERIOD OF TIME. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO AVOID ANY DAMAGE TO EXISTING FOLIAGE THAT LIES IN THE PROJECT AREA UNLESS DESIGNATED FOR REMOVAL AND SHALL BE LIABLE FOR SUCH DAMAGE AT HIS/HER EXPENSE.
- TOPSOIL SHALL BE STOCKPILED TO THE EXTENT PRACTICABLE ON THE SITE FOR USE ON AREAS TO BE REVEGETATED. ANY AND ALL STOCKPILES SHALL BE LOCATED AND PROTECTED FROM EROSION ELEMENTS.
- TEMPORARY VEGETATION SHALL BE INSTALLED ON ALL DISTURBED AREAS WHERE PERMANENT SURFACE IMPROVEMENTS ARE NOT SCHEDULED FOR IMMEDIATE INSTALLATION. SEEDING WILL BE DONE ACROSS THE SLOPE FOLLOWING THE CONTOURS. VEGETATION SHALL CONFORM TO THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS. PROJECT SCHEDULING SHOULD TAKE ADVANTAGE OF SPRING OR FALL PLANTING SEASONS FOR NATURAL GERMINATION. SEEDED AREAS SHALL BE IRRIGATED IN ACCORDANCE WITH THE TOWN OF ERIE'S STANDARDS AND SPECIFICATIONS.
- AT ALL TIMES, A WATER TRUCK SHALL BE ON-SITE AND THE PROPERTY SHALL BE MAINTAINED AND/OR WATERED TO PREVENT WIND-CAUSED EROSION. EARTHWORK OPERATIONS SHALL BE DISCONTINUED WHEN FUGITIVE DUST SIGNIFICANTLY IMPACTS ADJACENT PROPERTY. IF EARTHWORK IS COMPLETE OR DISCONTINUED AND DUST FROM THE SITE CONTINUES TO CREATE PROBLEMS, THE OWNER/DEVELOPER SHALL IMMEDIATELY INSTITUTE MITIGATIVE MEASURES AND SHALL CORRECT DAMAGE TO ADJACENT PROPERTY.
- FILL SLOPES SHALL BE COMPACTED BY MEANS OF SHEEPSFOOT COMPACTOR OR OTHER SUITABLE EQUIPMENT. COMPACTING SHALL CONTINUE UNTIL SLOPES ARE STABLE AND THERE IS NOT AN APPRECIABLE AMOUNT OF LOOSE SOIL ON THE SLOPES.
- TEMPORARY CUT/FILL SLOPES SHALL ABIDE BY THE SOILS REPORT. PERMANENT SLOPES SHALL BE AS SHOWN ON PLANS.
- DEPTH OF MOISTURE-DENSITY CONTROL SHALL BE FULL DEPTH ON ALL EMBANKMENT AND SIX (6) INCHES ON THE BASE OF CUTS AND FILLS.
- OUTLET SIDES OF ALL STORM PIPES SHALL BE GRADED TO DRAIN AND SHALL HAVE SUFFICIENT EROSION PROTECTION.
 - THE PERMITTEE OR HIS AGENT SHALL NOTIFY THE SITE GEOTECHNICAL ENGINEER WHEN THE GRADING OPERATION IS READY FOR EACH OF THE FOLLOWING INSPECTIONS:
 - INITIAL INSPECTION WHEN THE PERMITTEE IS READY TO BEGIN WORK, BUT NOT LESS THAN TWO (2) DAYS BEFORE ANY GRADING OR GRUBBING IS STARTED.
 - AFTER THE NATURAL GROUND OR BEDROCK IS EXPOSED AND PREPARED TO RECEIVE FILL, BUT BEFORE FILL IS PLACED.
 - EXCAVATION INSPECTION AFTER THE EXCAVATION IS STARTED BUT BEFORE THE VERTICAL DEPTH OF THE EXCAVATION EXCEEDS TEN (10) FEET.
 - FILL INSPECTION AFTER THE FILL PLACEMENT IS STARTED, BUT BEFORE THE FILL EXCEEDS TEN (10) FEET.

GENERAL NOTES - SEWER

- THE CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING SEWERS TO BE CONNECTED TO PRIOR TO CONSTRUCTION STAKING.
- CONNECTION TO EXISTING TOWN OF ERIE LINES WILL BE PERMITTED UPON INITIAL ACCEPTANCE OF THE NEW SANITARY SEWER SYSTEM. EXISTING PIPE AT THE POINT OF CONNECTION SHALL NOT BE "BROKEN OUT" UNTIL THE NEW SYSTEM IS ACCEPTED. IF CONNECTING TO AN EXISTING MANHOLE, THE NEW LINE SHALL BE PLUGGED UNTIL THE NEW SYSTEM IS ACCEPTED.
- MINIMUM VERTICAL SEPARATIONS BETWEEN ALL UTILITY PIPES SHALL BE EIGHTEEN (18) INCHES. IF VERTICAL SEPARATIONS ARE LESS THAN EIGHTEEN (18) INCHES, THE UTILITY PIPES SHALL BE REINFORCED AND PROTECTED AS REQUIRED BY CURRENT TOWN STANDARD SPECIFICATIONS.
- WATER AND SANITARY SEWER LINES SHALL HAVE A MINIMUM HORIZONTAL SEPARATION OF TEN (10) FEET. WHEN A TEN (10) FOOT SEPARATION IS NOT PROVIDED OR WHEN SEWER LINES CROSS WATER LINES WITH LESS THAN ONE AND ONE-HALF (1½) FEET OF VERTICAL SEPARATION, SEWER LINE JOINTS SHALL BE CONCRETE ENCASED. FOR PERPENDICULAR CROSSINGS, ENCASED JOINTS SHALL EXTEND TEN (10) FEET, PERPENDICULAR TO THE WATER LINE IN BOTH DIRECTIONS.
- ALL SANITARY SEWER SERVICES AND WATER SERVICES ARE TO BE TEN (10) FEET APART.
- SERVICE LATERALS SHALL EXTEND FIVE (5) FEET BEYOND RIGHTS OF WAY OR UTILITY EASEMENTS, WHICHEVER IS GREATER. THE ENDS SHALL BE MARKED BY A GREEN PAINTED WOOD POST UNTIL CURB AND GUTTER IS IN PLACE. WHEN CURB AND GUTTER IS IN PLACE THE LATERALS SHALL BE MARKED ON THE CONCRETE CURB FACE WITH AN "S" OR "X".
- THE LENGTH OF SANITARY SEWER LINE IS THE HORIZONTAL DISTANCE BETWEEN CENTER OF MANHOLE TO CENTER OF MANHOLE. THEREFORE, THE DISTANCES INDICATED ON THE PLANS ARE APPROXIMATE AND COULD VARY DUE TO VERTICAL ALIGNMENT AND MANHOLE DIMENSIONS.
- SERVICE LINE CONNECTIONS TO DEAD END MANHOLES THAT HAVE NO FURTHER POSSIBILITY OF EXTENSION SHALL BE ALLOWED AND SHALL HAVE A MINIMUM DROP OF 0.75 X MAIN DIAMETER. SERVICE LINE CONNECTINGS TO IN-LINE MANHOLES ARE NOT PERMITTED. MINIMUM SERVICE LINE SLOPE: 4 INCHES=2%; 6 INCHES= 1%; 8 INCHES=0.4%.
- ALL FOUR (4) THROUGH FIFTEEN (15) INCH SANITARY SEWER PIPE SHALL BE POLYVINYL CHLORIDE (PVC) AND SHALL BE IN ACCORDANCE WITH ASTM D-3034-SDR35, "STANDARD SPECIFICATION FOR PVC SEWER PIPE AND FITTINGS". ANY SANITARY SEWER HAVING A DEPTH IN EXCESS OF FIFTEEN (15) FEET SHALL BE COORDINATED WITH THE PUBLIC WORKS DEPARTMENT.
- BEDDING MATERIAL SHALL CONFORM TO TOWN OF ERIE STANDARDS AND SPECIFICATIONS.
- WARNING TAPE SHALL BE INSTALLED 12" MINIMUM AND 18" MAXIMUM ABOVE SEWER PIPE.
- PRECAST CONCRETE MANHOLE SECTIONS SHALL BE IN ACCORDANCE WITH ASTM C-478. MANHOLE STEPS SHALL BE POLYPROPYLENE COVERED STEEL CONFORMING TO ASTM. D-4101 AND ASTMA-615. CAST IRON RING AND COVER SHALL CONFORM TO ASTM A-48.
- MANHOLES SHALL BE A MINIMUM FOUR (4) FOOT DIAMETER AND CONSTRUCTED PER THE STANDARDS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL TAKE CARE TO PROPERLY SHAPE ALL MANHOLE INVERTS AND BENCHES IN ACCORDANCE WITH THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS, TO PROMOTE SMOOTH FLOW THROUGH THE MANHOLE. INVERTS OF LINES INTERSECTING AT 90 DEGREES AND AT HIGHLY DIVERGENT OR FLAT SLOPES ARE ESPECIALLY CRITICAL. MANHOLE INVERTS SHALL BE CONSTRUCTED WITH A SMOOTH TROWEL FINISH, AND BENCH FINISHED WITH A LIGHT BROOMED, NON-SKID, FINISH.
- SEWER TEES AND/OR WYES SHALL BE STAKED BY A SURVEY CREW. THE CONTRACTOR SHALL FURNISH TO THE ENGINEER "AS- CONSTRUCTED" LOCATION OF TEES AND WYES. ALL SERVICE LINES ARE FOUR (4) INCH UNLESS OTHERWISE NOTED.
- THE CONTRACTOR, AT THE OWNER'S EXPENSE, WILL MAKE ALL SEWER SERVICE TAPS.
- PRIOR TO BACKFILL THE TOWN OF ERIE ENGINEERING STAFF SHALL INSPECT ALL SANITARY SEWER MAINS AND SERVICE EXTENSIONS.
- MANHOLE RIMS SHALL BE SET AT AN ELEVATION RELATIVE TO THE PAVEMENT, IN ACCORDANCE WITH THE TOWN OF ERIE STANDARDS.

WHETHER THE MANHOLE IS AT PAVED OR UNPAVED GRADE, A MINIMUM OF ONE (1) AND A MAXIMUM OF FOUR (4) CONCRETE RINGS SHALL BE USED TO ADJUST THE RIM ELEVATION TO FINAL GRADE. THE MAXIMUM ACCEPTABLE VERTICAL ADJUSTMENT UTILIZING CONCRETE RINGS IS EIGHTEEN (18) INCHES.
- INITIAL ACCEPTANCE OF THE NEW SANITARY SEWER MAINS IS CONTINGENT UPON COMPLETION OF ITEMS LISTED IN THE TOWNS STANDARDS AND SPECIFICATIONS.

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For
Submittal

Lot 1 Erie Air Park Replat D

3020-3100 Airport Drive
Erie, Colorado 80516

| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

Project No: 23126A
By: JAS/DPA
File: 23126A-1

General Notes

Sheet

C-002

GENERAL NOTES - WATER

- AT ALL POINTS OF CONNECTION OF NEW WATER MAINS TO EXISTING MAINS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR EXCAVATING AND VERIFYING LOCATION OF THE EXISTING LINES PRIOR TO ANY CONSTRUCTION.
- EXCEPT IN CASE OF AN EMERGENCY, VALVES ON THE TOWN OF ERIE WATER SYSTEM SHALL BE OPERATED BY OR UNDER THE DIRECTION OF THE APPROPRIATE TOWN OF ERIE PERSONNEL. THE CONTRACTOR SHALL GIVE THE TOWN OF ERIE ENGINEERING STAFF 48 HOURS NOTICE TO ARRANGE FOR OPERATING VALVES. BOTH THE CONTRACTOR AND THE APPROPRIATE TOWN OF ERIE PERSONNEL SHALL BE PRESENT WHEN THE VALVES ARE OPERATED.
- WATER AND SANITARY SEWER LINES SHALL HAVE A MINIMUM HORIZONTAL SEPARATION OF TEN (10) FEET. WHEN A TEN (10) FOOT SEPARATION IS NOT PROVIDED OR WHEN SEWER LINES CROSS WATER LINES WITH LESS THAN ONE AND ONE-HALF (1½) FEET OF VERTICAL SEPARATION, SEWER LINE JOINTS SHALL BE CONCRETE ENCASED. FOR PERPENDICULAR CROSSINGS, ENCASED JOINTS SHALL EXTEND TEN (10) FEET, PERPENDICULAR TO THE WATER LINE IN BOTH DIRECTIONS.
- ALL WATER LINES SHALL HAVE A MINIMUM OF FOUR AND ONE-HALF (4½) FEET OF COVER AND BE LOCATED A MINIMUM OF TEN (10) FEET FROM THE SANITARY SEWER AND THREE (3) FEET FROM THE EDGE OF CONCRETE CURB AND GUTTER PAN.
- CHANGES IN DIRECTION OF WATERLINE PIPE SHALL REQUIRE BENDS IN ALL INSTANCES. AXIAL DEFLECTION AT THE JOINTS SHALL NOT BE ALLOWED.
- WHEN IT IS NECESSARY TO DEPRESS WATER LINES AT UTILITY CROSSINGS, A MINIMUM CLEARANCE OF ONE AND ONE-HALF (1-1/2) FEET SHALL BE MAINTAINED BETWEEN OUTSIDES OF PIPE.
- DISTANCES FOR WATER LINES ARE THE HORIZONTAL DISTANCE BETWEEN THE CENTERS OF THE FITTINGS. THEREFORE, DISTANCES SHOWN ON THE PLANS ARE APPROXIMATE AND COULD VARY DUE TO VERTICAL ALIGNMENT AND FITTING DIMENSIONS.
- ALL WATER LINE VALVES SHALL BE SET ADJACENT TO THE TEE. EXCEPT FOR POINTS THAT FALL IN THE FLOW LINE OF A CONCRETE CROSS PAN. IN WHICH CASE, THE VALVE SHALL BE LOCATED SO THAT SURFACE DRAINAGE DOES NOT INFILTRATE THE VALVE BOX. VALVE BOXES SHALL BE SET AT AN ELEVATION IN ACCORDANCE WITH TOWN PAVING REQUIREMENTS.
- ALL WATER MAINS SHALL BE POLYVINYL CHLORIDE (PVC) PRESSURE PIPE UNLESS SPECIFIED OTHERWISE. NOMINAL PVC PIPE SIZES 4-INCH THROUGH 12-INCH SHALL CONFORM TO ALL REQUIREMENTS OF AWWA STANDARD C-900, PRESSURE CLASS 150 (DR18). NOMINAL PVC PIPE SIZES 16-INCH THROUGH 24-INCH SHALL CONFORM TO ALL REQUIREMENTS OF AWWA STANDARD C-905, PRESSURE CLASS 165 (DR25). ALL PVC PIPES SHALL HAVE OUTSIDE DIAMETERS EQUIVALENT TO CAST IRON PIPE.
- FIRE HYDRANT ASSEMBLY INCLUDES THE FIRE HYDRANT, SIX (6) INCH VALVE, AND SIX (6) INCH PIPE. INSTALLATION SHALL BE IN ACCORDANCE WITH THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS.
- ALL FITTINGS SHALL BE MADE FROM DUCTILE IRON, FURNISHED WITH MECHANICAL JOINT ENDS OR INTEGRAL RESTRAINED JOINTS, AND SHALL HAVE A PRESSURE RATING OF 350 PSI.
- POLYETHYLENE WRAPPING SHALL BE INSTALLED AROUND ALL DUCTILE IRON PIPES, FITTINGS, VALVES, FIRE HYDRANT BARRELS AND ROD AND CLAMPS. THE POLYETHYLENE SHALL HAVE A MINIMUM THICKNESS OF EIGHT (8) MILS, IN ACCORDANCE WITH AWWA STANDARD C-105.
- ALL WATER LINE PIPE SHALL BE PROVIDED WITH A MINIMUM GAGE SIZE OF 12 SINGLE STRAND INSULATED COPPER WIRE. SPLICES IN TRACER WIRE SHALL BE CAPPED IN WATER PROOF GEL CAP TYPE CONNECTORS SUITED FOR DIRECT BURY APPLICATION (3M TYPE DBY-6 LOW VOLTAGE OR EQUAL). WIRE SHALL BE ATTACHED TO TOP OF WATER LINE WITH 2-INCH WIDE PVC TAPE @ 5-FT INTERVALS ALONG PIPE. TRACER WIRE SHALL EXTEND TO THE SURFACE AND BE COILED IN A LOCATE BOX AT THE BACKSIDE OF EITHER EACH FIRE HYDRANT OR VALVE. UNDER THE SUPERVISION OF TOWN OF ERIE ENGINEERING STAFF, TEST SHALL BE MADE BY THE CONTRACTOR @ THE COMPLETION OF CONSTRUCTION TO INSURE THAT THE TRACER WIRES CARRY A CONTINUOUS CURRENT BETWEEN ALL ACCESS POINTS.
- WARNING TAPE SHALL BE INSTALLED 12" MINIMUM AND 18" MAXIMUM ABOVE WATER PIPE.
- BEDDING MATERIAL SHALL CONFORM TO TOWN OF ERIE STANDARDS AND SPECIFICATIONS.
- VALVES SHALL OPEN COUNTER CLOCKWISE. VALVES 12-INCH AND SMALLER SHALL BE RESILIENT SEAT GATE VALVES. LARGER VALVES SHALL BE BUTTERFLY VALVES.
- VALVE BOXES SHALL BE RAISED TO ONE-FOURTH (1/4) INCH BELOW GRADE AFTER COMPLETION OF SURFACE PAVING OR FINAL GRADING. VALVE BOXES IN NON-PAVED AREAS SHALL HAVE A CONCRETE COLLAR AROUND THE VALVE LID IN ACCORDANCE WITH THE DETAIL.
- ALL SERVICE LINE TAPS SHALL HAVE DOUBLE STRAP BRASS TAPPING SADDLES. (ROMAC 202B OR APPROVED EQUAL).
- ALL RESIDENTIAL WATER TAPS SHALL BE THREE-QUARTER (3/4) INCH OR AS REQUIRED BY THE CURRENT BUILDING CODE.
- ALL WATER SERVICE LATERALS SHALL EXTEND FIVE (5) FEET BEYOND RIGHT OF WAY OR UTILITY EASEMENTS, WHICHEVER IS GREATER. THE ENDS SHALL BE MARKED BY A BLUE PAINTED WOOD POST UNTIL CURB AND GUTTER IS IN PLACE. WHEN CURB AND GUTTER IS IN PLACE THE LATERALS SHALL BE MARKED ON THE CONCRETE CURB FACE WITH A "V" or "W".
- CONCRETE THRUST BLOCKS AND/OR "MEGA-LUG" MECHANICAL RESTRAINTS ARE REQUIRED AT ALL MECHANICAL FITTINGS. THRUST BLOCKS MAY NOT BE REQUIRED IF PIPE RESTRAINT IS PROVIDED IN ACCORDANCE WITH RESTRAINED PIPE DETAIL.
- NO WORK SHALL BE BACKFILLED (INCLUDING BEDDING MATERIAL ABOVE THE SPRING LINE OF THE PIPE) UNTIL THE CONSTRUCTION HAS BEEN INSPECTED AND APPROVED FOR BACKFILLING BY THE TOWN OF ERIE ENGINEERING STAFF.
- ONLY ONE CONNECTION TO THE EXISTING WATER DISTRIBUTION SYSTEM SHALL BE MADE UNTIL ALL HYDROSTATIC TESTING, CHLORINATION AND FLUSHING HAS BEEN COMPLETED.
- DISINFECTION AND HYDROSTATIC TESTING SHALL BE DONE IN THE PRESENCE OF A TOWN OF ERIE ENGINEERING STAFF. CONTACT THE TOWN OF ERIE DEPARTMENT OF PUBLIC WORKS, FORTY-EIGHT (48) HOURS PRIOR TO DISINFECTING AND/OR TESTING.
- DISINFECTION AND FLUSHING SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF HEALTH AND THE PROCEDURE SET FORTH IN AWWA C651, "STANDARD FOR DISINFECTING WATER MAINS". THE CHLORINATION OF THE WATER LINE SHALL BE PERFORMED PRIOR TO THE HYDROSTATIC TESTING. ALL VALVES, FIRE HYDRANTS AND OTHER APPURTANCES SHALL BE OPERATED WHILE PIPELINE IS FILLED WITH THE CHLORINATING AGENT TO INSURE THAT HIGH CHLORINE CONTACT IS MADE WITH ALL INTERNAL SURFACES.

- ALL WATER LINES SHALL BE HYDROSTATIC TESTED. PRESSURE AND LEAKAGE TESTS SHALL BE CONDUCTED ACCORDING TO THE APPLICABLE SECTIONS OF AWWA C600/605 TO A MINIMUM PRESSURE OF ONE HUNDRED AND FIFTY (150) POUNDS PER SQUARE (PSI) INCH AT THE LOW POINT OF THE SECTION BEING TESTED FOR THE DURATION OF TWO (2) HOURS. THE MAXIMUM LENGTH OF LINE TO BE TESTED SHALL BE ONE THOUSAND (1,000) FEET. ALL JOINTS IN CONNECTIONS ARE TO BE WATERTIGHT WITHIN TOLERANCES ALLOWED BY THE SPECIFICATIONS IN AWWA C600/605. ANY LEAKAGE THAT IS DISCOVERED BY OBSERVATION OR TESTS SHALL BE LOCATED AND MADE WATERTIGHT BY THE CONTRACTOR. PRESSURE AND LEAKAGE TESTS SHALL NOT BE CONDUCTED UNTIL THE LINE HAS PASSED ALL REQUIRED DISINFECTION TESTS.
- INITIAL ACCEPTANCE OF THE NEW WATER LINES ARE CONTINGENT UPON RECEIVING COPIES OF:
 - WATER TRENCH COMPACTION TEST RESULTS
 - HYDRO STATIC TESTING OF 100% OF THE SYSTEM
 - HEALTH DEPARTMENT TESTS. (CHLORINE AND/OR CLEAR WATER AS REQUIRED)
- ALL METER PITS AND CURB STOPS SHALL BE PROTECTED AT THE TIME OF INSTALLATION WITH A MINIMUM OF THREE (3) T-POSTS AND ORANGE SAFETY FENCE. THE T-POST AND SAFETY FENCE SHALL REMAIN IN PLACE AND IN GOOD CONDITION UNTIL THE LANDSCAPING IS INSTALLED.
- ALL WATER VAULTS SHALL BE WATER TIGHT. CONTRACTOR SHALL SEAL VAULTS TO ENSURE SURFACE WATER DOES NOT INFILTRATE INTO THE VAULTS. VAULT LIDS SHALL BE PLACED TO ENSURE THAT SURFACE WATER DOES NOT FLOW INTO THE VAULTS.

GENERAL NOTES - STORM DRAIN

- EXCEPT WHERE NOTED, ALL STORM SEWER PIPE SHALL BE REINFORCED CONCRETE, CLASS III AND SHALL CONFORM TO REQUIREMENTS OF ASTM C76. ALL RCP SHALL HAVE RUBBER GASKETED JOINTS AND SHALL CONFORM TO REQUIREMENTS OF ASTM C443, AND SHALL PROVIDE WATERTIGHT PERFORMANCE CHARACTERISTICS.
- TONGUE AND GROOVE JOINTS SHALL NOT BE ALLOWED.
- THE MINIMUM COVERAGE FOR ALL STORM DRAINAGE PIPES SHALL BE

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| 1.5 FEET FOR CLASS III PIPE AND 1 FOOT FOR CLASS IV PIPE. |
|---|
- BEDDING MATERIAL SHALL CONFORM TO TOWN OF ERIE STANDARDS AND SPECIFICATIONS.
- ALL MANHOLES SHALL BE CONCRETE AND CONFORM TO CDOT STANDARD M-604-20.
- THE MINIMUM MANHOLE DIAMETER SHALL BE AS SPECIFIED BELOW:

| | |
|----------------------|---------------------|
| <u>PIPE DIAMETER</u> | <u>MANHOLE SIZE</u> |
| 15" TO 18" | 4' DIAMETER |
| 21" TO 42" | 5' DIAMETER |
| 48" TO 54" | 6' DIAMETER |
| 60" AND LARGER | BOX BASE MANHOLE |
- ALL STREET INLETS SHALL BE CURB OPENING TYPE R CONFORMING TO CDOT STANDARD M-604-12, EXCEPT WHERE OTHERWISE NOTED.
- ALL INLET ACCESS COVERS SHALL HAVE THE WORDS "NO DUMPING - DRAINS TO RIVERS" AND "STORM SEWER" CAST INTO THE COVER PER TOWN OF ERIE STANDARD DETAIL.
- ALL END SECTIONS SHALL CONFORM TO CDOT STANDARD M-603-10.
- WHERE RIPRAP OR GROUTED BOULDERS ARE CALLED FOR ON THE PLANS FOR EROSION CONTROL, IT SHALL CONFORM TO THE URBAN STORM DRAINAGE CRITERIA MANUAL SPECIFICATIONS (LATEST REVISION).

GENERAL NOTES - ROADWAY

- ALL STATIONING IS BASED ON CENTERLINE OF ROADWAYS UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL PREPARE THE SUBGRADE BY SCARIFYING THE UPPER ONE (1) FOOT OF THE SUBGRADE IN CUT AREAS OR AREAS WITH LITTLE OR NO FILL, UNLESS SPECIFIED IN THE SOILS REPORT. THE WORK SHALL CONFORM TO THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
- PAVEMENT SHALL NOT BE CONSTRUCTED UNTIL ALL UNDERGROUND UTILITIES HAVE BEEN INSTALLED, TESTED AND ACCEPTED BY THE TOWN OF ERIE ENGINEERING STAFF.
- IT SHALL BE THE RESPONSIBILITY OF THE OWNER/CONTRACTOR TO SUPERVISE AND CERTIFY THAT PROPER COMPACTION HAS BEEN OBTAINED BY SUBCONTRACTORS AND AGENCIES CONCERNING UTILITY LINE BACKFILL INCLUDING, BUT NOT LIMITED TO, SEWER, WATER, ELECTRICAL, GAS AND LANDSCAPE IRRIGATION LINES AND ACCEPTED BY THE TOWN OF ERIE ENGINEERING STAFF AND THE SOILS ENGINEER.
- STREET PAVING SHALL NOT START UNTIL:
 - A SOILS REPORT AND PAVEMENT DESIGN IS ACCEPTED BY THE TOWN OF ERIE ENGINEERING STAFF.
 - ALL STREETS ARE COMPACTED IN ACCORDANCE WITH THE SOILS REPORT AND THE TOWN OF ERIE SPECIFICATIONS.
 - ALL COMPACTION TEST REPORTS HAVE BEEN SUBMITTED TO THE TOWN ENGINEERING STAFF PRIOR TO PROOF ROLLS.
 - PROOF ROLLS ARE PERFORMED USING A LOADED SINGLE AXLE 2000 GALLON WATER TRUCK AND MONITORED BY THE TOWN OF ERIE ENGINEERING STAFF.
- THE OWNER/CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING ALL UTILITY MANHOLE COVERS AND ACCESS LIDS TO GRADE.
- ALL CONCRETE SHALL BE A MINIMUM OF CLASS B, IN CONFORMANCE WITH CDOT STANDARDS.
- ALL CONCRETE EDGES MUST BE ROUNDED TO A FOURTH (1/4) INCH RADIUS, EXCEPT WHERE SHOWN OTHERWISE ON DRAWINGS.
- ONE HALF (1/2) INCH EXPANSION JOINTS SHALL BE INSTALLED AT ALL CURB RETURNS, CURB CUTS AND EXISTING STRUCTURES. CONTROL JOINTS SHALL BE INSTALLED PER THE TOWNS STANDARDS AND SPECIFICATIONS.
- THE GRADATION OF THE MINERAL AGGREGATE WILL BE GRADING SX (1/2" NOMINAL) FOR ALL TOP LIFTS AND OVERLAYS.
- TACK COAT SHALL BE USED PRIOR TO OVERLAY. (CSS-1H), 50:50 DILUTION, 0.10 GAL/SY. ALL EDGES ABUTTING NEW PAVEMENT SHALL BE TACKED.
- WHEN IT IS REQUIRED TO MATCH EXISTING PAVEMENT, EXISTING PAVEMENT SHALL BE SAW CUT IN A MANNER TO AFFECT A SMOOTH, VERTICAL STRAIGHT CUT EDGE. T PATCH MILLING MUST BE DONE PER STANDARD DETAILS.
- ALL SAWCUT EDGES OF EXISTING PAVEMENT SHALL BE CLEAN AND COATED WITH TACK COAT PRIOR TO PLACING NEW PAVEMENT ADJACENT TO THE EXISTING PAVEMENT.
- ALL ASPHALT SHALL BE ONE FOURTH (1/4) INCH ABOVE CONCRETE EDGES, MANHOLE COVERS AND ACCESS LIDS.
- SIGNAGE AND STRIPING SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COLORADO DEPARTMENT OF TRANSPORTATION M&S STANDARDS, AND THE TOWN OF ERIE STANDARD DESIGN CRITERIA AND STANDARD CONSTRUCTION REQUIREMENTS.
- THE PURCHASE AND INSTALLATION OF STREET NAME SIGNS SHALL BE THE RESPONSIBILITY OF THE OWNER/CONTRACTOR. THE OWNER/CONTRACTOR SHALL SECURE THE APPROVAL OF THE TOWN OF ERIE ENGINEERING STAFF FOR TYPE AND LOCATION OF THE STREET NAME SIGNS PRIOR TO INSTALLATION.
- ALL NEW ROADWAY SECTIONS SHALL HAVE SUBGRADE PREPARATION AND INITIAL ASPHALT PAVEMENT PLACED WITH A 1% CROWN. FINAL OVERLAY IS TO BE PLACED WITH A 2% CROWN. SEE DETAIL S77 IN THE "STANDARD DETAILS-STREET" FOR MORE INFORMATION.
- DETERMINATION OF CROWN FOR CUL DE SAC PAVING SHALL BE EVALUATED ON A CASE BY CASE BASIS.

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For
Submittal

Lot 1 Erie Air Park Replat D

3020-3100 Airport Drive
Erie, Colorado 80516

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|---|----------|-------------------|
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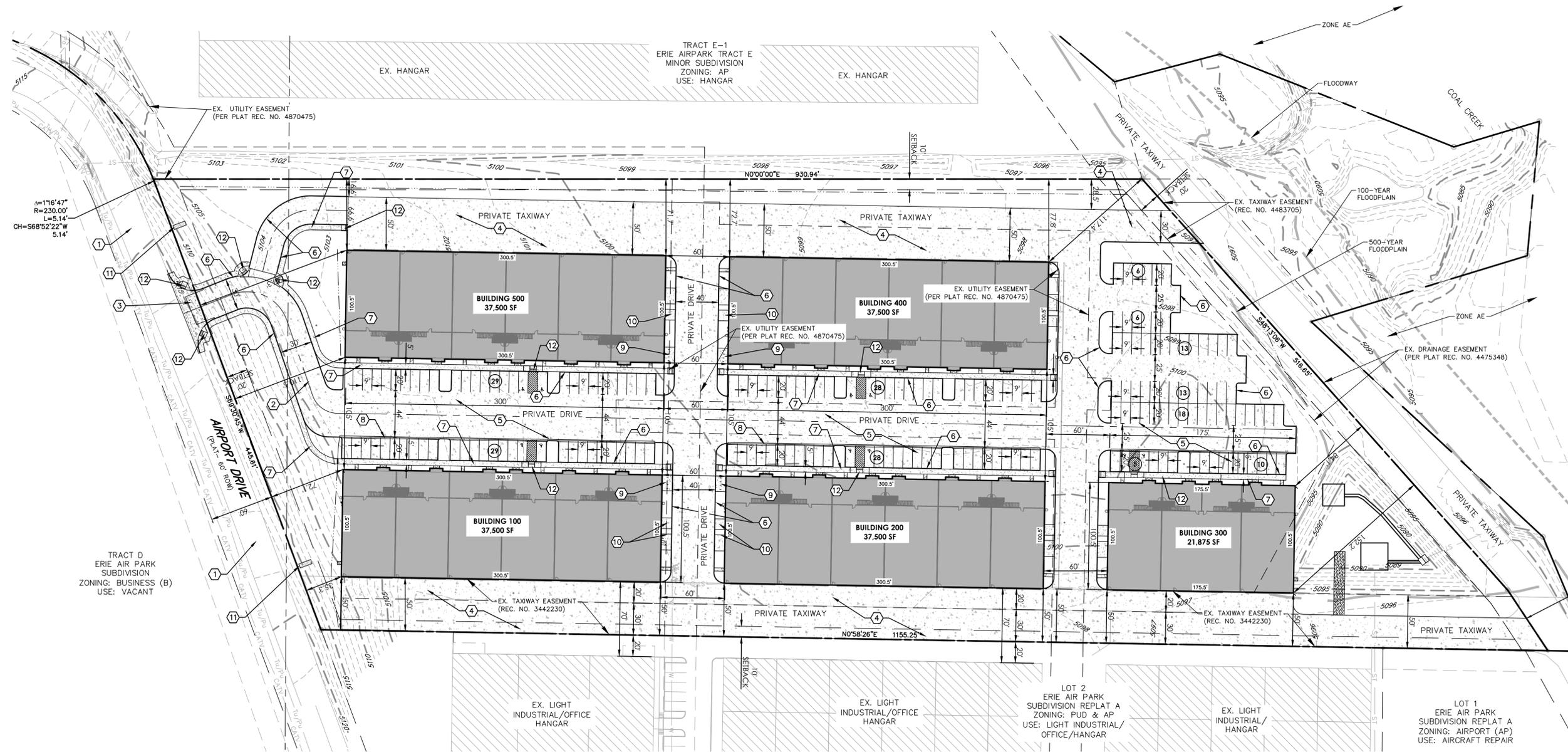
Project No: 23126A
By: JAS/DPA
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General Notes

Sheet

C-003

For
Submittal



TRACT D
ERIE AIR PARK
SUBDIVISION
ZONING: BUSINESS (B)
USE: VACANT

TRACT E-1
ERIE AIRPARK TRACT E
MINOR SUBDIVISION
ZONING: AP
USE: HANGAR

LOT 2
ERIE AIR PARK
SUBDIVISION REPLAT A
ZONING: PUD & AP
USE: LIGHT INDUSTRIAL/
OFFICE/HANGAR

LOT 1
ERIE AIR PARK
SUBDIVISION REPLAT A
ZONING: AIRPORT (AP)
USE: AIRCRAFT REPAIR

- NOTES:
- EV PARKING SPACES WILL BE PROVIDED IN ACCORDANCE WITH IBC AND IECC 2021.
- MINIMUM NUMBER OF SPACES WITH EVSE INSTALLED (2% OF 185 PROVIDED) = 4 SPACES REQUIRED/4 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV-READY SPACES (8% OF 185 PROVIDED) = 15 SPACES REQUIRED/16 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV-CAPABLE SPACES (10% OF 185 PROVIDED) = 19 SPACES REQUIRED/20 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV CAPABLE LIGHT SPACE (10% OF 185 PROVIDED) = 19 SPACES/20 SPACES PROVIDED.



| KEYED NOTES | |
|-------------|--|
| 1. | EXISTING ASPHALT ROADWAY TO REMAIN. |
| 2. | PROPOSED CONCRETE DRIVEWAY. |
| 3. | PROPOSED STANDARD DRIVE ENTRANCE. |
| 4. | PROPOSED CONCRETE TAXIWAY. |
| 5. | PROPOSED CONCRETE PARKING AREA. |
| 6. | PROPOSED CURB AND GUTTER. |
| 7. | PROPOSED CONCRETE SIDEWALK. |
| 8. | PROPOSED 2" CONCRETE DRAIN PAN. |
| 9. | PROPOSED BICYCLE RACKS ON CONCRETE SLAB. |
| 10. | PROPOSED TRASH ENCLOSURE. |
| 11. | PROPOSED MONUMENT SIGN. |
| 12. | ADA CURB RAMP. |

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Replat D**
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Erie, Colorado 80516

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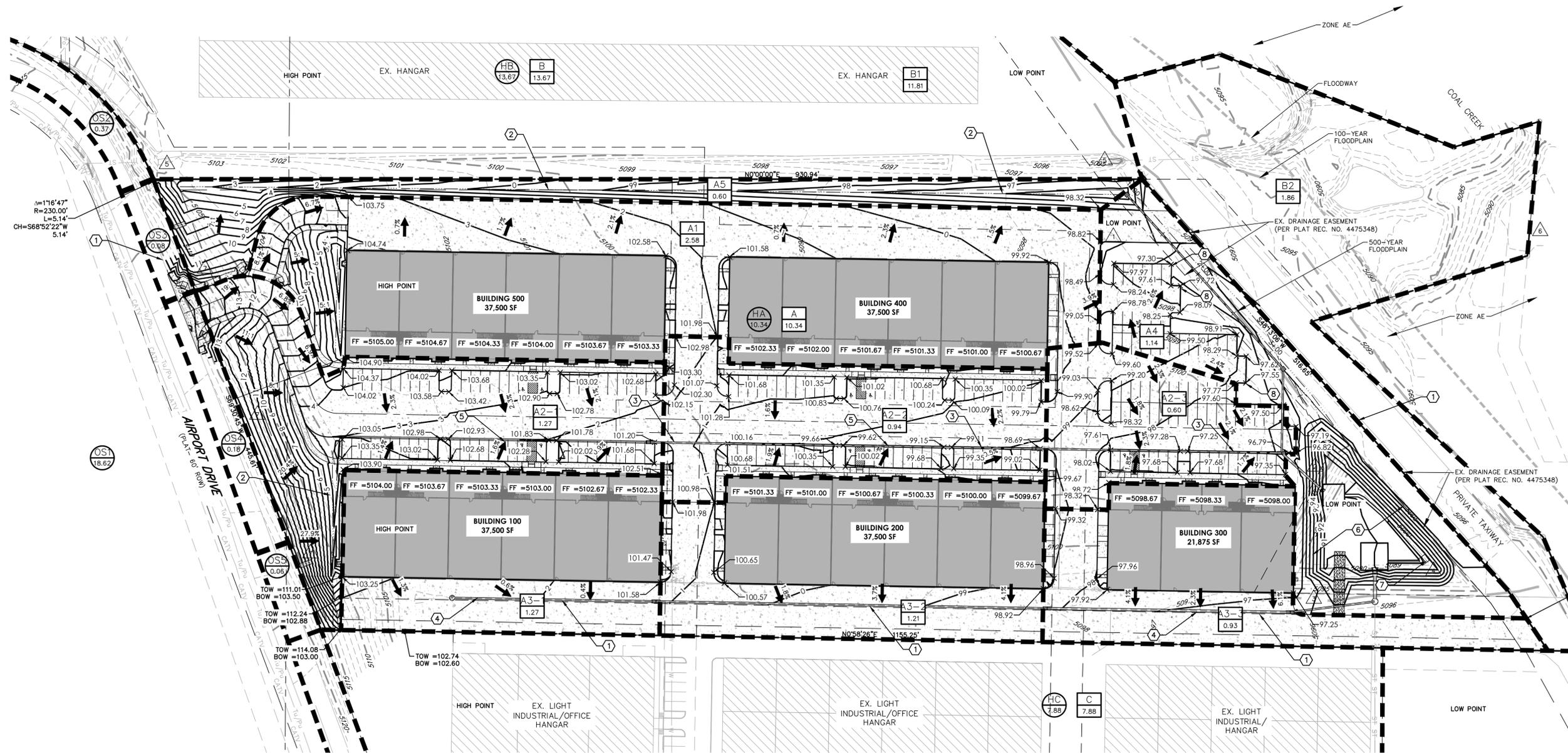
Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Site Plan

Sheet

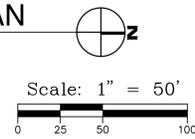
C-102

For
Submittal



GRADING AND DRAINAGE PLAN

1 SCALE: 1" = 50'



KEYED NOTES

1. SAW CUT AND MATCH GRADE AT EXISTING ASPHALT.
2. PROPOSED DRAINAGE SWALE.
3. PROPOSED STORM SEWER.
4. PROPOSED STORM SEWER FOR ROOF DRAINS.
5. PROPOSED 2' CONCRETE DRAIN PAN.
6. PROPOSED STORMWATER QUALITY/RETENTION FACILITY.
7. PROPOSED LIMITED RELEASE OUTLET STRUCTURE.
8. PROPOSED 1' CURB CUT.

**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
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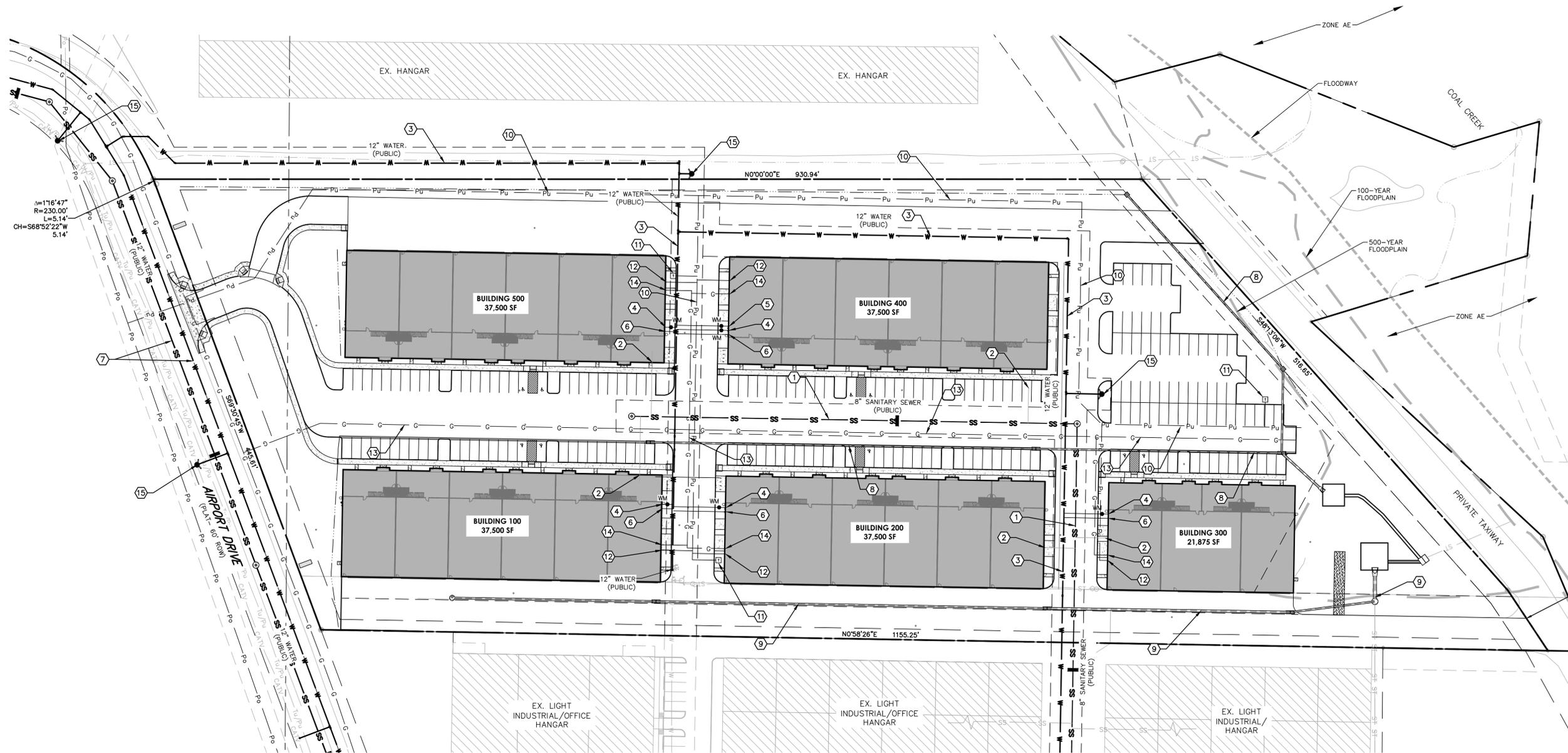
Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Grading and
Drainage Plan**

Sheet

C-103

For
Submittal



1 UTILITY PLAN
SCALE: 1" = 50'

Scale: 1" = 50'

0 25 50 100

| KEYED NOTES | |
|-------------|---|
| 1. | PROPOSED 12" SANITARY SEWER MAIN. SEE CIVIL ENGINEERING CONSTRUCTION PLAN SET. |
| 2. | PROPOSED 4" SANITARY SEWER SERVICE AT 2% SLOPE. |
| 3. | PROPOSED 12" WATER MAIN. SEE CIVIL ENGINEERING CONSTRUCTION PLAN SET. |
| 4. | PROPOSED DOMESTIC WATER SERVICE, METER AND METER PIT. |
| 5. | PROPOSED IRRIGATION SERVICE, METER AND METER PIT. |
| 6. | PROPOSED FIRE SERVICE, PROVIDE 6' X 10' FIRE ENTRY ROOM WITH EXTERIOR DOOR. |
| 7. | SEWER AND WATER MAIN EXTENSIONS DOWN AIRPORT DRIVE TO TRACT E-1 AND E-3. SEE CIVIL ENGINEERING CONSTRUCTION PLAN SET. |

| | |
|-----|--|
| 8. | PROPOSED STORM SEWER SYSTEM. |
| 9. | PROPOSED STORM SEWER FOR ROOF DRAINS. |
| 10. | PROPOSED ELECTRICAL SERVICE WITH LANDSCAPE SCREENING PER CHAPTER 6 OF THE UDC. SEE LANDSCAPE PLANS. |
| 11. | PROPOSED ELECTRICAL TRANSFORMER. |
| 12. | PROPOSED ELECTRIC METERS AND DISCONNECT WITH WITH LANDSCAPE SCREENING PER CHAPTER 6 OF THE UDC. SEE LANDSCAPE PLANS. |
| 13. | PROPOSED GAS SERVICE. |
| 14. | PROPOSED GAS METERS WITH WITH LANDSCAPE SCREENING PER CHAPTER 6 OF THE UDC. SEE LANDSCAPE PLANS. |
| 15. | PROPOSED FIRE HYDRANT LATERAL. |

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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Utility Plan

Sheet

For
Submittal



**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

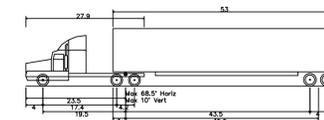
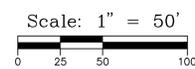
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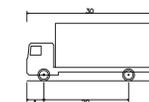
Turning Analysis

1 TURNING ANALYSIS

SCALE: 1" = 50'

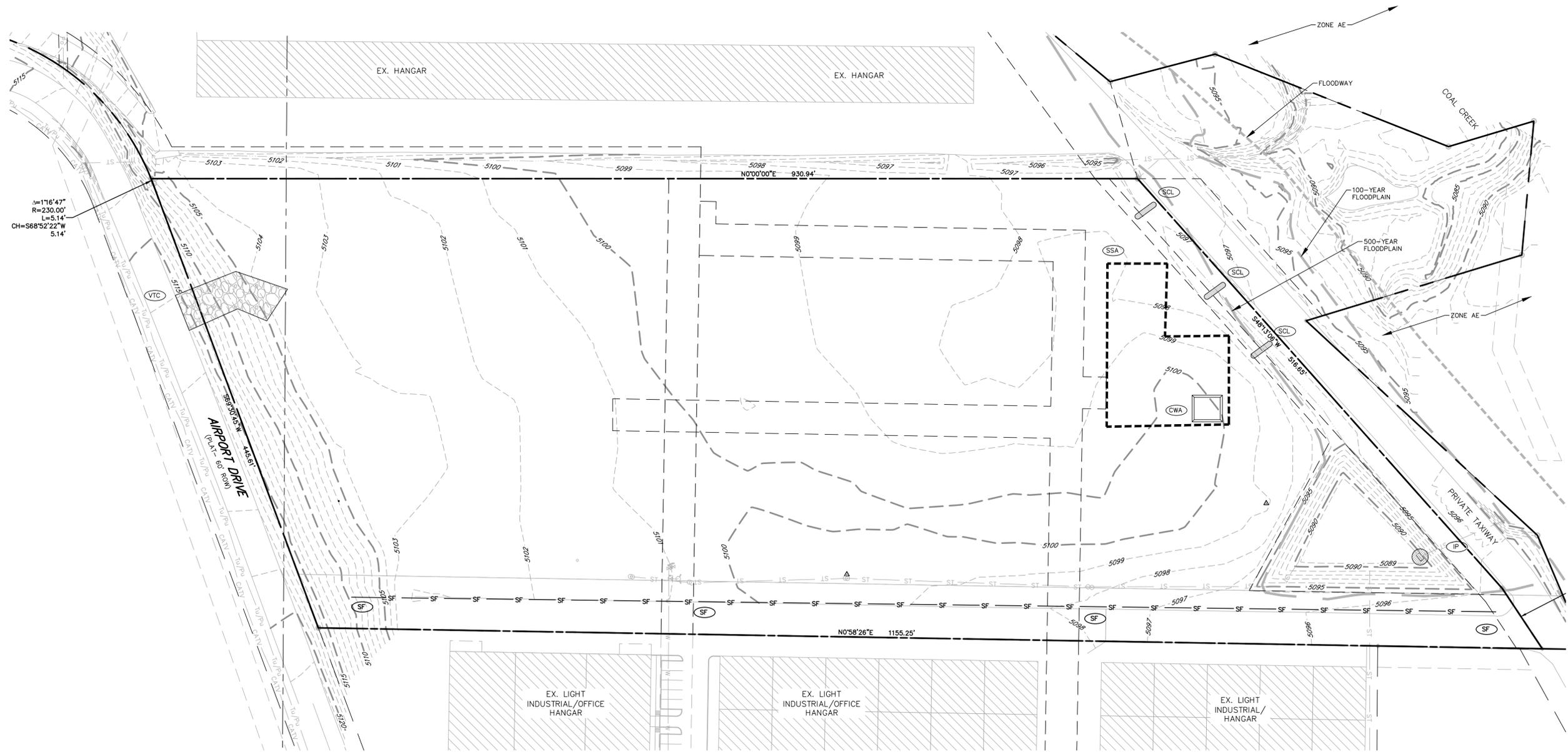


WB-67 - Interstate Semi-Trailer
Overall Length 73.50ft
Overall Width 8.50ft
Overall Body Height 13.50ft
Min. Body Ground Clearance 1.34ft
Max Track Width 8.50ft
Lock-to-lock time 6.00s
Max Steering Angle (Virtual) 28.40°



SU-30 - Single Unit Truck
Overall Length 30.00ft
Overall Width 8.00ft
Overall Body Height 13.50ft
Min. Body Ground Clearance 1.34ft
Track Width 8.00ft
Lock-to-lock time 3.00s
Max Steering Angle (Virtual) 31.60°

For
Submittal



$\Delta = 116'47"$
 $R = 230.00'$
 $L = 5.14'$
 $CH = S68^{\circ}52'22"W$
 $5.14'$



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Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Initial Stormwater
Management Plan**

Sheet

C-201

For
Submittal



$\Delta = 116'47"$
 $R = 230.00'$
 $L = 5.14'$
 $CH = S68^{\circ}52'22"W$
 $5.14'$

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**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

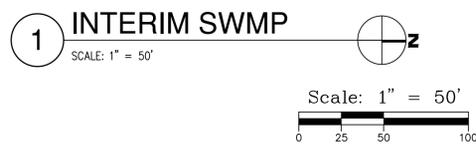
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Interim
Stormwater
Management Plan**

Sheet

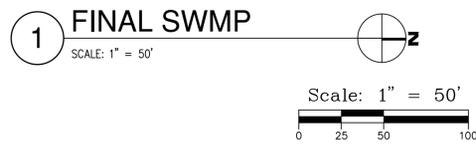
C-202



For
Submittal



$\Delta = 116'47"$
 $R = 230.00'$
 $L = 5.14'$
 $CH = S68^{\circ}52'22"W$
 $5.14'$



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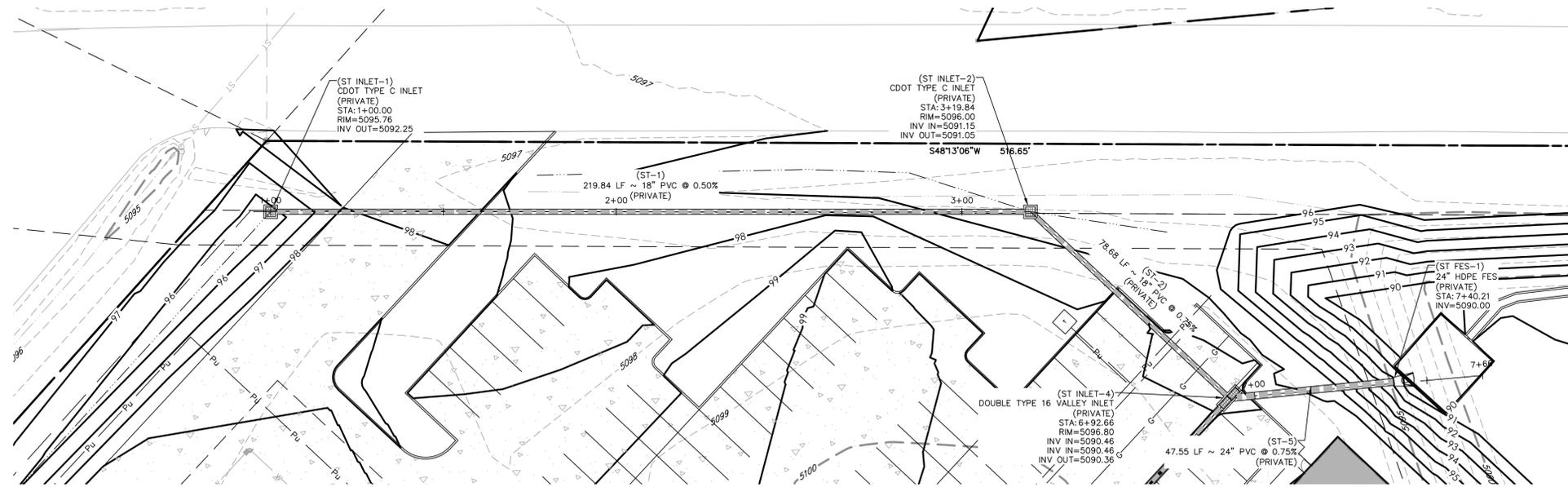
Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Final Stormwater
Management Plan**

Sheet

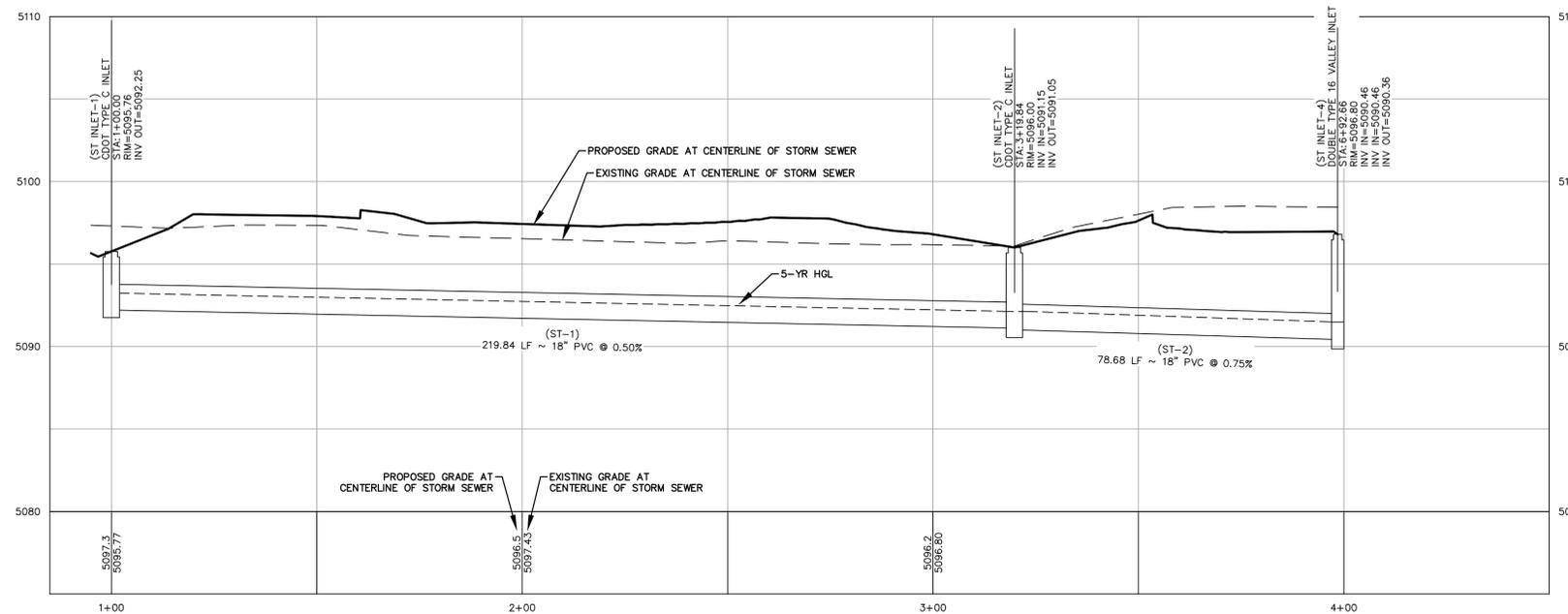
C-203

For
Submittal



1 STORM SEWER PLAN
SCALE: 1" = 20'

Scale: 1" = 20'
0 10 20 40



1 STORM SEWER PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

**Lot 1
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Replat D**

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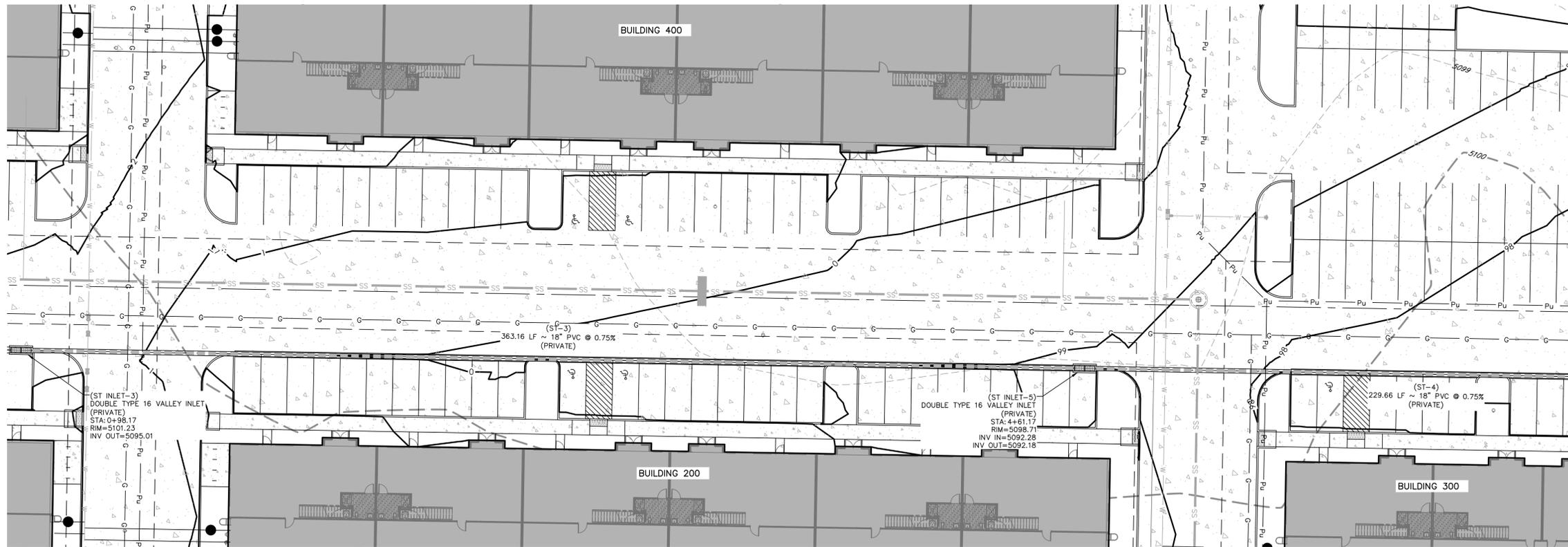
Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Storm Sewer Plan
and Profile**

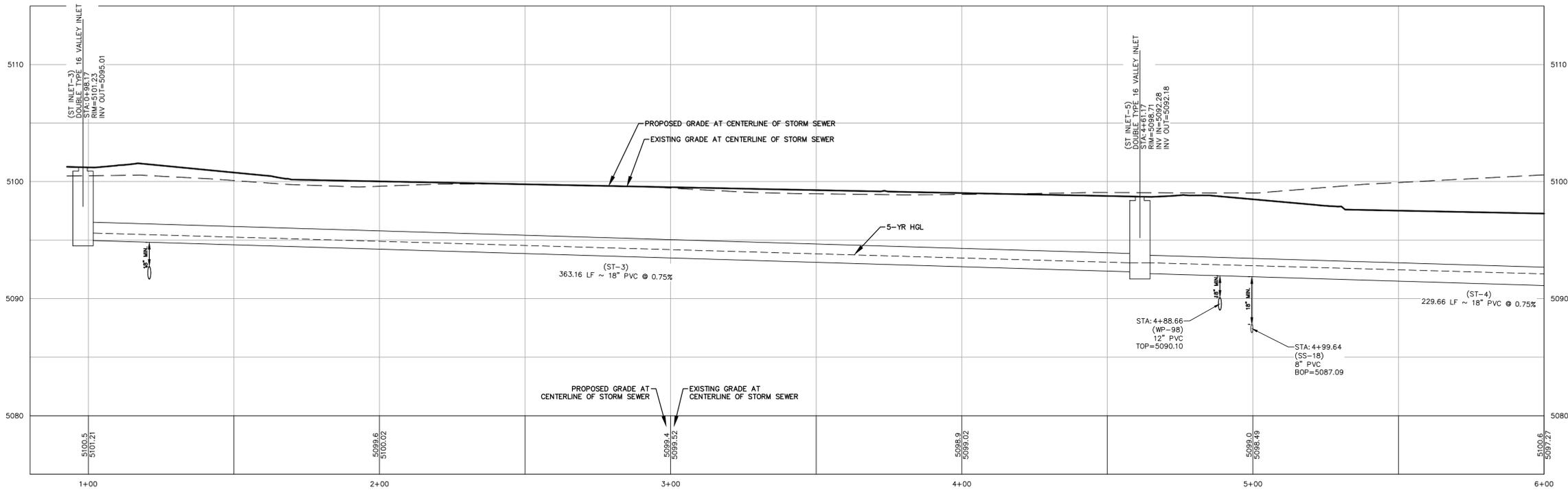
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C-301

For
Submittal



1 STORM SEWER PLAN
SCALE: 1" = 20'



1 STORM SEWER PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

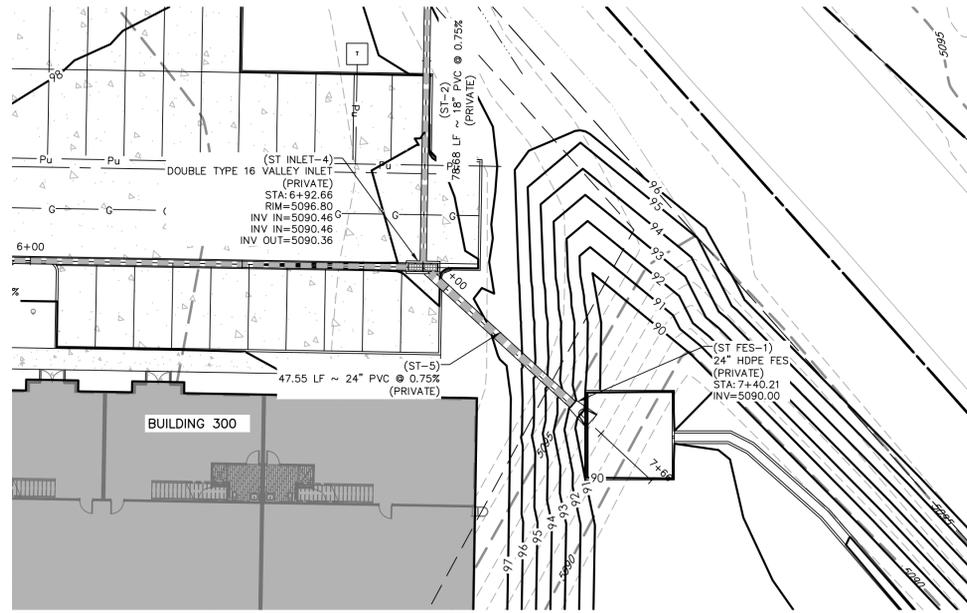
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**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
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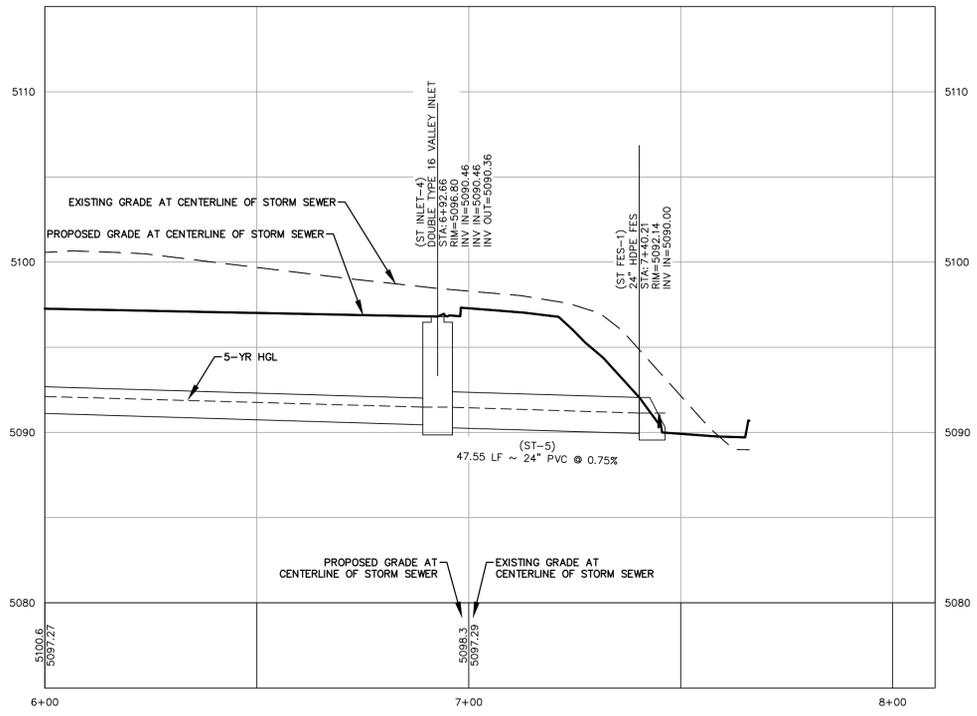
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Storm Sewer Plan
and Profile**



1 STORM SEWER PLAN
 SCALE: 1" = 20'
 Scale: 1" = 20'
 0 10 20 40



1 STORM SEWER PROFILE
 SCALE: HORIZ: 1" = 20'
 VERT: 1" = 5'

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For
 Submittal

**Lot 1
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 3020-3100 Airport Drive
 Erie, Colorado 80516

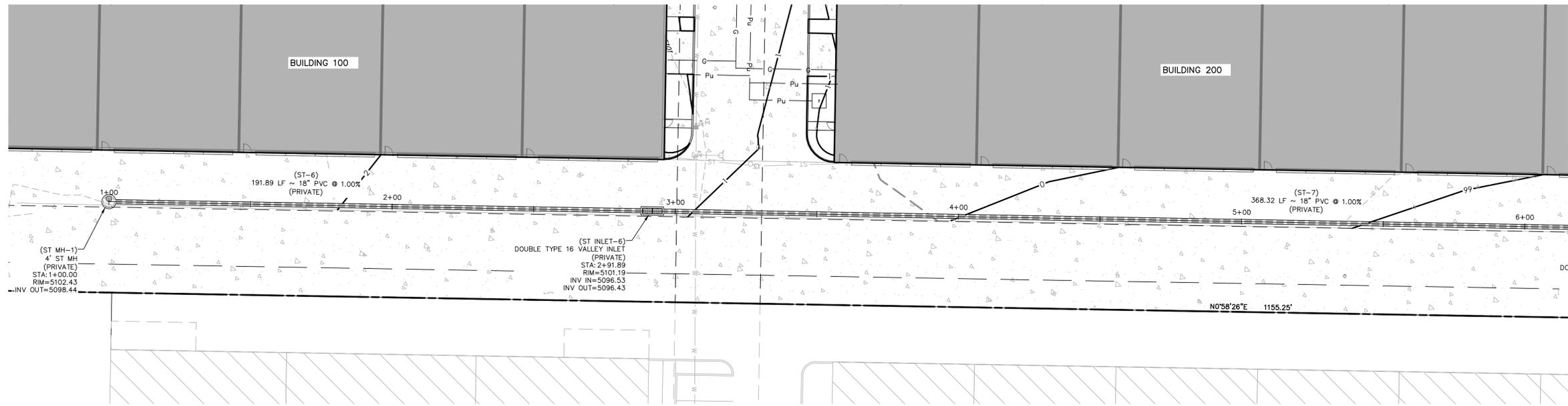
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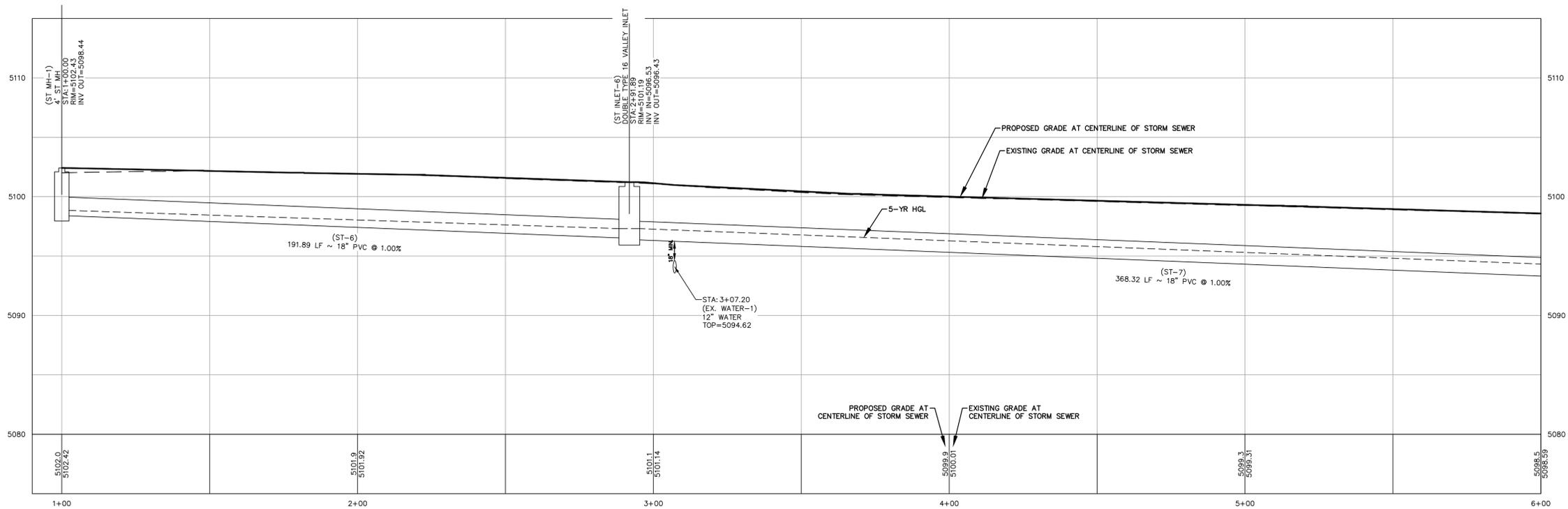
**Storm Sewer Plan
 and Profile**

Sheet

For
Submittal



1 STORM SEWER PLAN
SCALE: 1" = 20'



1 STORM SEWER PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

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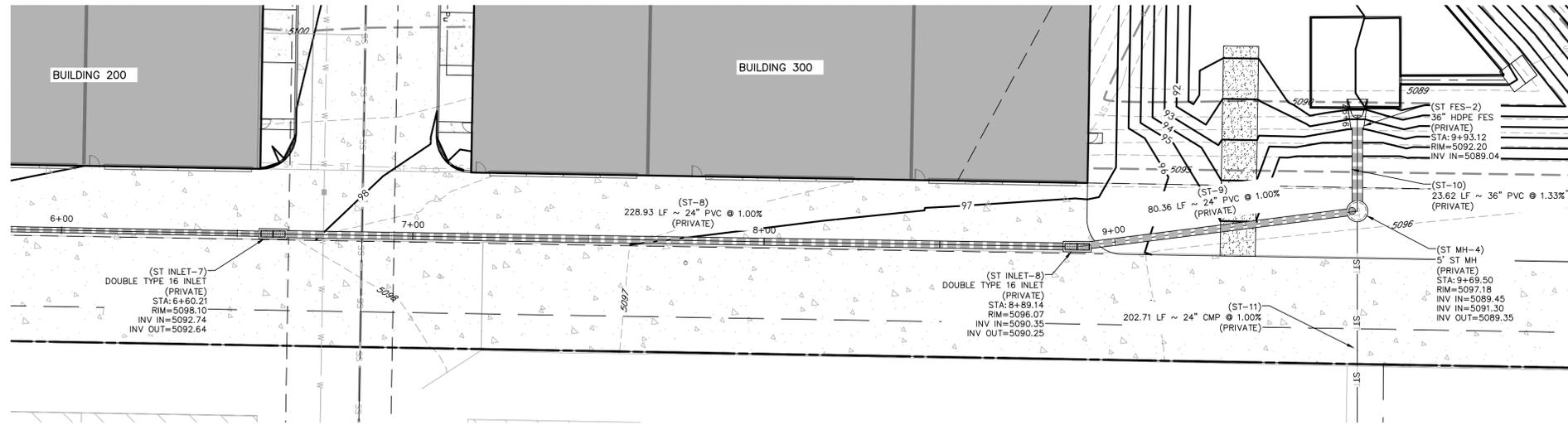
Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Storm Sewer Plan
and Profile**

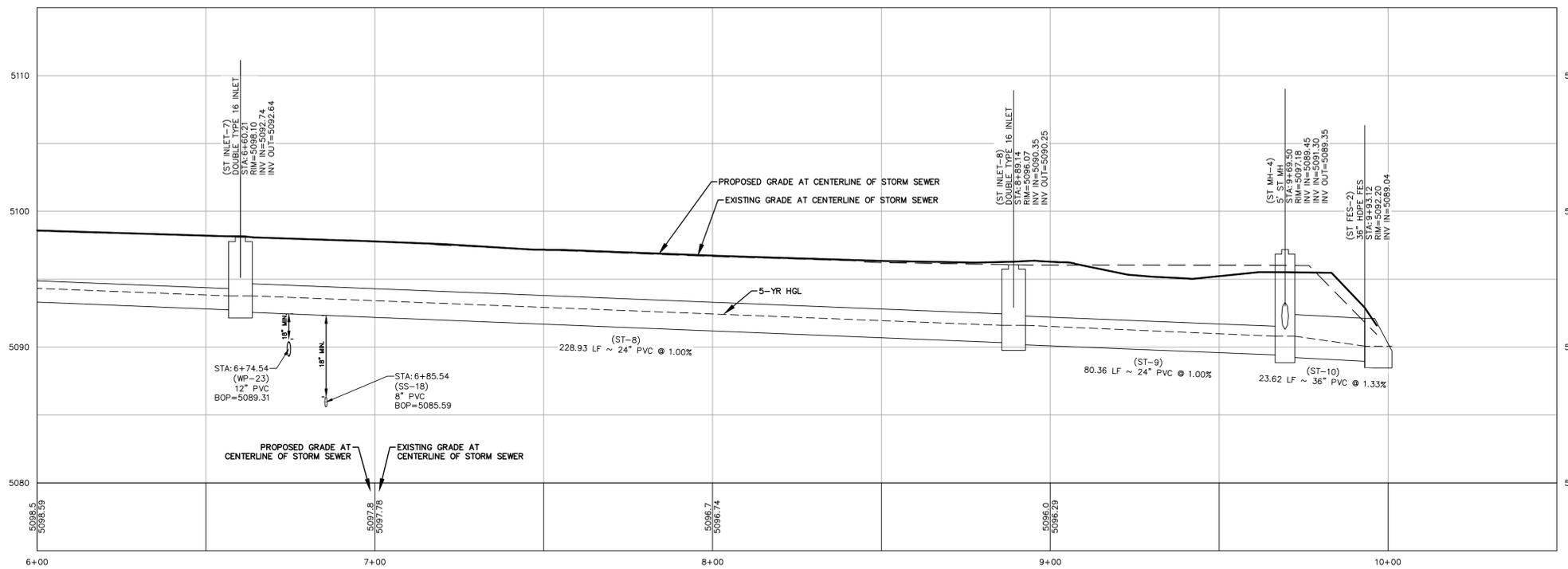
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C-304

For
Submittal



1 STORM SEWER PLAN
SCALE: 1" = 20'



1 STORM SEWER PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

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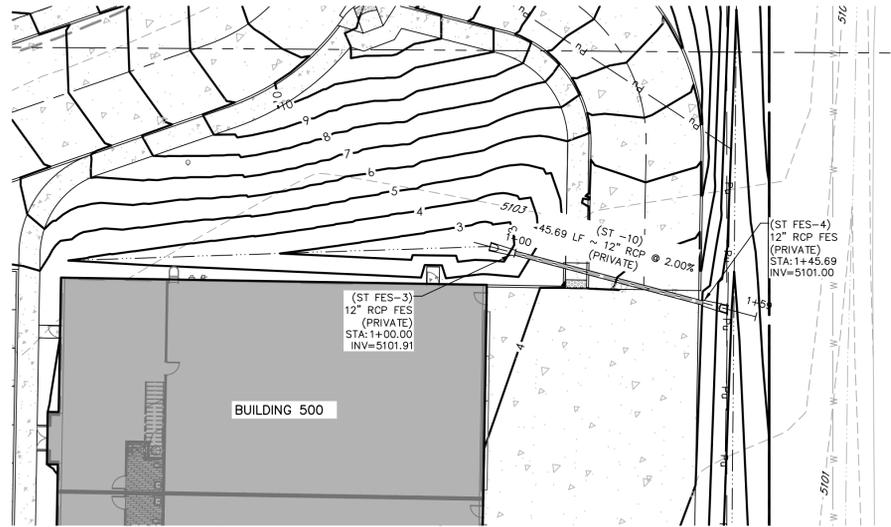
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File: 23126A-1

**Storm Sewer Plan
and Profile**

Sheet

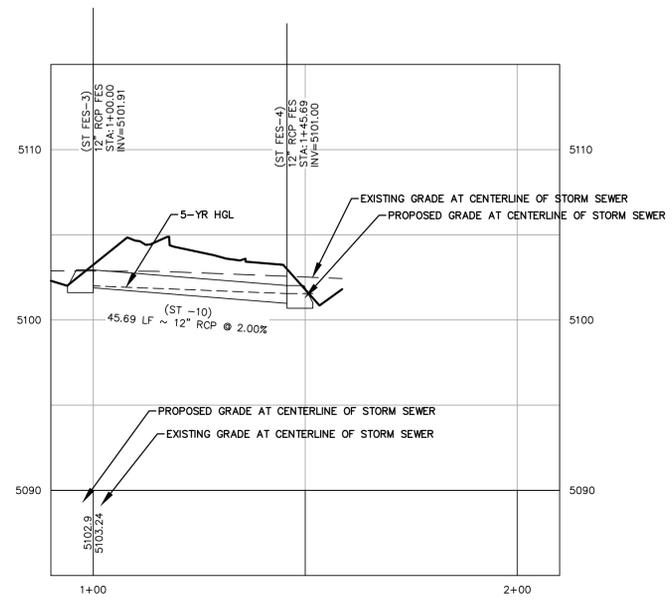
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1 STORM SEWER PLAN
SCALE: 1" = 20'

Scale: 1" = 20'
0 10 20 40



1 STORM SEWER PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

For
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**Storm Sewer Plan
and Profile**

Sheet

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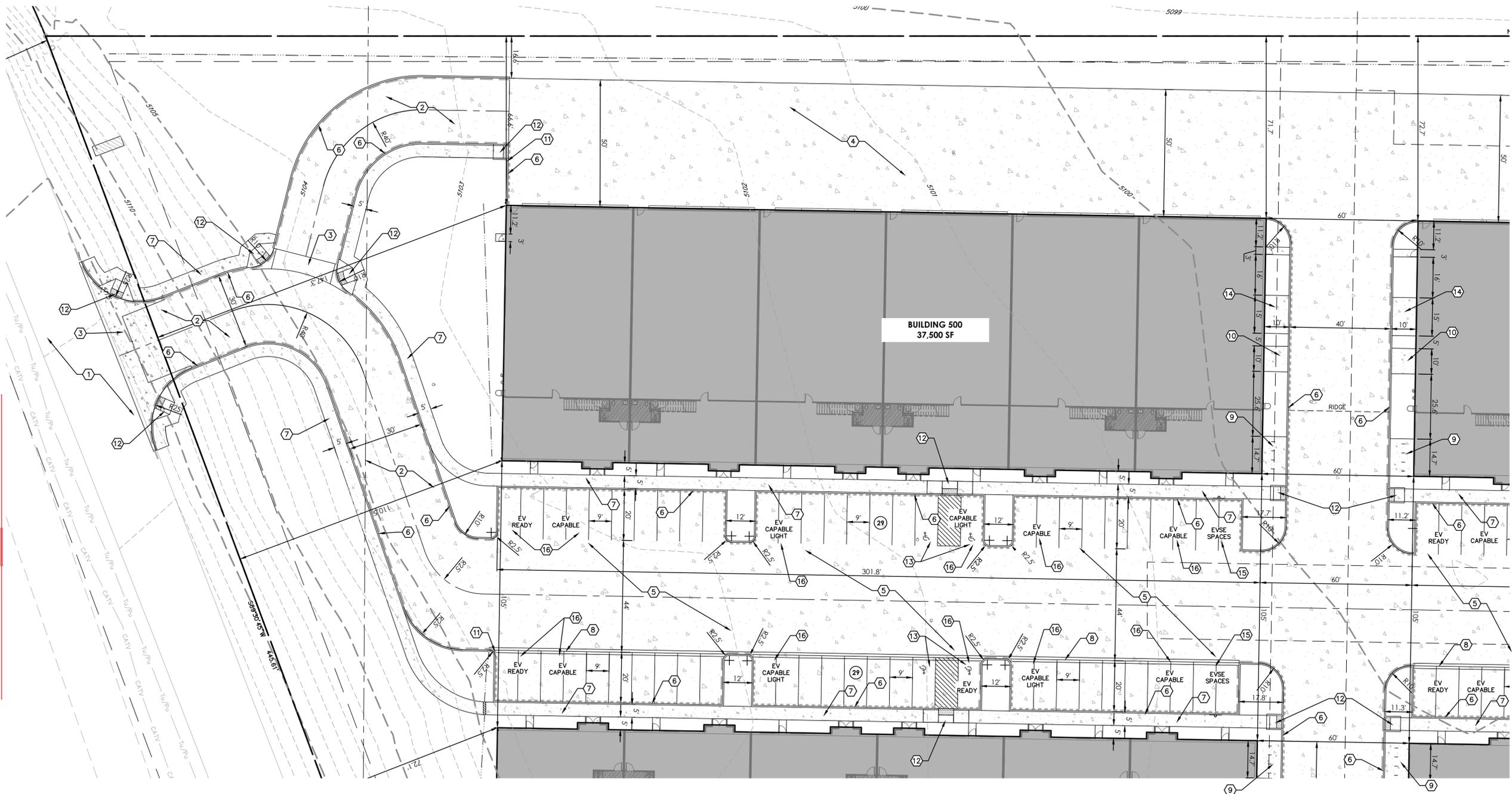
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Detailed Site Plan

Sheet

C-401



| LEGEND | |
|--------|---------------------|
| | PROPOSED CATCH CURB |
| | PROPOSED SPILL CURB |

NOTES:

EV PARKING SPACES WILL BE PROVIDED IN ACCORDANCE WITH IBC AND IECC 2021.

- MINIMUM NUMBER OF SPACES WITH EVSE INSTALLED (2% OF 185 PROVIDED) = 4 SPACES REQUIRED/4 SPACES PROVIDED.
- MINIMUM NUMBER OF EV-READY SPACES (8% OF 185 PROVIDED) = 15 SPACES REQUIRED/16 SPACES PROVIDED.
- MINIMUM NUMBER OF EV-CAPABLE SPACES (10% OF 185 PROVIDED) = 19 SPACES REQUIRED/20 SPACES PROVIDED.
- MINIMUM NUMBER OF EV CAPABLE LIGHT SPACE (10% OF 185 PROVIDED) = 19 SPACES/20 SPACES PROVIDED.

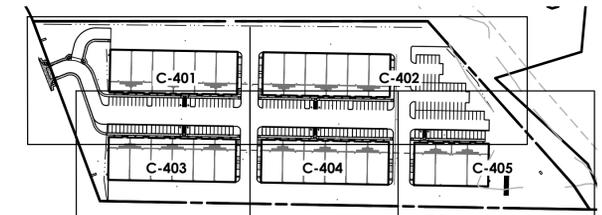
1
DETAILED
SITE PLAN

SCALE: 1" = 20'

Scale: 1" = 20'
0 10 20 40

KEYED NOTES

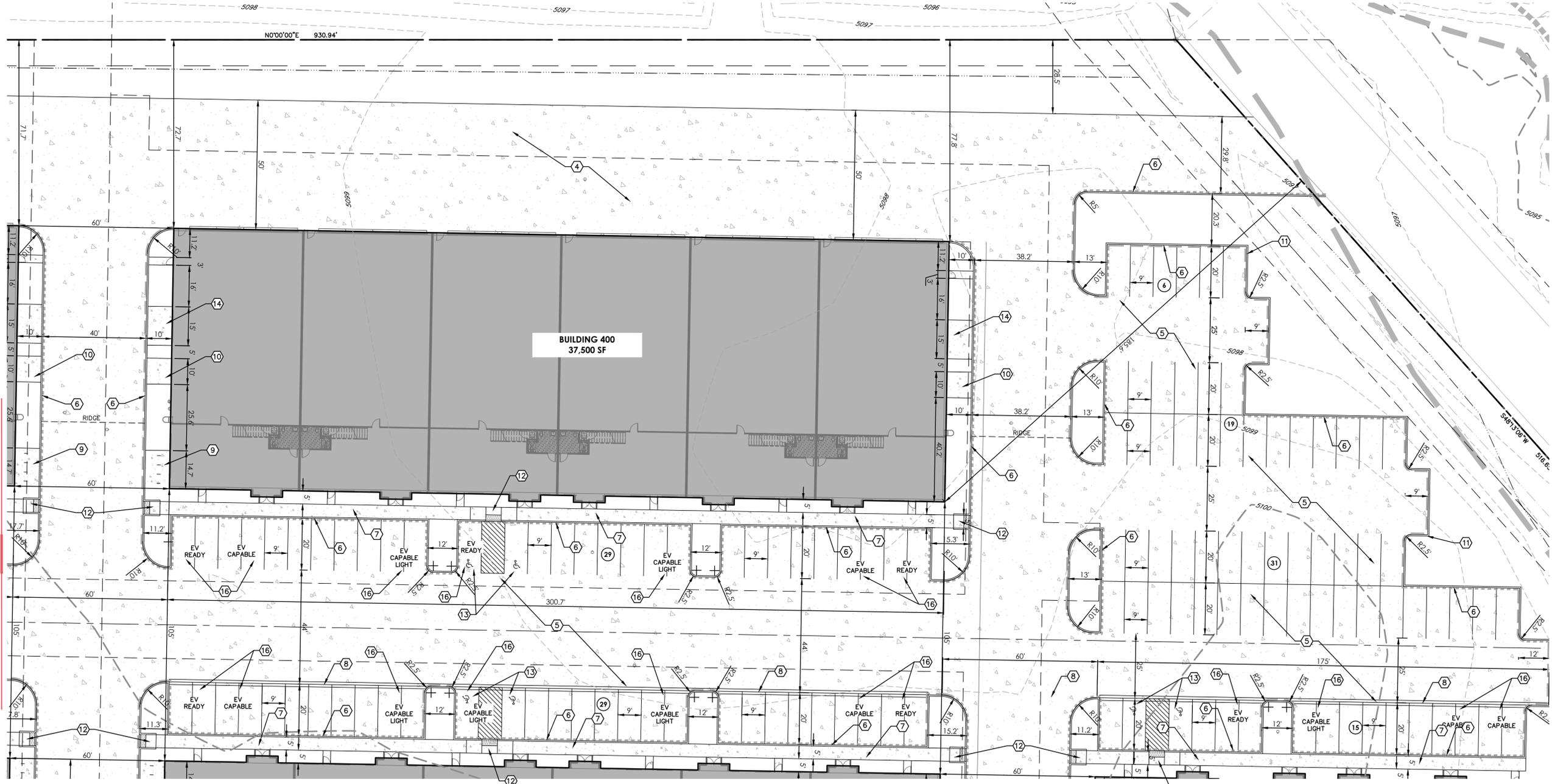
- EXISTING ASPHALT ROADWAY TO REMAIN.
- PROPOSED CONCRETE DRIVEWAY.
- PROPOSED STANDARD DRIVE ENTRANCE.
- PROPOSED CONCRETE TAXIWAY.
- PROPOSED CONCRETE PARKING AREA.
- PROPOSED CURB AND GUTTER.
- PROPOSED CONCRETE SIDEWALK.
- PROPOSED 2' CONCRETE DRAIN PAN.
- PROPOSED BICYCLE RACKS ON CONCRETE SLAB.
- PROPOSED TRASH ENCLOSURE WITH FLUSH CURB.
- PROPOSED 1' CURB CUT.
- ADA CURB RAMP.
- TWO (2) PROPOSED ADA PARKING SPACES WITH VAN ACCESSIBLE AND ADA SIGNAGE.
- CONCRETE ACCESS FOR OVERHEAD DOOR WITH FLUSH CURB.
- PARKING SPACE WITH THE REQUIRED NUMBER OF ELECTRIC VEHICLE SUPPLY EQUIPMENT (EVSE) INSTALLED.
- PARKING SPACE WITH THE REQUIRED NUMBER OF EV-READY AND EV-CAPABLE EQUIPMENT INSTALLED.



KEY MAP

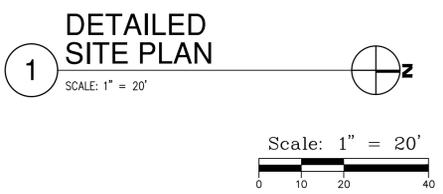
SCALE: 1" = 200'

For
Submittal

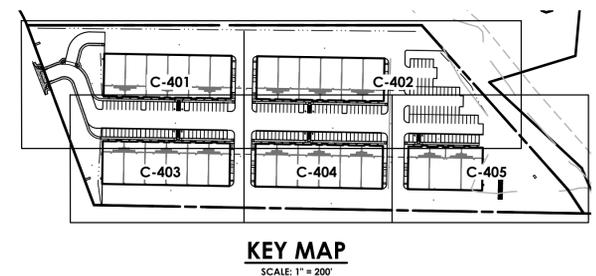


| LEGEND | |
|--------|---------------------|
| | PROPOSED CATCH CURB |
| | PROPOSED SPILL CURB |

- NOTES:
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| KEYED NOTES | |
|-------------|---|
| 1. | EXISTING ASPHALT ROADWAY TO REMAIN. |
| 2. | PROPOSED CONCRETE DRIVEWAY. |
| 3. | PROPOSED STANDARD DRIVE ENTRANCE. |
| 4. | PROPOSED CONCRETE TAXIWAY. |
| 5. | PROPOSED CONCRETE PARKING AREA. |
| 6. | PROPOSED CURB AND GUTTER. |
| 7. | PROPOSED CONCRETE SIDEWALK. |
| 8. | PROPOSED 2' CONCRETE DRAIN PAN. |
| 9. | PROPOSED BICYCLE RACKS ON CONCRETE SLAB. |
| 10. | PROPOSED TRASH ENCLOSURE WITH FLUSH CURB. |
| 11. | PROPOSED 1' CURB CUT. |
| 12. | ADA CURB RAMP. |
| 13. | TWO (2) PROPOSED ADA PARKING SPACES WITH VAN ACCESSIBLE AND ADA SIGNAGE. |
| 14. | CONCRETE ACCESS FOR OVERHEAD DOOR WITH FLUSH CURB. |
| 15. | PARKING SPACE WITH THE REQUIRED NUMBER OF ELECTRIC VEHICLE SUPPLY EQUIPMENT (EVSE) INSTALLED. |
| 16. | PARKING SPACE WITH THE REQUIRED NUMBER OF EV-READY AND EV-CAPABLE EQUIPMENT INSTALLED. |



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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Detailed Site Plan

Sheet

C-402

For
Submittal

**Lot 1
Erie Air Park
Replat D**
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Erie, Colorado 80516

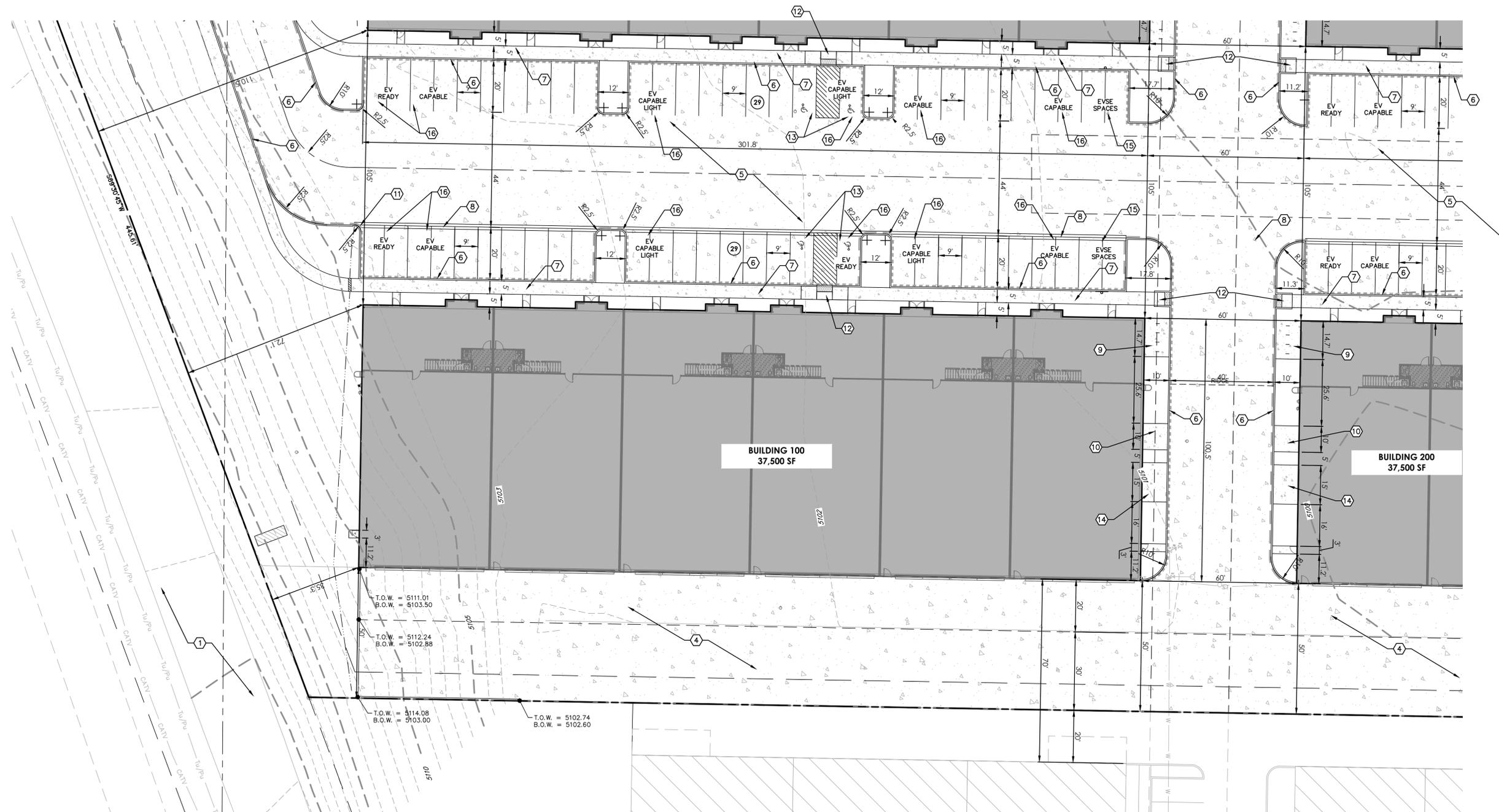
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Detailed Site Plan

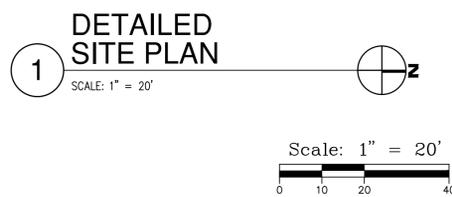
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C-403

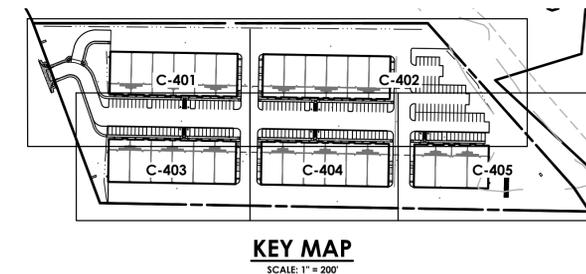


| LEGEND | |
|--------|---------------------|
| | PROPOSED CATCH CURB |
| | PROPOSED SPILL CURB |

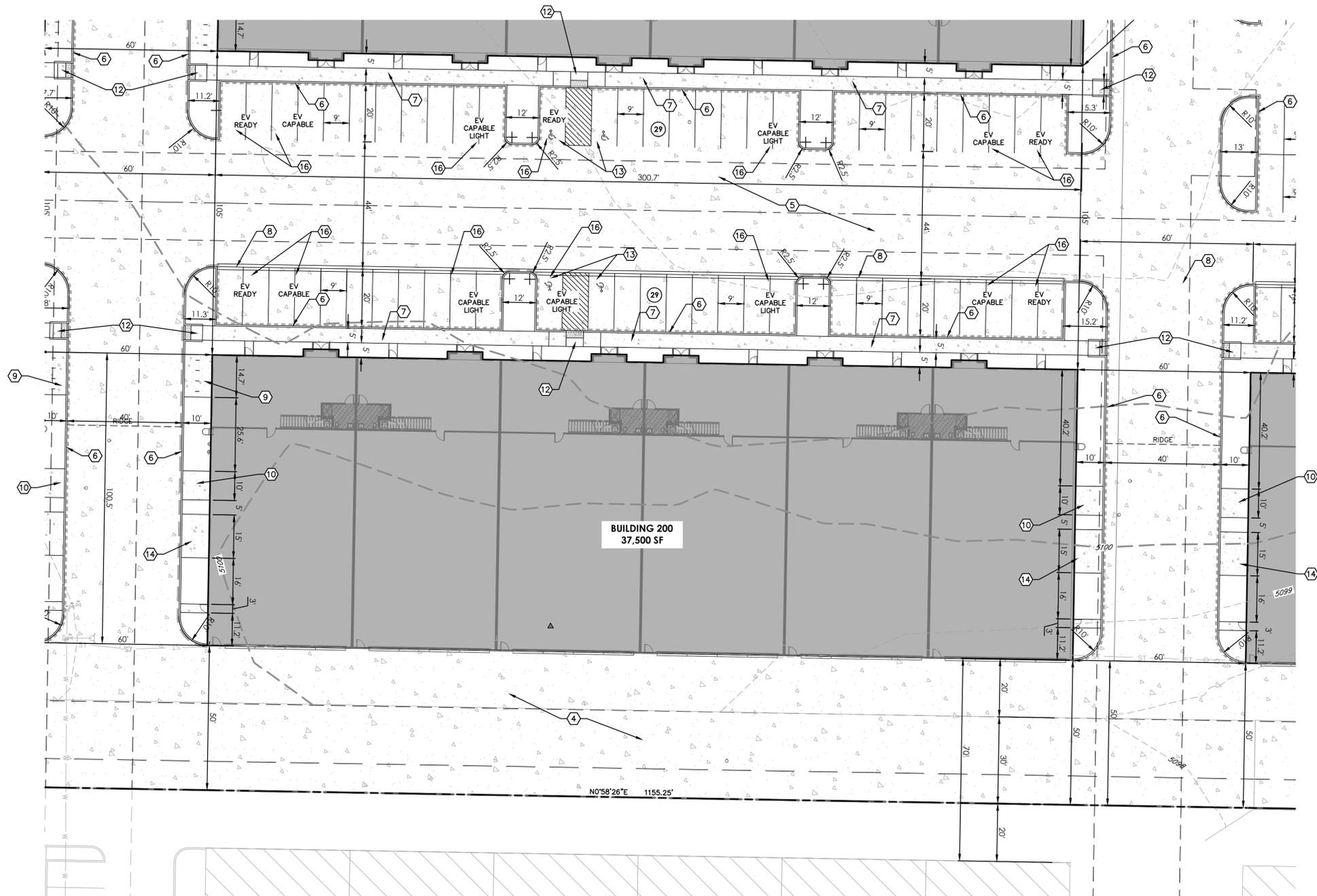
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 - MINIMUM NUMBER OF EV CAPABLE LIGHT SPACE (10% OF 185 PROVIDED) = 19 SPACES/20 SPACES PROVIDED.



| KEYED NOTES | |
|-------------|---|
| 1. | EXISTING ASPHALT ROADWAY TO REMAIN. |
| 2. | PROPOSED CONCRETE DRIVEWAY. |
| 3. | PROPOSED STANDARD DRIVE ENTRANCE. |
| 4. | PROPOSED CONCRETE TAXIWAY. |
| 5. | PROPOSED CONCRETE PARKING AREA. |
| 6. | PROPOSED CURB AND GUTTER. |
| 7. | PROPOSED CONCRETE SIDEWALK. |
| 8. | PROPOSED 2' CONCRETE DRAIN PAN. |
| 9. | PROPOSED BICYCLE RACKS ON CONCRETE SLAB. |
| 10. | PROPOSED TRASH ENCLOSURE WITH FLUSH CURB. |
| 11. | PROPOSED 1' CURB CUT. |
| 12. | ADA CURB RAMP. |
| 13. | TWO (2) PROPOSED ADA PARKING SPACES WITH VAN ACCESSIBLE AND ADA SIGNAGE. |
| 14. | CONCRETE ACCESS FOR OVERHEAD DOOR WITH FLUSH CURB. |
| 15. | PARKING SPACE WITH THE REQUIRED NUMBER OF ELECTRIC VEHICLE SUPPLY EQUIPMENT (EVSE) INSTALLED. |
| 16. | PARKING SPACE WITH THE REQUIRED NUMBER OF EV-READY AND EV-CAPABLE EQUIPMENT INSTALLED. |



For
Submittal



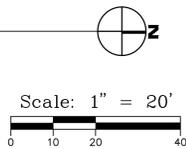
| LEGEND | |
|--------|---------------------|
| | PROPOSED CATCH CURB |
| | PROPOSED SPILL CURB |

NOTES:

- EV PARKING SPACES WILL BE PROVIDED IN ACCORDANCE WITH IBC AND IECC 2021.
- MINIMUM NUMBER OF SPACES WITH EVSE INSTALLED (2% OF 185 PROVIDED) = 4 SPACES REQUIRED/4 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV-READY SPACES (8% OF 185 PROVIDED) = 15 SPACES REQUIRED/16 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV-CAPABLE SPACES (10% OF 185 PROVIDED) = 19 SPACES REQUIRED/20 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV CAPABLE LIGHT SPACE (10% OF 185 PROVIDED) = 19 SPACES/20 SPACES PROVIDED.

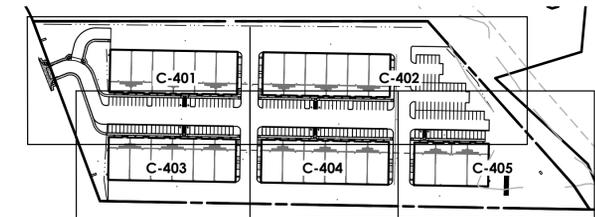
1 DETAILED SITE PLAN

SCALE: 1" = 20'



KEYED NOTES

- EXISTING ASPHALT ROADWAY TO REMAIN.
- PROPOSED CONCRETE DRIVEWAY.
- PROPOSED STANDARD DRIVE ENTRANCE.
- PROPOSED CONCRETE TAXIWAY.
- PROPOSED CONCRETE PARKING AREA.
- PROPOSED CURB AND GUTTER.
- PROPOSED CONCRETE SIDEWALK.
- PROPOSED 2' CONCRETE DRAIN PAN.
- PROPOSED BICYCLE RACKS ON CONCRETE SLAB.
- PROPOSED TRASH ENCLOSURE WITH FLUSH CURB.
- PROPOSED 1' CURB CUT.
- ADA CURB RAMP.
- TWO (2) PROPOSED ADA PARKING SPACES WITH VAN ACCESSIBLE AND ADA SIGNAGE.
- CONCRETE ACCESS FOR OVERHEAD DOOR WITH FLUSH CURB.
- PARKING SPACE WITH THE REQUIRED NUMBER OF ELECTRIC VEHICLE SUPPLY EQUIPMENT (EVSE) INSTALLED.
- PARKING SPACE WITH THE REQUIRED NUMBER OF EV-READY AND EV-CAPABLE EQUIPMENT INSTALLED.



KEY MAP
SCALE: 1" = 200'

Lot 1
Erie Air Park
Replat D

3020-3100 Airport Drive
Erie, Colorado 80516

| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

Project No: 23126A
By: JAS/DPA
File: 23126A-1

Detailed Site Plan

Sheet

C-404

**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

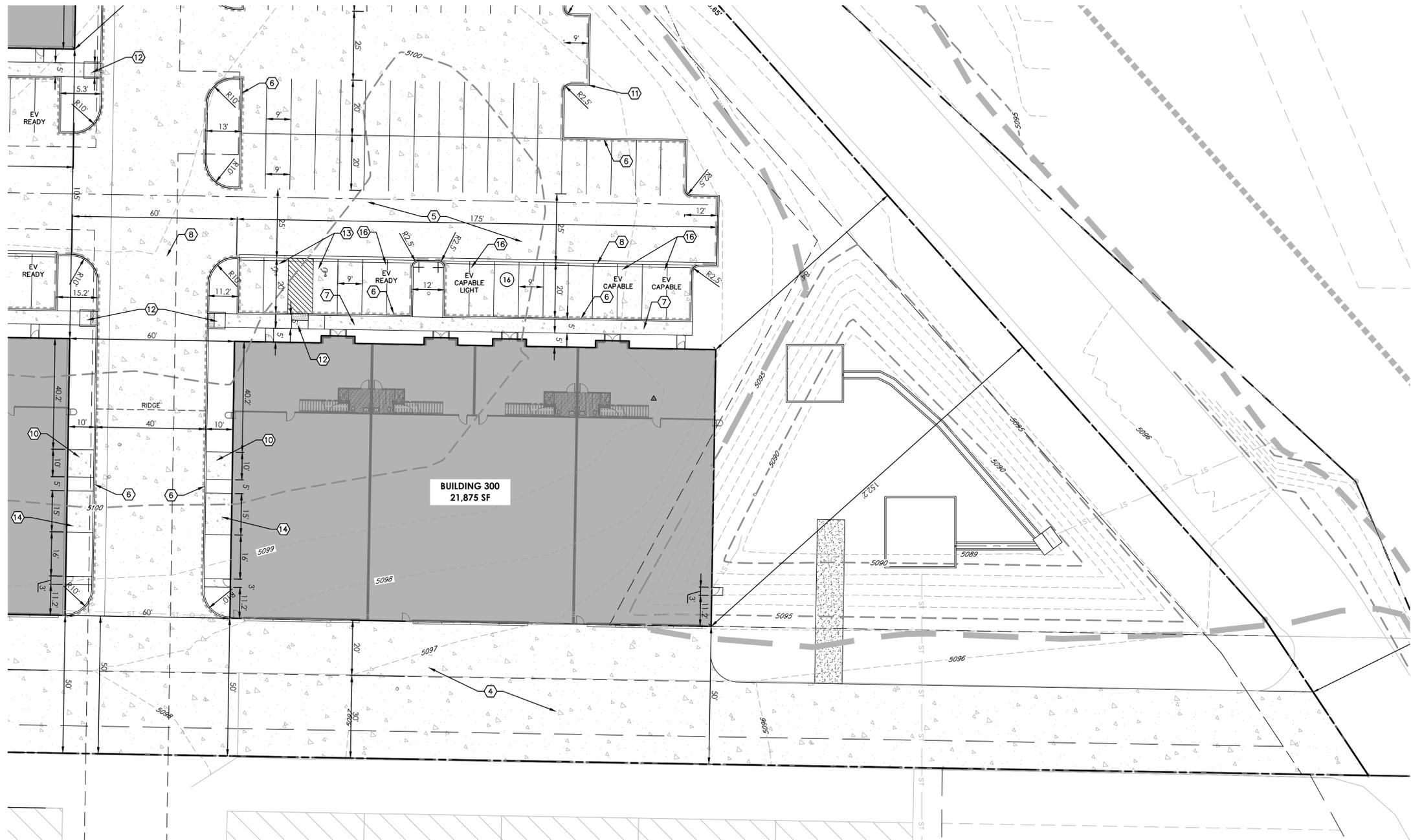
| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

Project No: 23126A
By: JAS/DPA
File: 23126A-1

Detailed Site Plan

Sheet

C-405



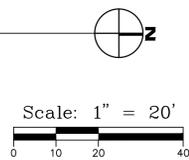
| LEGEND | |
|--------|---------------------|
| | PROPOSED CATCH CURB |
| | PROPOSED SPILL CURB |

NOTES:

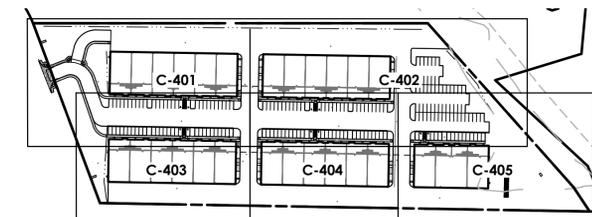
- EV PARKING SPACES WILL BE PROVIDED IN ACCORDANCE WITH IBC AND IECC 2021.
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 - MINIMUM NUMBER OF EV CAPABLE LIGHT SPACE (10% OF 185 PROVIDED) = 19 SPACES/20 SPACES PROVIDED.

1
**DETAILED
SITE PLAN**

SCALE: 1" = 20'

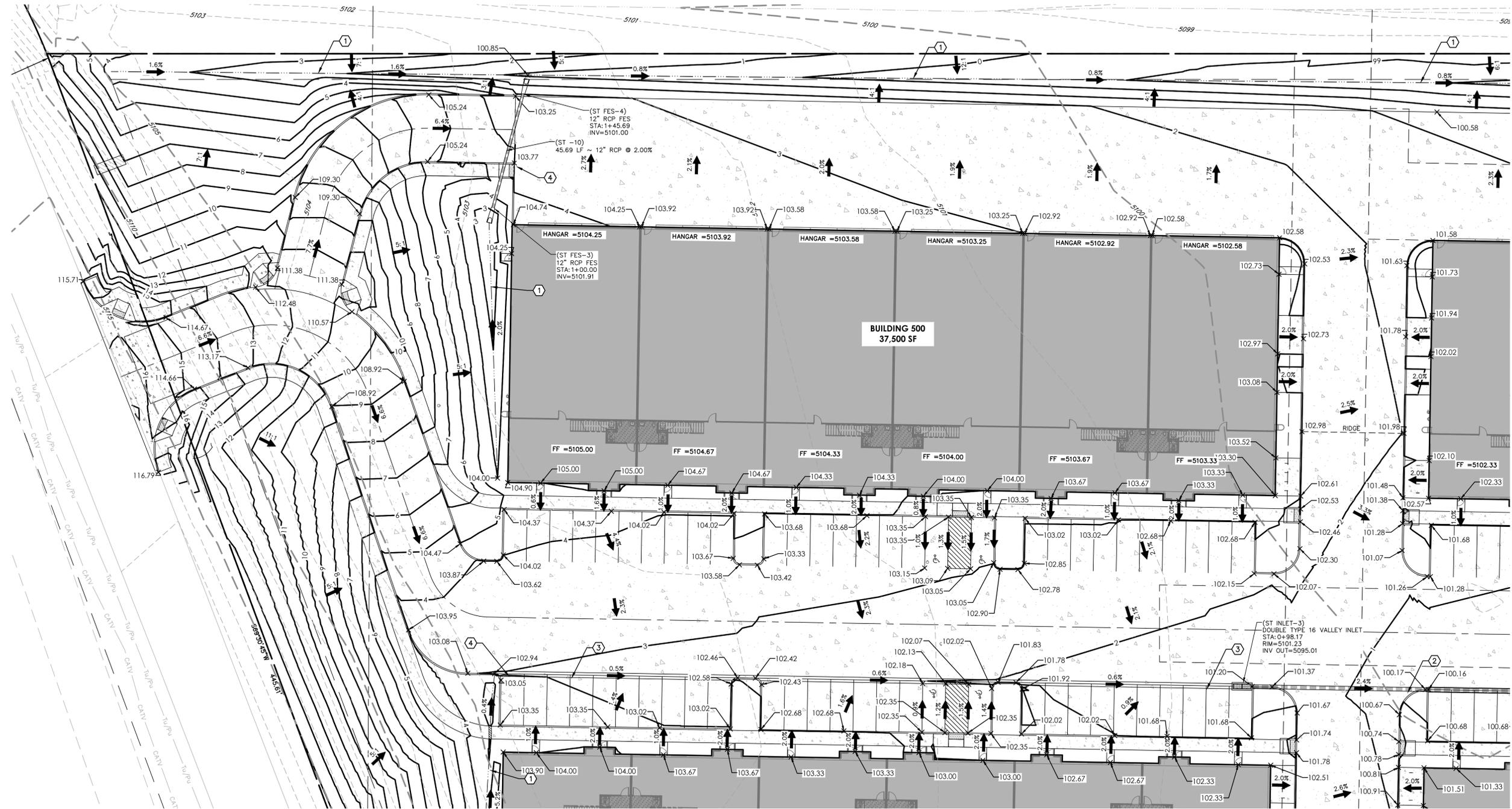


| KEYED NOTES | |
|-------------|---|
| 1. | EXISTING ASPHALT ROADWAY TO REMAIN. |
| 2. | PROPOSED CONCRETE DRIVEWAY. |
| 3. | PROPOSED STANDARD DRIVE ENTRANCE. |
| 4. | PROPOSED CONCRETE TAXIWAY. |
| 5. | PROPOSED CONCRETE PARKING AREA. |
| 6. | PROPOSED CURB AND GUTTER. |
| 7. | PROPOSED CONCRETE SIDEWALK. |
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| 9. | PROPOSED BICYCLE RACKS ON CONCRETE SLAB. |
| 10. | PROPOSED TRASH ENCLOSURE WITH FLUSH CURB. |
| 11. | PROPOSED 1' CURB CUT. |
| 12. | ADA CURB RAMP. |
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| 14. | CONCRETE ACCESS FOR OVERHEAD DOOR WITH FLUSH CURB. |
| 15. | PARKING SPACE WITH THE REQUIRED NUMBER OF ELECTRIC VEHICLE SUPPLY EQUIPMENT (EVSE) INSTALLED. |
| 16. | PARKING SPACE WITH THE REQUIRED NUMBER OF EV-READY AND EV-CAPABLE EQUIPMENT INSTALLED. |



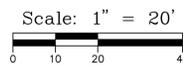
KEY MAP
SCALE: 1" = 200'

For
Submittal



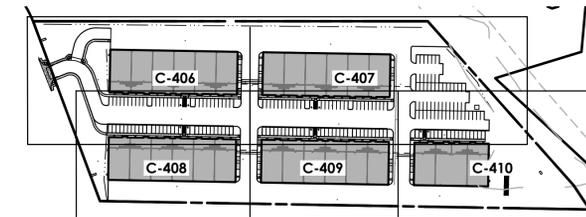
1 DETAILED GRADING PLAN

SCALE: 1" = 50'



KEYED NOTES

1. PROPOSED DRAINAGE SWALE.
2. PROPOSED STORM SEWER.
3. PROPOSED 2' CONCRETE DRAIN PAN.
4. PROPOSED 1' CURB CUT.



KEY MAP

SCALE: 1" = 200'

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Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

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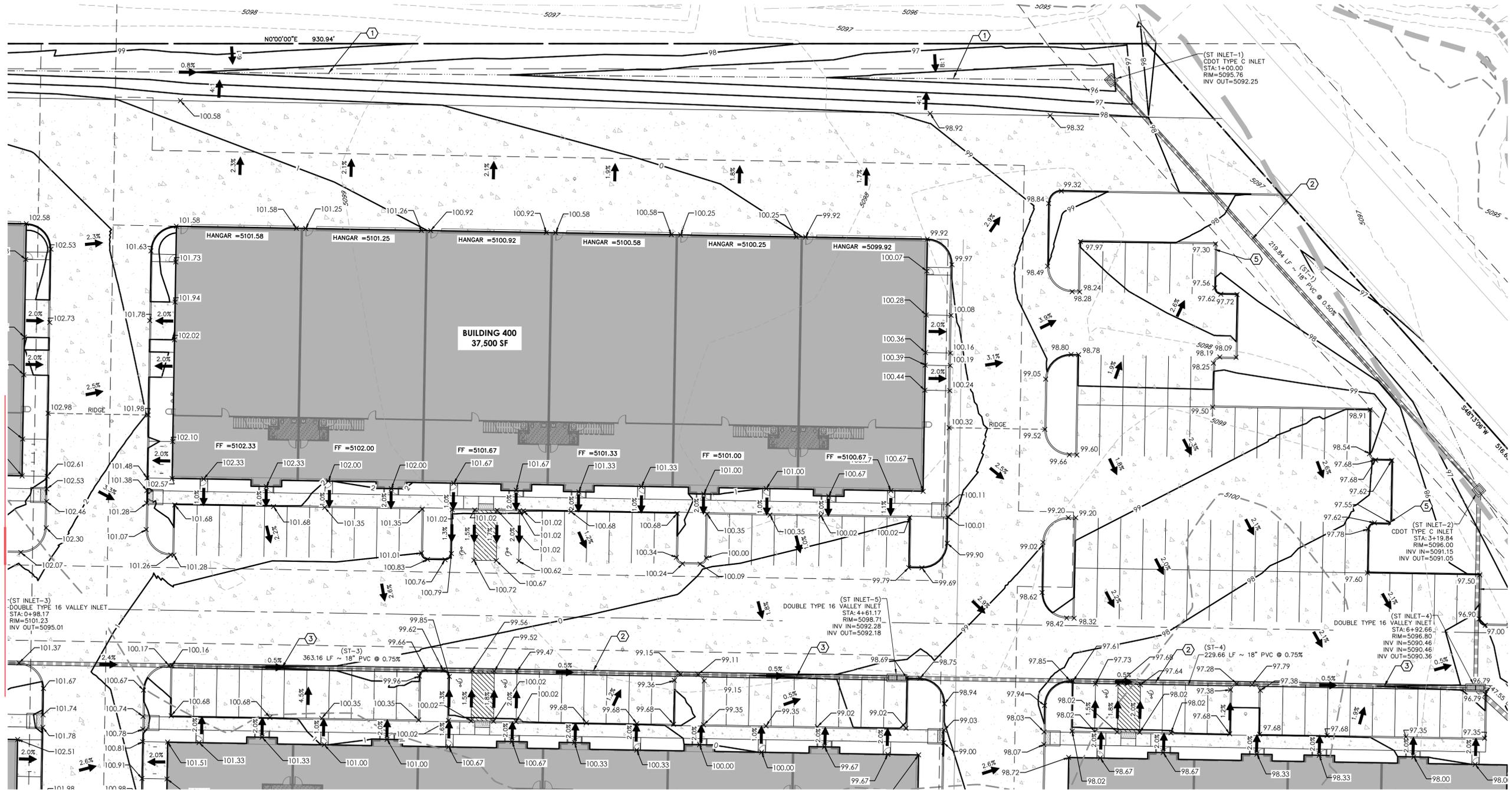
Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Detailed Grading
Plan**

Sheet

C-406

For
Submittal

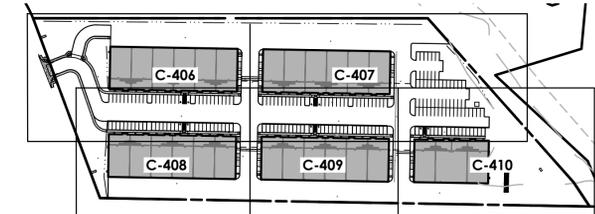


1 DETAILED GRADING PLAN

SCALE: 1" = 50'

KEYED NOTES

1. PROPOSED DRAINAGE SWALE.
2. PROPOSED STORM SEWER.
3. PROPOSED 2' CONCRETE DRAIN PAN.
4. PROPOSED STORMWATER QUALITY/DETENTION FACILITY.
5. PROPOSED 1' CURB CUT.



KEY MAP

SCALE: 1" = 200'

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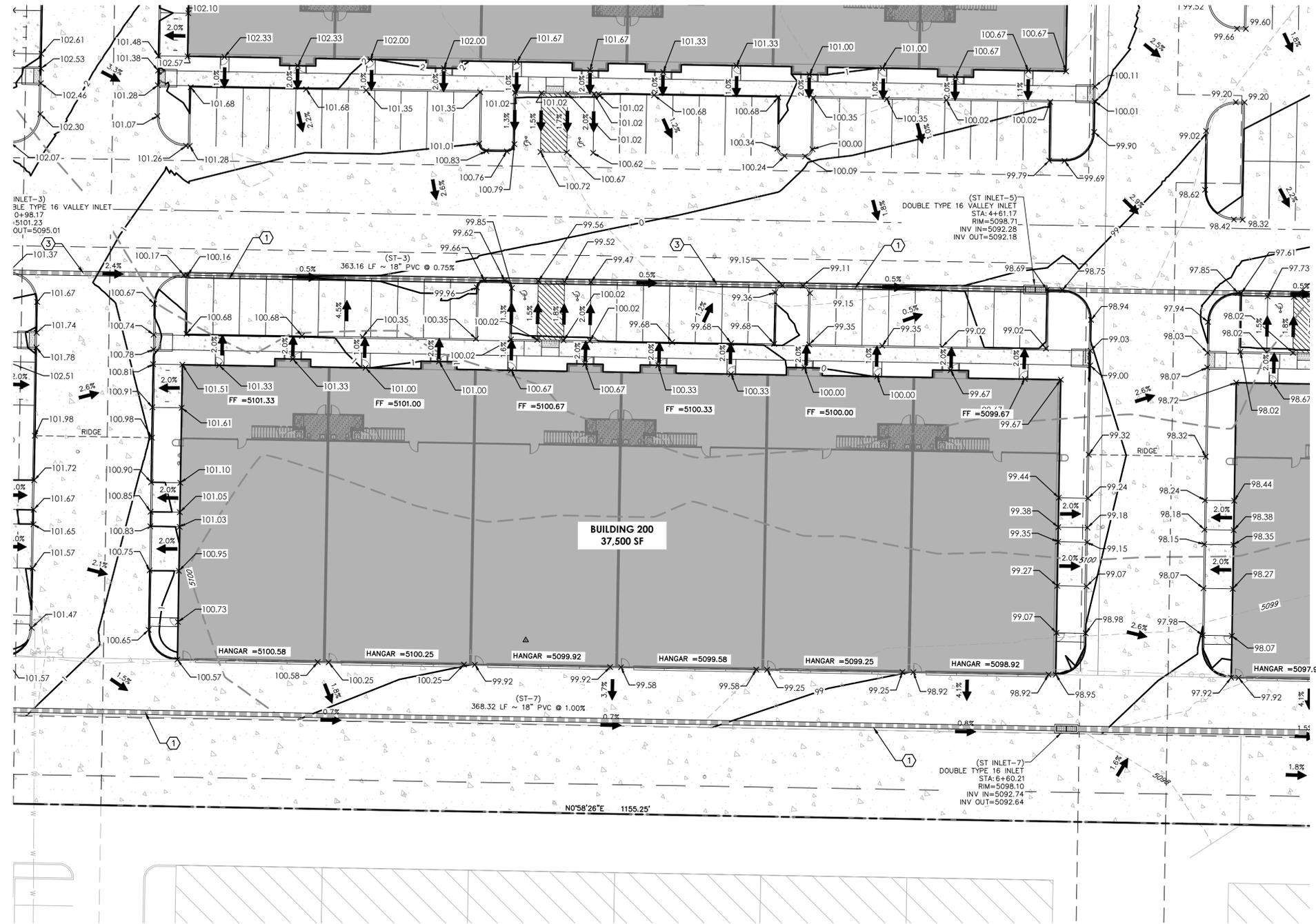
Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Detailed Grading
Plan**

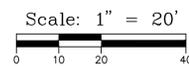
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C-407

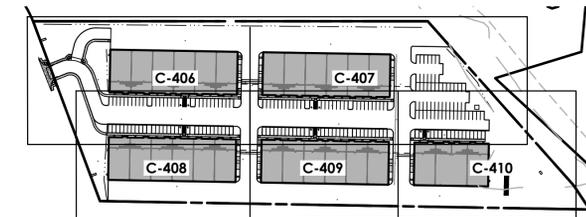
For
Submittal



1 DETAILED GRADING PLAN
SCALE: 1" = 50'



| KEYED NOTES | |
|-------------|---|
| 1. | PROPOSED STORM SEWER. |
| 2. | PROPOSED STORM SEWER FOR ROOF DRAINS. |
| 3. | PROPOSED 2' CONCRETE DRAIN PAN. |
| 4. | PROPOSED STORMWATER QUALITY/DETECTION FACILITY. |
| 5. | PROPOSED LIMITED RELEASE OUTLET STRUCTURE. |



KEY MAP
SCALE: 1" = 200'

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Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Detailed Grading
Plan**

Sheet

C-409

For
Submittal

**Lot 1
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Replat D**
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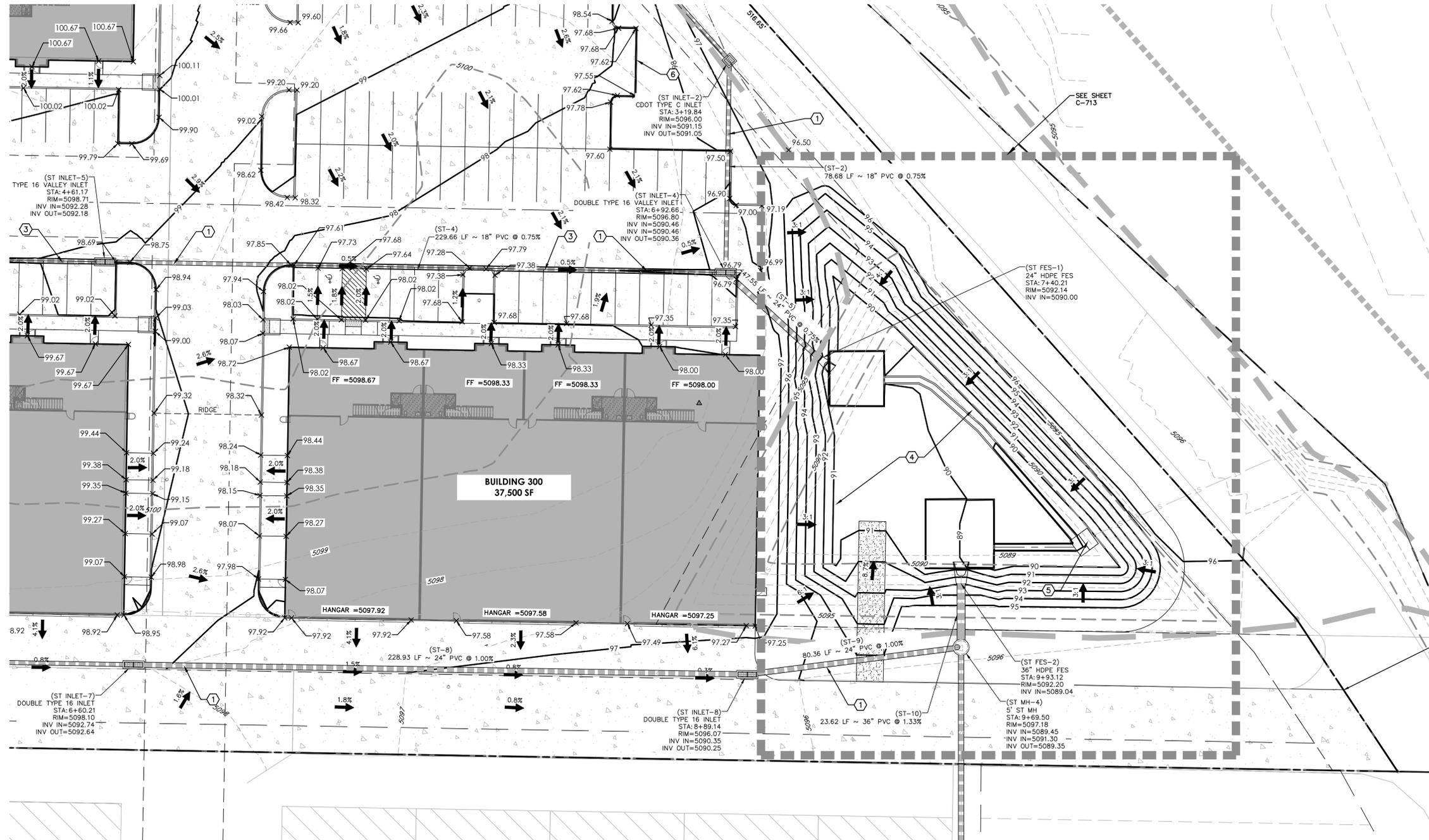
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

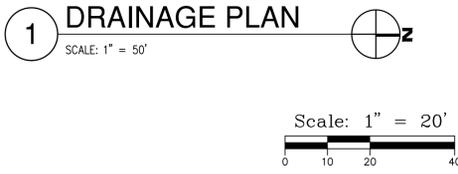
**Detailed Grading
Plan**

Sheet

C-410

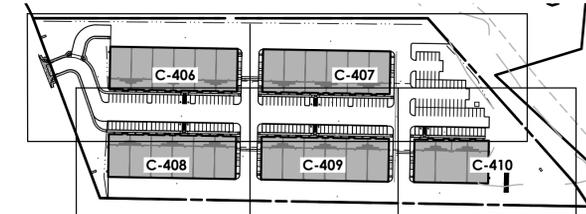


**GRADING AND
DRAINAGE PLAN**

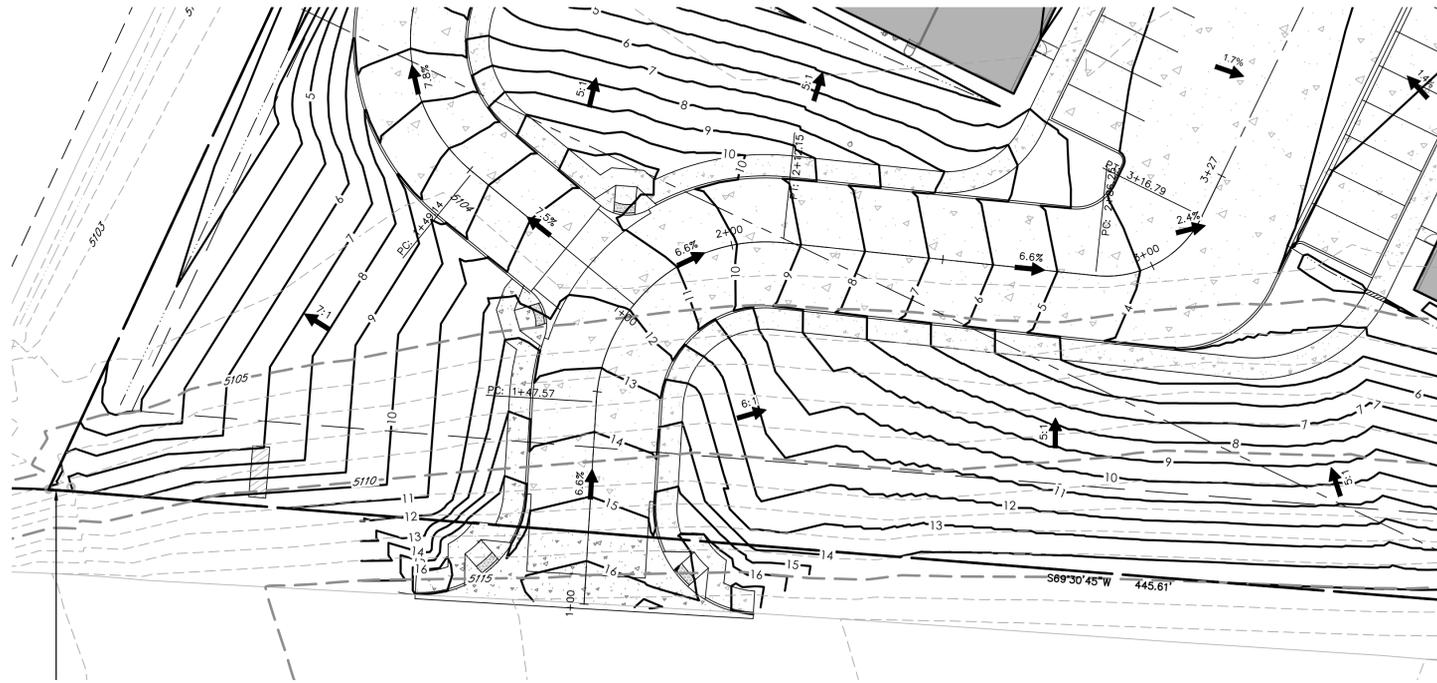


KEYED NOTES

1. PROPOSED STORM SEWER.
2. PROPOSED STORM SEWER FOR ROOF DRAINS.
3. PROPOSED 2' CONCRETE DRAIN PAN.
4. PROPOSED STORMWATER QUALITY/RETENTION FACILITY.
5. PROPOSED LIMITED RELEASE OUTLET STRUCTURE.
6. PROPOSED 1' CURB CUT.



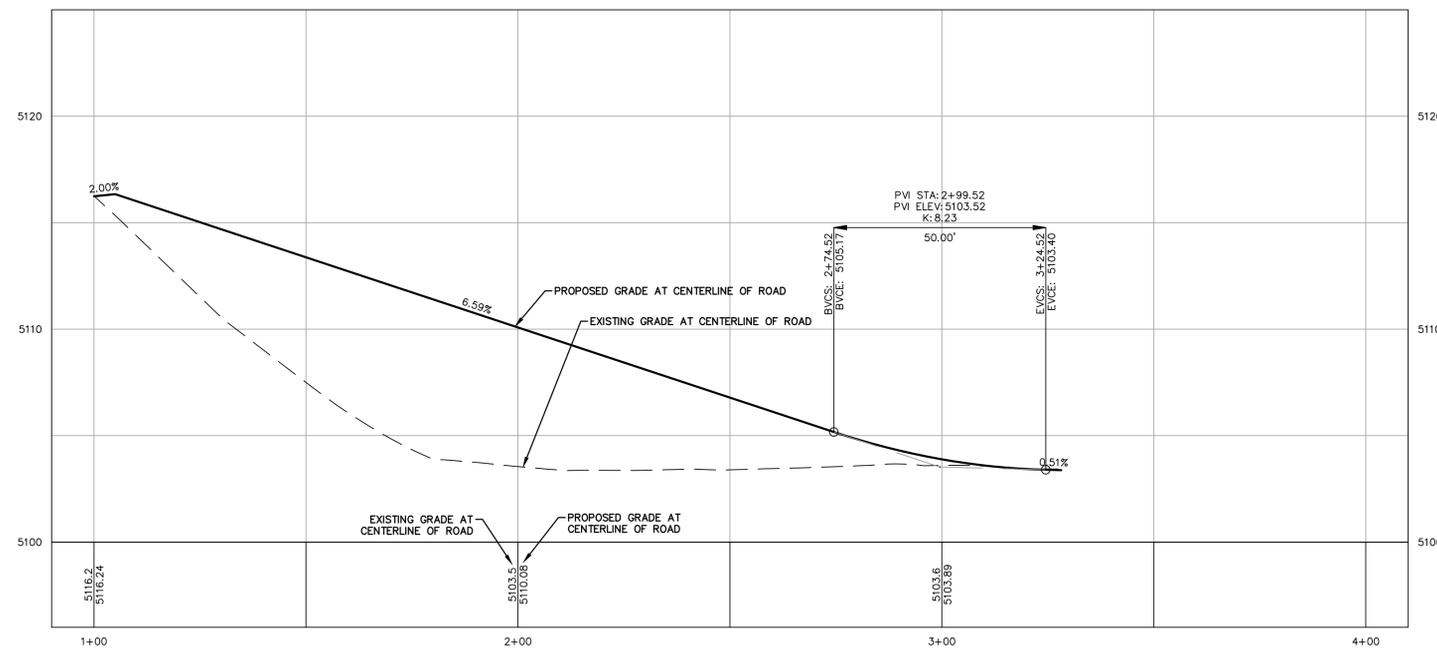
KEY MAP
SCALE: 1" = 200'



1 ROADWAY PLAN
SCALE: 1" = 20'



Scale: 1" = 20'



1 ROADWAY PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

For
Submittal

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Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

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File: 23126A-1

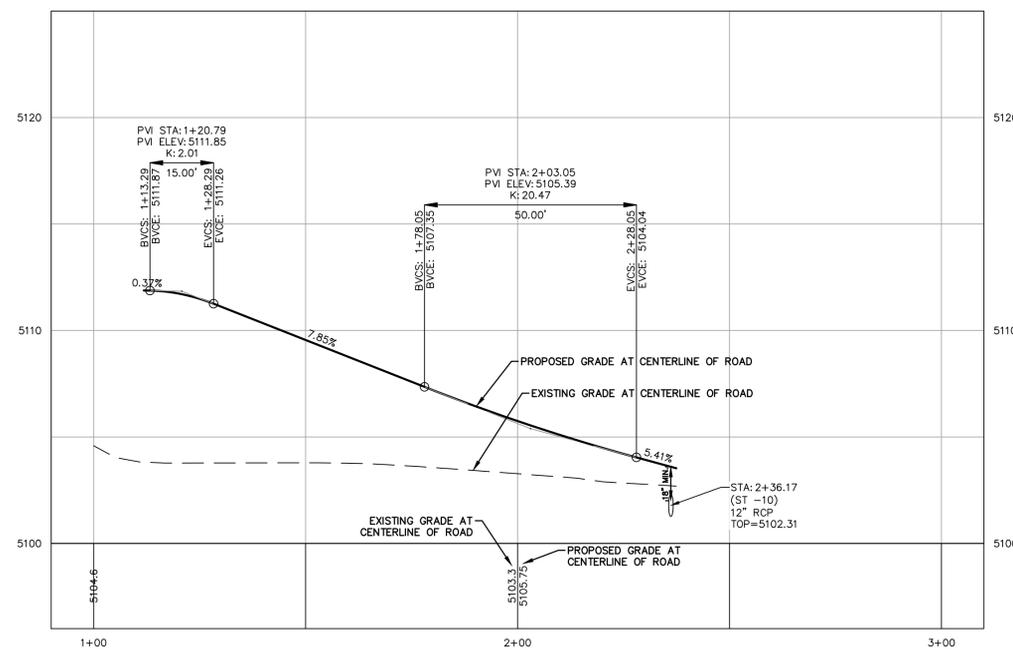
**Roadway Plan
and Profile**

Sheet

C-501



1 ROADWAY PLAN
 SCALE: 1" = 20'
 Scale: 1" = 20'



1 ROADWAY PROFILE
 SCALE: HORIZ: 1" = 20'
 VERT: 1" = 5'

For
 Submittal

**Lot 1
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 3020-3100 Airport Drive
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Project No: 23126A
 By: JAS/DPA
 File: 23126A-1

**Roadway Plan
 and Profile**

Sheet

For
Submittal

**Lot 1
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3020-3100 Airport Drive
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Photometric Plan

Sheet

C-601



For
Submittal

LUMINAIRE SCHEDULE

| CALLOUT | SYMBOL | MODEL | DESCRIPTION | LAMP | MOUNTING | MOUNTING HEIGHT | NOTES | QUANTITY |
|---------|--------|---|---|---------|----------|-----------------|------------|----------|
| AL-1 | | LITHONIA LIGHTING, DSX1 LED P5 30K T3M MVOLT | DSX1 LED P5 30K T3M MVOLT | (1) LED | POLE | 20' | LED DRIVER | 4 |
| AL-3 | | LITHONIA LIGHTING, DSX1 LED P5 30K T4M MVOLT | DSX1 LED P5 30K T4M MVOLT | (1) LED | POLE | 20' | LED DRIVER | 5 |
| WL-1 | | LITHONIA LIGHTING, DSXW1 LED 20C 1000 30K T3M MVOLT | DSXW1 LED WITH (2) 10 LED LIGHT ENGINES, TYPE T3M OPTIC, 3000K, @ 1000mA. | (1) LED | WALL | 20' | LED DRIVER | 72 |

NOTE: ALL EXTERIOR LIGHT FIXTURES SHALL BE FULL CUT-OFF.



D-Series Size 1 LED Area Luminaire

Specifications

EPA: 1.07 f/4 (0.08 ft)

Length: 33" (08.4 cm)

Width: 13" (03.3 cm)

Height H1: 7-1/2" (019 cm)

Height H2: 3-1/2" (09.1 cm)

Weight (max): 27 lbs (12.2 kg)

Introduction

The modern styling of the D-Series is striking yet unobtrusive - making a bold, progressive statement even as it blends seamlessly with its environment. The D-Series distills the benefits of the latest in LED technology into a high performance, high efficacy, long-life luminaire.

The outstanding photometric performance results in sites with excellent uniformity, greater pole spacing and lower power density. It is ideal for replacing up to 750W metal halide in pedestrian and area lighting applications with typical energy savings of 65% and expected service life of over 100,000 hours.

Ordering Information EXAMPLE: DSX1 LED P7 40K T3M MVOLT SPA NLTAR2 PIRHN DDBXD

| Series | LEDs | Color Temperature | Distribution | Voltage | Mounting |
|-----------------|-----------------|--|--------------------|--|----------------------------|
| DSX1 LED | Forward optics | 30K 3000K | T15 Type I short | T195 Type V very short | MVOLT ¹ |
| | | 40K 4000K | T25 Type II short | T255 Type V short | 120" |
| | Retained optics | 50K 5000K | T35 Type III short | T355 Type V medium | 208" |
| | | 60K 6000K | T45 Type IV short | T455 Type V wide | 240" |
| Control options | PIR | High flow, motion/ambient sensor, 8-15' mounting height, ambient sensor enabled at 5% ^{1,2} | HS | House-side shield ¹² | DDBXD Dark bronze |
| | | | DF | Double face (208, 240, 480V) ^{1,18} | DBXD Black |
| | PIRHCIV | High flow, motion/ambient sensor, 8-15' mounting height, ambient sensor enabled at 1% ^{1,2} | ES | External glare shield ¹⁸ | DWDXD White |
| | | | ESD | External glare shield ¹⁸ | DBDXD Textured dark bronze |

Shipped installed

NLTAR2 Hi-Light AR projection 2 enabled⁴

PIRHN Network, high/low motion/ambient sensor³

PER NEMA twist-lock receptacle only (controls ordered separately)¹⁰

PER5 Five-pin receptacle only (controls ordered separately)^{10,11}

PERD Six-pin receptacle only (controls ordered separately)^{10,11}

DNG 8-10' downray wires pulled outside fixture (for use with an external control, ordered separately)¹²

DS Dual switching^{10,11,14}

Other options

PIRHCIV High flow, motion/ambient sensor, 8-15' mounting height, ambient sensor enabled at 1%^{1,2}

PIRHCIV Bi-level, motion/ambient sensor, 15-30' mounting height, ambient sensor enabled at 1%^{1,2}

FAO Field adjustable output¹⁴

Shipped installed

HS House-side shield¹²

SF Single face (120, 277 or 347V)^{1,19}

DF Double face (208, 240, 480V)^{1,18}

ESD Left rotated optics¹

R90 Right rotated optics¹

Shipped separately

BS Bird spikes¹⁵

ES External glare shield¹⁸

Finish options

DDBXD Dark bronze

DBXD Black

DWDXD White

DBDXD Textured dark bronze

DBDXD Textured black

DWDXD Textured natural aluminum

DWDXD Textured white

Accessories

CSM Bird deterrent spikes

DSWWSU Wire guard (optional)

DSWWSU Wire guard (optional)

DSWWSU Wire guard (optional)

DSWWSU Wire guard (optional)

Notes

1. 20C, 1000 is not available with PIR, PIRHCIV or PIRHCIV.

2. MVOLT driver operates on any line voltage from 120-277V (50/60 Hz).

3. Single face (SF) requires 120, 277 or 347 voltage option. Double face (DF) requires 208, 240 or 480 voltage option.

4. Only available with 30C, 700mA or 1000mA. Not available with PIR or PIRHCIV.

5. Back box ships installed on fixture. Cannot be field installed. Cannot be ordered as an accessory.

6. Photocell (PE) requires 120, 208, 240, 277 or 347 voltage option. Not available with motion/ambient light sensors (PIR or PIRHCIV).

7. Reference Motion Sensor table on page 3.

8. Cold weather (25C) rated. Not compatible with conduit entry applications. Not available with BWV mounting option. Not available with fusing. Not available with 347 or 480 voltage options. Emergency components located in back box housing. Emergency mode IES files located on product page at www.lithonia.com

9. Not available with SPD.

10. Not available with ELCW.

11. Also available as a separate accessory; see Accessories information.

12. Not available with ELCW.



D-Series Size 1 LED Wall Luminaire

Specifications

Width: 13-3/4" (04.9 cm)

Depth: 10" (25.4 cm)

Height: 6-3/8" (16.2 cm)

Weight: 12 lbs (5.4 kg)

Back Box (BBW, ELCW)

Width: 13-3/4" (04.9 cm)

Depth: 4" (10.2 cm)

Height: 6-3/8" (16.2 cm)

Weight: 5 lbs (2.3 kg)

ELCW Weight: 10 lbs (4.5 kg)

Introduction

The D-Series Wall luminaire is a stylish, fully integrated LED solution for building-mount applications. It features a sleek, modern design and is carefully engineered to provide long-lasting, energy-efficient lighting with a variety of optical and control options for customized performance.

With an expected service life of over 20 years of nighttime use and up to 74% in energy savings over comparable 250W metal halide luminaires, the D-Series Wall is a reliable, low-maintenance lighting solution that produces sites that are exceptionally illuminated.

Ordering Information EXAMPLE: DSXW1 LED 20C 1000 40K T3M MVOLT DDBTXD

| Series | LEDs | Drive Current | Color Temperature | Distribution | Voltage | Mounting | Control Options |
|-----------------|---------------------------|---|--------------------------------|---------------------------------|--------------------|------------------|---|
| DSXW1 LED | 10C 10 LEDs (one engine) | 350 350 mA | 30K 3000K | T25 Type II Short | MVOLT ¹ | Shipped included | Shipped installed |
| | | 530 530 mA | 40K 4000K | T2M Type II Medium | 120" | | |
| | 20C 20 LEDs (two engines) | 700 700 mA | 50K 5000K | T35 Type III Short | 208" | BBW | PE Photometric cell, button type ⁴ |
| | | 1000 1000 mA (1 A) ¹ | AMBPC Amber phosphor converted | T3M Type III Medium | 240" | | |
| Control Options | PIRHCIV | High flow, motion/ambient sensor, 15-30' mounting height, ambient sensor enabled at 1% ^{1,2} | HS | House-side shield ¹² | DDBTXD Dark bronze | DBTXD Black | ESSTD Textured sandstone |
| | | | | | | | |

Shipped installed

SF Single face (120, 277 or 347V)^{1,19}

DF Double face (208, 240 or 480V)^{1,18}

HS House-side shield¹²

SPD Separate surge protection¹²

Shipped separately

BSM Bird deterrent spikes

WG Wire guard

VG Vandal guard

DDL Diffused drop lens

Other options

PIRHCIV High flow, motion/ambient sensor, 15-30' mounting height, ambient sensor enabled at 1%^{1,2}

PIRHCIV Bi-level, motion/ambient sensor, 15-30' mounting height, ambient sensor enabled at 1%^{1,2}

FAO Field adjustable output¹⁴

Shipped installed

HS House-side shield¹²

SF Single face (120, 277 or 347V)^{1,19}

DF Double face (208, 240, 480V)^{1,18}

ESD Left rotated optics¹

R90 Right rotated optics¹

Shipped separately

BS Bird spikes¹⁵

ES External glare shield¹⁸

Finish options

DDBTXD Dark bronze

DBTXD Black

DWDXD White

DBTXD Textured dark bronze

DBTXD Textured black

DWDXD Textured natural aluminum

DWDXD Textured white

Accessories

CSM Bird deterrent spikes

DSWWSU Wire guard (optional)

DSWWSU Wire guard (optional)

DSWWSU Wire guard (optional)

DSWWSU Wire guard (optional)

Notes

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10. Not available with ELCW.

11. Also available as a separate accessory; see Accessories information.

12. Not available with ELCW.

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Erie, Colorado 80516

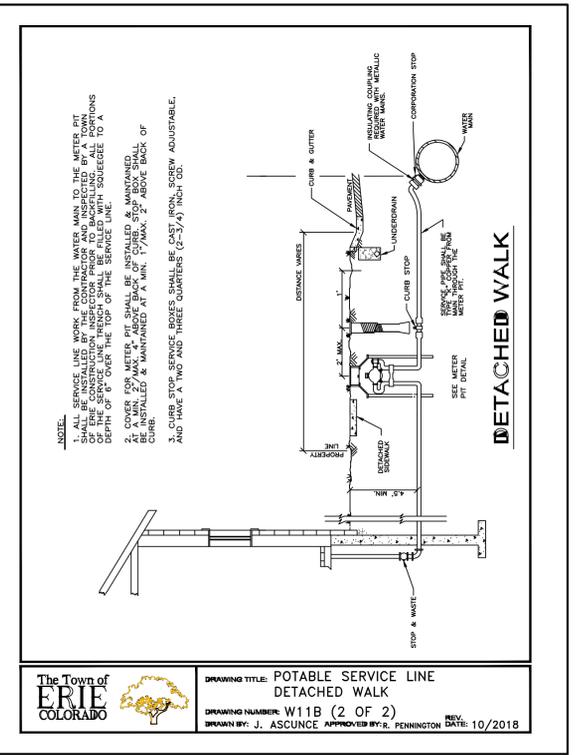
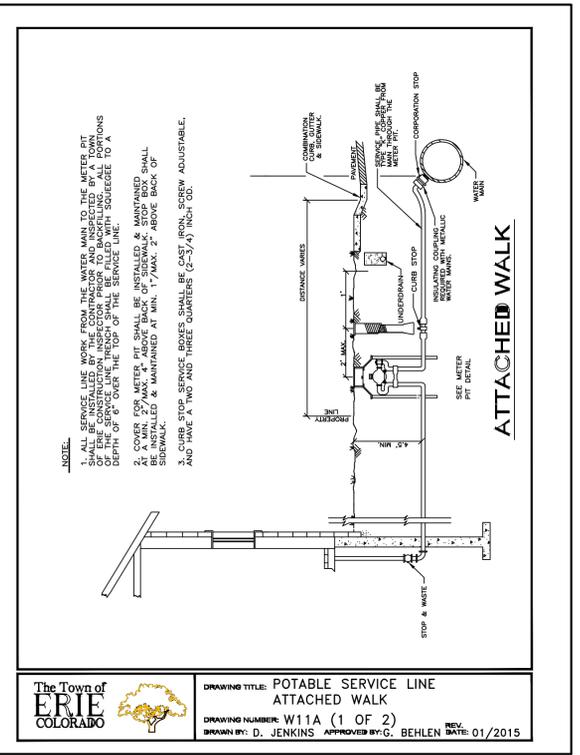
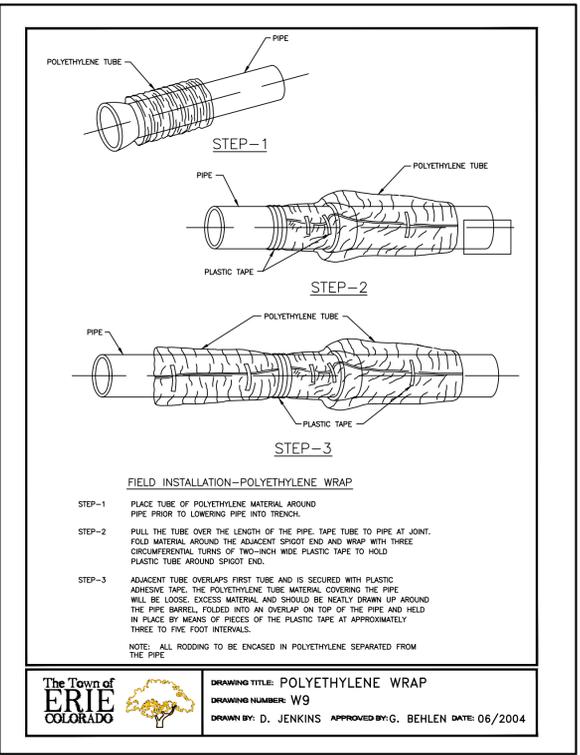
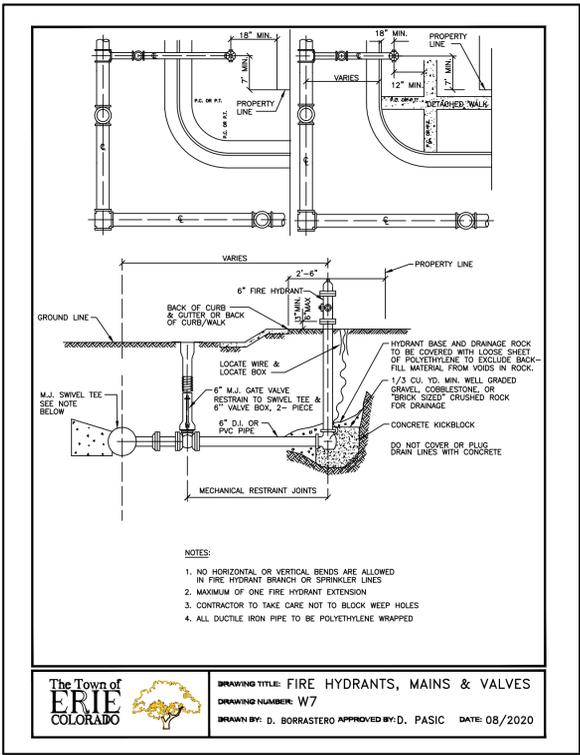
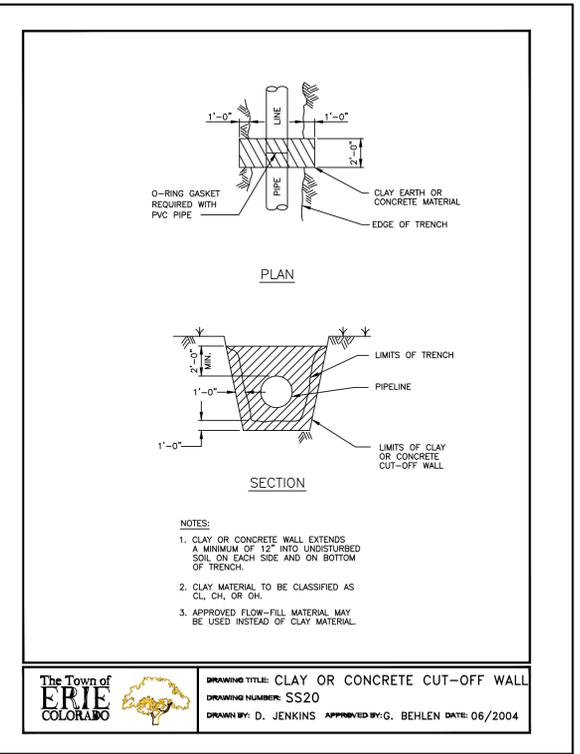
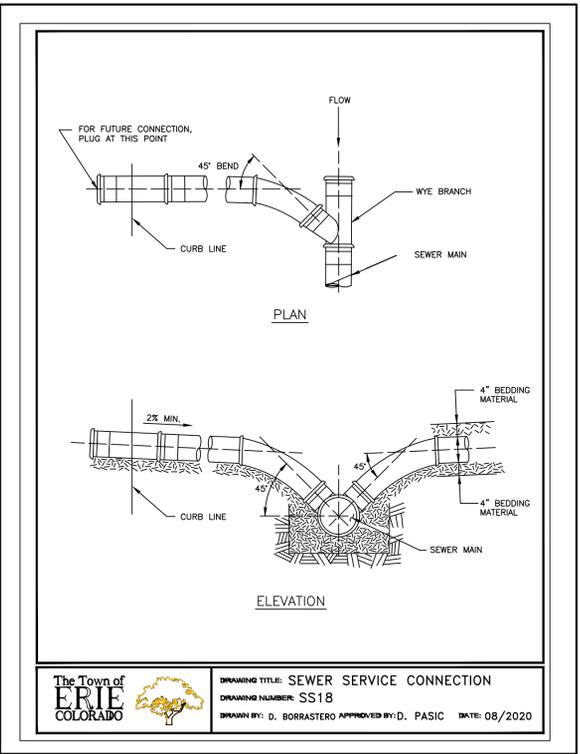
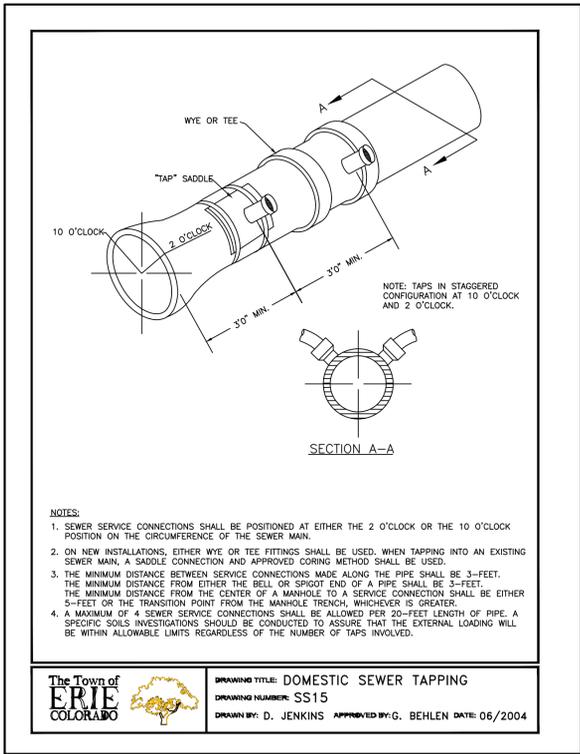
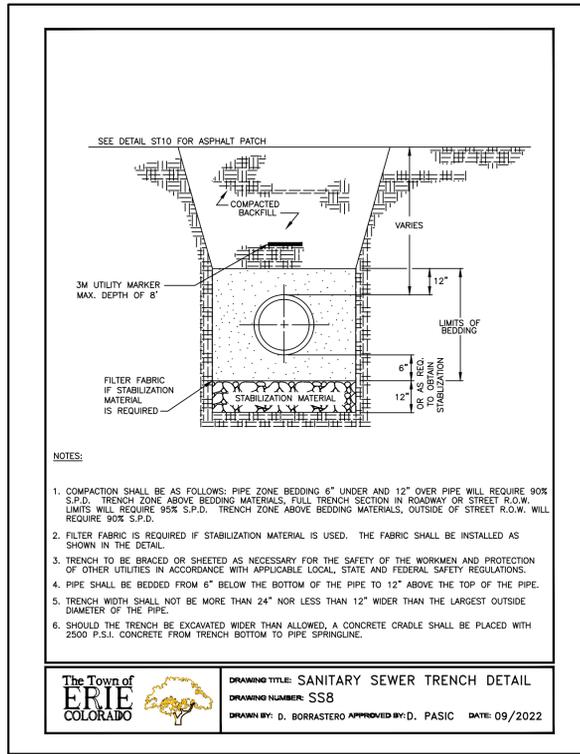
| # | Date | Description |
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| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

Project No: 23126A
By: JAS/DPA
File: 23126A-1

Lighting Details

Sheet

C-602



**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

| # | Date | Description |
|---|----------|-------------------|
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Details

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GENERAL NOTES

- POLYMER CONCRETE SHALL BE CAST AGAINST UNDISTURBED EARTH. FORMS SHALL BE USED TO OBTAIN ADEQUATE BEARING AND TO CONFINE THE CONCRETE. THRUST BLOCKING SHALL BEAR ON THE FITTING OR END CAP ONLY AND SHOULD NOT BE ALLOWED TO SPILL OVER THE JOINT OR AGAINST THE PIPE.
- METER PIT SHALL BE CONSTRUCTED OF MODIFIED POLYETHYLENE WITH MINIMUM WALL THICKNESS OF 0.50".
- EXTENSIONS AND OFF GRADE EXTENSIONS SHALL BE INSERTED BETWEEN THE DOME AND TOP RING TO PUT LID TO GRADE.
- NO CONNECTIONS OR CHANGES IN PIPE DIAMETER SHALL BE MADE IN THE METER PIT OR IN THE DISTANCE OF FIVE FEET BEYOND THE METER PIT WALL ON THE OUTLET SIDE.
- LAWN SPRINKLER CONNECTIONS SHALL BE A MINIMUM OF FIVE FEET FROM THE METER PIT WALL ON THE OUTLET SIDE.
- ANY VARIATION OR DEVIATION FROM THIS STANDARD REQUIRES PREVIOUS APPROVAL FROM THE PUBLIC WORKS DEPT.
- METER PIT MUST BE INSTALLED IN UNDEVELOPED AREA. PRIOR APPROVAL FROM PUBLIC WORKS DEPT. REQUIRED FOR ANY DEVIATION FROM THIS REQUIREMENT.

| METER SIZE | LAYING LENGTH (A) |
|-------------|-------------------|
| 5/8" x 3/4" | 7-1/2" |
| 1" | 10-3/4" |

DRAWING TITLE: POTABLE WATER METER PIT
DRAWING NUMBER: W12A
DRAWN BY: D. BORRASTERO **APPROVED BY:** D. PASIC **REV. DATE:** 08/2020

NOTES:

- SEE THRUST BLOCKING CHART FOR MINIMUM BEARING SURFACE AREAS.
- BASED ON 150 PSI INTERNAL PIPE PRESSURE PLUS WATER HAMMER.
- 4" x 4" AND 1" x 1" WATER HAMMER = 10 P.S.I. 16" x 20" AND 24" WATER HAMMER = 70 P.S.I.
- BASED ON 3,000 PSI SOIL BEARING CAPACITY.
- THRUST BLOCKING SHALL BE CAST AGAINST UNDISTURBED EARTH. FORMS SHALL BE USED AS REQUIRED TO OBTAIN ADEQUATE BEARING AND TO CONFINE THE CONCRETE. THRUST BLOCKING SHALL BEAR ON THE FITTING OR END CAP ONLY AND SHOULD NOT BE ALLOWED TO SPILL OVER THE JOINT OR AGAINST THE PIPE.

DRAWING TITLE: THRUST BLOCK
DRAWING NUMBER: W14A
DRAWN BY: D. JENKINS **APPROVED BY:** G. BEHLEN **REV. DATE:** 01/2015

TABLE OF BEARING AREAS IN SQ. FT. FOR CONCRETE THRUST BLOCKING
FOR 100 P.S.I. INTERNAL STATIC PRESSURE AND 1,000 LBS. PER SQUARE FOOT SOIL BEARING CAPACITY.

| SIZE | BENDS | | | | TEES* | GATE VALVES | DEAD ENDS | CROSS W/ 1 BRANCH PLUGGED | CROSS W/ 2 BRANCHES PLUGGED |
|------|-------|------|---------|---------|-------|-------------|-----------|---------------------------|-----------------------------|
| | 90° | 45° | 22-1/2° | 11-1/4° | | | | | |
| 3 | 1.0 | 0.6 | 0.3 | 1.0 | 0.7 | 0.5 | 0.7 | 0.7 | 0.7 |
| 4 | 1.8 | 1.0 | 0.5 | 1.0 | 1.3 | 0.5 | 1.3 | 1.3 | 1.3 |
| 6 | 4.0 | 2.2 | 1.1 | 1.0 | 2.8 | 0.7 | 2.8 | 2.8 | 2.8 |
| 8 | 7.1 | 3.8 | 2.0 | 1.0 | 5.0 | 2.4 | 5.0 | 5.0 | 5.0 |
| 10 | 11.1 | 6.0 | 3.0 | 1.5 | 7.8 | 4.5 | 7.8 | 7.8 | 7.8 |
| 12 | 16.0 | 8.6 | 4.4 | 2.2 | 11.3 | 7.3 | 11.3 | 11.3 | 11.3 |
| 14 | 21.7 | 11.8 | 6.0 | 3.0 | 15.4 | 11.0 | 15.4 | 15.4 | 15.4 |
| 15 | 25.0 | 13.5 | 7.0 | 3.5 | 17.6 | | 17.6 | 17.6 | 17.6 |
| 16 | 28.4 | 15.3 | 8.0 | 4.0 | 20.0 | | 20.0 | 20.0 | 20.0 |
| 18 | 36.0 | 19.4 | 10.0 | 5.0 | 25.4 | | 25.4 | 25.4 | 25.4 |
| 20 | 44.2 | 24.0 | 12.2 | 6.1 | 31.4 | | 31.4 | 31.4 | 31.4 |
| 21 | 49.0 | 26.5 | 13.5 | 6.8 | 34.6 | | 34.6 | 34.6 | 34.6 |
| 22 | 54.0 | 29.0 | 14.8 | 7.4 | 38.0 | | 38.0 | 38.0 | 38.0 |
| 24 | 64.0 | 34.5 | 17.7 | 8.8 | 45.0 | | 45.0 | 45.0 | 45.0 |
| 30 | 100.0 | 54.0 | 27.6 | 13.8 | 71.0 | | 71.0 | 71.0 | 71.0 |
| 36 | 144.0 | 78.0 | 40.0 | 20.0 | 102.0 | | 102.0 | 102.0 | 102.0 |

* SIZE IS BRANCH SIZE

AREAS GIVEN IN TABLE ARE BASED UPON AN INTERNAL STATIC PRESSURE OF 100 P.S.I. AND A SOIL BEARING CAPACITY OF 1,000 LBS. PER SQUARE FOOT. BEARING AREAS FOR ANY PRESSURE AND SOIL BEARING CAPACITY MAY BE OBTAINED BY MULTIPLYING THE TABULATED VALUES BY A CORRECTION FACTOR "F".

$F = \frac{\text{ACTUAL SPECIFIED TEST PRESSURE IN HUNDREDS OF LBS.}}{\text{ACTUAL SOIL BEARING CAPACITY IN THOUSANDS OF LBS.}}$

EXAMPLE: TO FIND BEARING AREA FOR 8" = 90° BEND WITH A STATIC INTERNAL PRESSURE OF 150 P.S.I. AND WITH A SOIL BEARING CAPACITY OF 3,000 LBS. PER SQUARE FOOT.

$F = \frac{1.5 + 3}{0.5} = 0.5$ TABULATED VALUE = 7.1 SQUARE FOOT.
 $0.5 \times 7.1 = 3.56$ SAY 4 SQUARE FEET OF 2 FOOT LONG BY 2 FOOT HIGH.

DRAWING TITLE: THRUST BLOCKING CHART
DRAWING NUMBER: W14B
DRAWN BY: D. JENKINS **APPROVED BY:** G. BEHLEN **REV. DATE:** 01/2015

NOTES:

- THRUST BLOCKING SHALL BE CAST AGAINST UNDISTURBED EARTH. FORMS SHALL BE USED AS REQUIRED TO OBTAIN ADEQUATE BEARING AND TO CONFINE THE CONCRETE. THRUST BLOCKING SHALL BEAR ON THE FITTING OR END CAP ONLY AND SHOULD NOT BE ALLOWED TO SPILL OVER THE JOINT OR AGAINST THE PIPE.
- VOLUME IS IN CUBIC FEET.
- ALL CONCRETE TO BE 4000 P.S.I. MIN.
- BLOCKS TO BE CENTERED HORIZONTALLY ON THE BEND.
- DESIGN BASED ON A TEST PRESSURE OF 150 P.S.I. AND SAFETY FACTOR (S_f) OF 1.5
- $V_g = S_f \times W_m \times \sin \theta$
- $W_m = 140 \# / FT^3$
- THE DESIGN ENGINEER IS RESPONSIBLE FOR VERIFYING THE ACTUAL SITE CONDITIONS WITH RESPECT TO THE ASSUMPTIONS LISTED ABOVE.

| SIZE OF PIPE (D) | 11 1/4 DEG. | | | 22 1/2 DEG. | | | 45 DEG. | | | | | | | | |
|------------------|-------------|----|----|-------------|----|----|---------|----|----|----|----|----|----|----|-----|
| | L | W | H | L | W | H | L | W | H | | | | | | |
| 4" | 12 | 24 | 12 | 4 | 12 | 34 | 12 | 8 | 22 | 37 | 32 | 22 | 15 | | |
| 6" | 18 | 32 | 27 | 18 | 9 | 15 | 52 | 40 | 15 | 18 | 28 | 64 | 32 | 28 | 33 |
| 8" | 21 | 40 | 33 | 21 | 16 | 22 | 61 | 40 | 22 | 31 | 35 | 64 | 45 | 35 | 58 |
| 10" | 24 | 50 | 36 | 24 | 25 | 30 | 59 | 48 | 30 | 49 | 42 | 72 | 52 | 42 | 90 |
| 12" | 31 | 56 | 36 | 31 | 36 | 36 | 70 | 48 | 36 | 70 | 45 | 80 | 62 | 45 | 129 |

DRAWING TITLE: UPPER VERTICAL THRUST BLOCK
DRAWING NUMBER: W15
DRAWN BY: D. JENKINS **APPROVED BY:** G. BEHLEN **REV. DATE:** 01/2015

NOTES:

TO BE USED IN COMMERCIAL OR INDUSTRIAL AREAS WHERE HYDRANTS ARE UNPROTECTED FROM THE MANIFOLD OF TRAFFIC. STEAMER CONNECTION ON FIRE HYDRANT SHOULD FACE THE STREET.

DRAWING TITLE: FIRE HYDRANT GUARDS
DRAWING NUMBER: W20
DRAWN BY: D. JENKINS **APPROVED BY:** G. BEHLEN **REV. DATE:** 01/2009

NOTE: SERVICE TAPS - WATER SERVICE TAPS SHALL BE MADE AT EITHER THE 2 O'CLOCK OR THE 10 O'CLOCK POSITION ON THE CIRCUMFERENCE OF A WATER MAIN. THE MINIMUM DISTANCE BETWEEN A TAP MADE AT THE 2 O'CLOCK POSITION AND THE ONE MADE AT THE 10 O'CLOCK POSITION SHALL BE 18-INCHES MEASURED ALONG THE PIPE. THE MINIMUM DISTANCE BETWEEN SUCCESSIVE TAPS MADE EITHER AT THE 2 O'CLOCK OR THE 10 O'CLOCK POSITION SHALL BE 3- FEET. THE MINIMUM DISTANCE FROM EITHER THE BELL OR THE SPIGOT END OF A PIPE TO A TAP SHALL BE 2- FEET. A MAXIMUM OF 4 WATER SERVICE TAPS SHALL BE ALLOWED PER LENGTH OF PIPE. DOUBLE STRAP SADDLE (ROMAC 202B OR APPROVED EQUAL) SHALL BE USED FOR ALL SERVICE TAPS.

DRAWING TITLE: DOMESTIC WATER TAPPING DETAIL
DRAWING NUMBER: W22
DRAWN BY: D. JENKINS **APPROVED BY:** G. BEHLEN **REV. DATE:** 01/2010

NOTES:

- UTILITY MARKER POST SHALL BE CARBONITE CUM-375 OR EQUAL WITH ANCHORS AND APPROPRIATE DECALS FOR WATER.
- COLOR FOR WATER-BLUE. COLOR FOR NON POTABLE-PURPLE.

DRAWING TITLE: FIBERGLASS MARKER POST
DRAWING NUMBER: W29
DRAWN BY: D. JENKINS **APPROVED BY:** G. BEHLEN **REV. DATE:** 01/2009

NOTES:

WIRE TO BE TAPED ON EACH SIDE OF EVERY JOINT.

DRAWING TITLE: TRACER WIRE
DRAWING NUMBER: W32A (1 OF 2)
DRAWN BY: J. ASCUNCE **APPROVED BY:** R. PENNINGTON **REV. DATE:** 09/2017

**Lot 1
Erie Air Park
Replat D**

3020-3100 Airport Drive
Erie, Colorado 80516

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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Details

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C-702

For
Submittal

TRACER WIRE BOX AT FIRE HYDRANT

TRACER WIRE BOX FOR AREA WITH NO FIRE HYDRANT

DRAWING TITLE: TRACER WIRE
DRAWING NUMBER: W32B (2 OF 2)
DRAWN BY: C. GERATY APPROVED BY: G. BEHLEN DATE: 01/2016

WATER TRENCH DETAIL

DRAWING TITLE: WATER TRENCH DETAIL
DRAWING NUMBER: W35
DRAWN BY: J. ASCUNCE APPROVED BY: R. PENNINGTON DATE: 09/2017

CLAY OR CONCRETE CUT-OFF WALL

DRAWING TITLE: CLAY OR CONCRETE CUT-OFF WALL
DRAWING NUMBER: W37
DRAWN BY: D. JENKINS APPROVED BY: G. BEHLEN DATE: 01/2015

STANDARD VALVE AND BOX

DRAWING TITLE: STANDARD VALVE AND BOX
DRAWING NUMBER: W38
DRAWN BY: D. JENKINS APPROVED BY: G. BEHLEN DATE: 1/2015

STANDARD MANHOLE

DRAWING TITLE: STANDARD MANHOLE
DRAWING NUMBER: STM1A
DRAWN BY: D. JENKINS APPROVED BY: G. BEHLEN DATE: 01/2012

24" MANHOLE RING AND COVER

DRAWING TITLE: 24" MANHOLE RING AND COVER
DRAWING NUMBER: STM1B
DRAWN BY: D. JENKINS APPROVED BY: G. BEHLEN DATE: 1/2015

MANHOLE STEPS

DRAWING TITLE: MANHOLE STEPS
DRAWING NUMBER: STM2
DRAWN BY: D. JENKINS APPROVED BY: G. BEHLEN DATE: 06/2004

TYPE R INLET COVER

DRAWING TITLE: INLET & INLET COVER
DRAWING NUMBER: STM3
DRAWN BY: D. BORRSTERO APPROVED BY: J. SMITH DATE: 11/2019

**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Details

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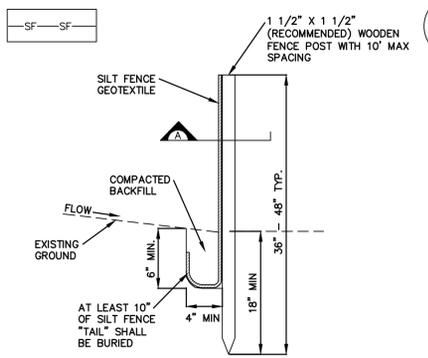
C-703

| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
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| 3 | 12/08/23 | Town Comments |
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

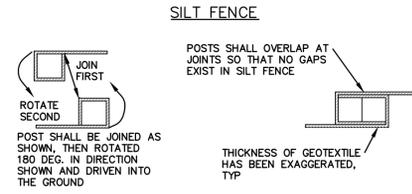
Civil Details

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SF

- SILT FENCE INSTALLATION NOTES**
- SILT FENCE MUST BE PLACED AWAY FROM THE TOE OF THE SLOPE TO ALLOW FOR WATER PONDING. SILT FENCE AT THE TOE OF A SLOPE SHOULD BE INSTALLED IN A FLAT LOCATION AT LEAST SEVERAL FEET (2-5 FT) FROM THE TOE OF THE SLOPE TO ALLOW ROOM FOR PONDING AND DEPOSITION.
 - A UNIFORM 6" X 4" ANCHOR TRENCH SHALL BE EXCAVATED USING TRENCHER OR SILT FENCE INSTALLATION DEVICE. NO ROAD GRADERS, BACKHOES, OR SIMILAR EQUIPMENT SHALL BE USED.
 - COMPACT ANCHOR TRENCH BY HAND WITH A "JUMPING JACK OR BY WHEEL ROLLING. COMPACTION SHALL BE SUCH THAT SILT FENCE RESISTS BEING PULLED OUT OF ANCHOR TRENCH BY HAND.
 - SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD BE NO NOTICEABLE SAG BETWEEN STAKES AFTER IT HAS BEEN ANCHORED TO THE STAKES.
 - SILT FENCE FABRIC SHALL BE ANCHORED TO THE STAKES USING 1" HEAVY DUTY STAPLES OR NAILS WITH 1" HEADS. STAPLES AND NAILS SHOULD BE PLACED 3" ALONG THE FABRIC DOWN THE STAKE.
 - AT THE END OF A RUN OF SILT FENCE ALONG A CONTOUR, THE SILT FENCE SHOULD BE TURNED PERPENDICULAR TO THE CONTOUR TO CREATE A "J-HOOK." THE "J-HOOK" EXTENDING PERPENDICULAR TO THE CONTOUR SHOULD BE OF SUFFICIENT LENGTH TO KEEP RUNOFF FROM FLOWING AROUND THE END OF THE SILT FENCE (TYPICALLY 10' - 20').
 - SILT FENCE SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.

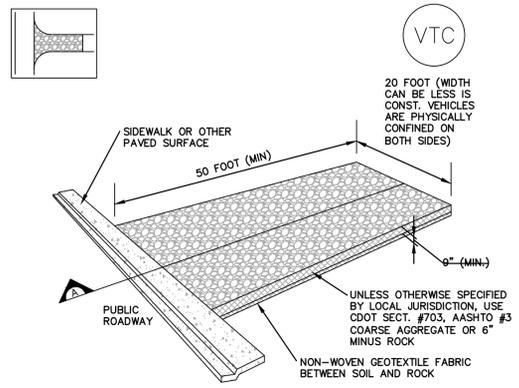


SILT FENCE

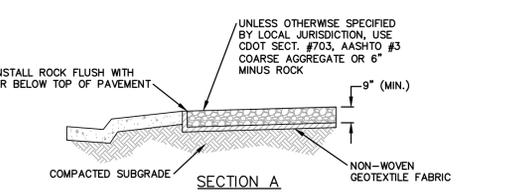
SECTION A

SF-1. SILT FENCE

1 SILT FENCE DETAIL
SCALE: N.T.S.



VTC



VTC-1. AGGREGATE VEHICLE TRACKING CONTROL

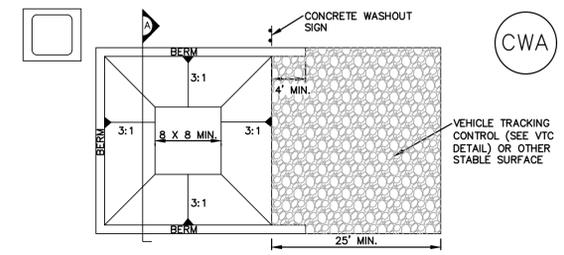
2 VEHICLE TRACKING CONTROL DETAIL
SCALE: N.T.S.

STABILIZED CONSTRUCTION ENTRANCE/EXIT INSTALLATION NOTES

- SEE PLAN VIEW FOR:
 - LOCATION OF CONSTRUCTION ENTRANCE(S)/EXIT(S).
 - TYPE OF CONSTRUCTION ENTRANCE(S)/EXIT(S) (WITH/WITHOUT WHEEL WASH, CONSTRUCTION MAT OR TRM).
- CONSTRUCTION MAT OR TRM STABILIZED CONSTRUCTION ENTRANCES ARE ONLY TO BE USED ON SHORT DURATION PROJECTS (TYPICALLY RANGING FROM A WEEK TO A MONTH) WHERE THERE WILL BE LIMITED VEHICULAR ACCESS.
- A STABILIZED CONSTRUCTION ENTRANCE/EXIT SHALL BE LOCATED AT ALL ACCESS POINTS WHERE VEHICLES ACCESS THE CONSTRUCTION SITE FROM PAVED RIGHT-OF-WAYS.
- STABILIZED CONSTRUCTION ENTRANCE/EXIT SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.
- A NON-WOVEN GEOETEXTILE FABRIC SHALL BE PLACED UNDER THE STABILIZED CONSTRUCTION ENTRANCE/EXIT PRIOR TO THE PLACEMENT OF ROCK.
- UNLESS OTHERWISE SPECIFIED BY LOCAL JURISDICTION, ROCK SHALL CONSIST OF DOT SECT. #703, AASHTO #3 COARSE AGGREGATE OR 6" (MINUS) ROCK.

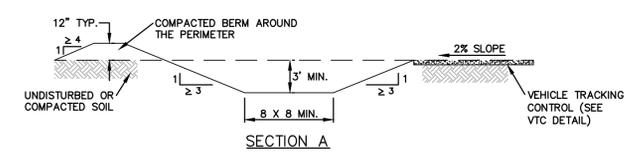
STABILIZED CONSTRUCTION ENTRANCE/EXIT MAINTENANCE NOTES

- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- ROCK SHALL BE REAPPLIED OR REGRADED AS NECESSARY TO THE STABILIZED ENTRANCE/EXIT TO MAINTAIN A CONSISTENT DEPTH.
- SEDIMENT TRACKED ONTO PAVED ROADS IS TO BE REMOVED THROUGHOUT THE DAY AND AT THE END OF THE DAY BY SHOVELING OR SWEEPING. SEDIMENT MAY NOT BE WASHED DOWN STORM SEWER DRAINS.



CWA

CONCRETE WASHOUT AREA PLAN



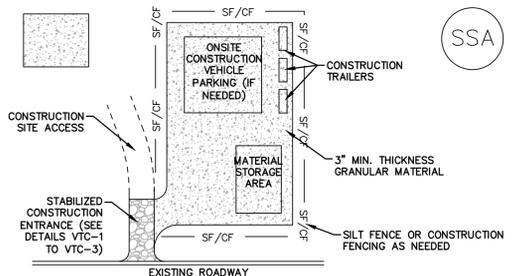
CWA-1. CONCRETE WASHOUT AREA

CWA INSTALLATION NOTES

- SEE PLAN VIEW FOR:
 - CWA INSTALLATION LOCATION.
- DO NOT LOCATE AN UNLINED CWA WITHIN 400' OF ANY NATURAL DRAINAGE PATHWAY OR WATERBODY. DO NOT LOCATE WITHIN 1,000' OF ANY WELLS OR DRINKING WATER SOURCES. IF SITE CONSTRAINTS MAKE THIS INFEASIBLE, OR IF HIGHLY PERMEABLE SOILS EXIST ON SITE, THE CWA MUST BE INSTALLED WITH AN IMPERMEABLE LINER (16 MIL MIN. THICKNESS) OR SURFACE STORAGE ALTERNATIVES USING PREFABRICATED CONCRETE WASHOUT DEVICES OR A LINED ABOVE GROUND STORAGE AREA SHOULD BE USED.
- THE CWA SHALL BE INSTALLED PRIOR TO CONCRETE PLACEMENT ON SITE.
- CWA SHALL INCLUDE A FLAT SUBSURFACE PIT THAT IS AT LEAST 8' BY 8' SLOPES LEADING OUT OF THE SUBSURFACE PIT SHALL BE 3:1 OR FLATTER. THE PIT SHALL BE AT LEAST 3' DEEP.
- BERM SURROUNDING SIDES AND BACK OF THE CWA SHALL HAVE MINIMUM HEIGHT OF 1'.
- VEHICLE TRACKING PAD SHALL BE SLOPED 2% TOWARDS THE CWA.
- SIGNS SHALL BE PLACED AT THE CONSTRUCTION ENTRANCE, AT THE CWA, AND ELSEWHERE AS NECESSARY TO CLEARLY INDICATE THE LOCATION OF THE CWA TO OPERATORS OF CONCRETE TRUCKS AND PUMP RIGS.
- USE EXCAVATED MATERIAL FOR PERIMETER BERM CONSTRUCTION.

CWA MAINTENANCE NOTES

- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- THE CWA SHALL BE REPAIRED, CLEANED, OR ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR CONCRETE WASTE. CONCRETE MATERIALS, ACCUMULATED IN PIT, SHALL BE REMOVED ONCE THE MATERIALS HAVE REACHED A DEPTH OF 2'.
- CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE IN A WATER-TIGHT CONTAINER AND DISPOSED OF PROPERLY.
- THE CWA SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.
- WHEN THE CWA IS REMOVED, COVER THE DISTURBED AREA WITH TOP SOIL, SEED AND MULCH OR OTHERWISE STABILIZED IN A MANNER APPROVED BY THE LOCAL JURISDICTION.



SSA

SSA-1. STABILIZED STAGING AREA

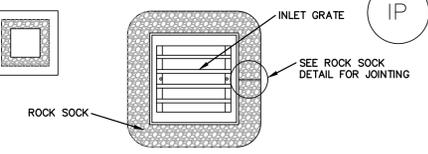
STABILIZED STAGING AREA INSTALLATION NOTES

- SEE PLAN VIEW FOR:
 - LOCATION OF STAGING AREA(S).
 - CONTRACTOR MAY ADJUST LOCATION AND SIZE OF STAGING AREA WITH APPROVAL FROM THE LOCAL JURISDICTION.
- STABILIZED STAGING AREA SHOULD BE APPROPRIATE FOR THE NEEDS OF THE SITE. OVERSIZING RESULTS IN A LARGER AREA TO STABILIZE FOLLOWING CONSTRUCTION.
- STAGING AREA SHALL BE STABILIZED PRIOR TO OTHER OPERATIONS ON THE SITE.
- THE STABILIZED STAGING AREA SHALL CONSIST OF A MINIMUM 3" THICK GRANULAR MATERIAL.
- UNLESS OTHERWISE SPECIFIED BY LOCAL JURISDICTION, ROCK SHALL CONSIST OF DOT SECT. #703, AASHTO #3 COARSE AGGREGATE OR 6" (MINUS) ROCK.
- ADDITIONAL PERIMETER BMPs MAY BE REQUIRED INCLUDING BUT NOT LIMITED TO SILT FENCE AND CONSTRUCTION FENCING.

STABILIZED STAGING AREA MAINTENANCE NOTES

- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- ROCK SHALL BE REAPPLIED OR REGRADED AS NECESSARY IF RUTTING OCCURS OR UNDERLYING SUBGRADE BECOMES EXPOSED.
- STABILIZED STAGING AREA SHALL BE ENLARGED IF NECESSARY TO CONTAIN PARKING, STORAGE, AND UNLOADING/LOADING OPERATIONS.
- THE STABILIZED STAGING AREA SHALL BE REMOVED AT THE END OF CONSTRUCTION. THE GRANULAR MATERIAL SHALL BE REMOVED OR, IF APPROVED BY THE LOCAL JURISDICTION, USED ON SITE, AND THE AREA COVERED WITH TOPSOIL, SEEDED AND MULCHED OR OTHERWISE STABILIZED IN A MANNER APPROVED BY LOCAL JURISDICTION.

4 STABILIZED STAGING AREA
SCALE: N.T.S.

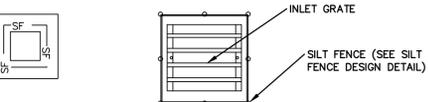


IP

IP-3. ROCK SOCK SUMP/AREA INLET PROTECTION

ROCK SOCK SUMP/AREA INLET PROTECTION INSTALLATION NOTES

- SEE ROCK SOCK DESIGN DETAIL FOR INSTALLATION REQUIREMENTS.
- STRAW WATTLES/SEDIMENT CONTROL LOGS MAY BE USED IN PLACE OF ROCK SOCKS FOR INLETS IN PERVIOUS AREAS. INSTALL PER SEDIMENT CONTROL LOG DETAIL.

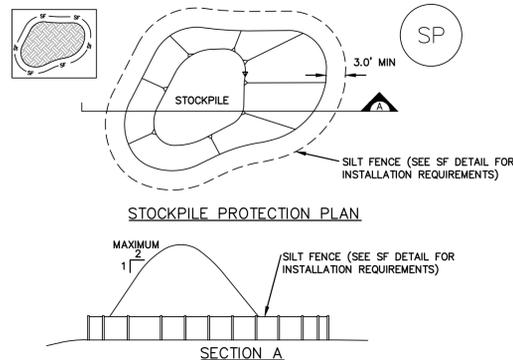


IP-4. SILT FENCE FOR SUMP INLET PROTECTION

SILT FENCE INLET PROTECTION INSTALLATION NOTES

- SEE SILT FENCE DESIGN DETAIL FOR INSTALLATION REQUIREMENTS.
- POSTS SHALL BE PLACED AT EACH CORNER OF THE INLET AND AROUND THE EDGES AT A MAXIMUM SPACING OF 3 FEET.
- STRAW WATTLES/SEDIMENT CONTROL LOGS MAY BE USED IN PLACE OF SILT FENCE FOR INLETS IN PERVIOUS AREAS. INSTALL PER SEDIMENT CONTROL LOG DETAIL.

3 INLET PROTECTION
SCALE: N.T.S.



SP-1. STOCKPILE PROTECTION

STOCKPILE PROTECTION INSTALLATION NOTES

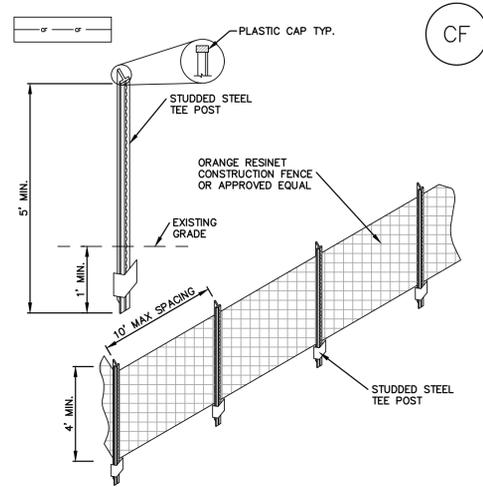
- SEE PLAN VIEW FOR:
 - LOCATION OF STOCKPILES.
 - TYPE OF STOCKPILE PROTECTION.
- INSTALL PERIMETER CONTROLS IN ACCORDANCE WITH THEIR RESPECTIVE DESIGN DETAILS. SILT FENCE IS SHOWN IN THE STOCKPILE PROTECTION DETAILS; HOWEVER, OTHER TYPES OF PERIMETER CONTROLS INCLUDING SEDIMENT CONTROL LOGS OR ROCK SOCKS MAY BE SUITABLE IN SOME CIRCUMSTANCES. CONSIDERATIONS FOR DETERMINING THE APPROPRIATE TYPE OF PERIMETER CONTROL FOR A STOCKPILE INCLUDE WHETHER THE STOCKPILE IS LOCATED ON A PERVIOUS OR IMPERVIOUS SURFACE, THE RELATIVE HEIGHTS OF THE PERIMETER CONTROL AND STOCKPILE, THE ABILITY OF THE PERIMETER CONTROL TO CONTAIN THE STOCKPILE WITHOUT FAILING IN THE EVENT THAT MATERIAL FROM THE STOCKPILE SHIFTS OR SLUMPS AGAINST THE PERIMETER, AND OTHER FACTORS.
- STABILIZE THE STOCKPILE SURFACE WITH SURFACE ROUGHENING, TEMPORARY SEEDING AND MULCHING, EROSION CONTROL BLANKETS, OR SOIL BINDERS. SOILS STOCKPILED FOR AN EXTENDED PERIOD (TYPICALLY FOR MORE THAN 60 DAYS) SHOULD BE SEEDED AND MULCHED WITH A TEMPORARY GRASS COVER ONCE THE STOCKPILE IS PLACED (TYPICALLY WITHIN 14 DAYS). USE OF MULCH ONLY OR A SOIL BINDER IS ACCEPTABLE IF THE STOCKPILE WILL BE IN PLACE FOR A MORE LIMITED TIME PERIOD (TYPICALLY 30-60 DAYS).
- FOR TEMPORARY STOCKPILES ON THE INTERIOR PORTION OF A CONSTRUCTION SITE, WHERE OTHER DOWNGRADE CONTROLS, INCLUDING PERIMETER CONTROL, ARE IN PLACE, STOCKPILE PERIMETER CONTROLS MAY NOT BE REQUIRED.

STOCKPILE PROTECTION MAINTENANCE NOTES

- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- IF PERIMETER PROTECTION MUST BE MOVED TO ACCESS SOIL STOCKPILE, REPLACE PERIMETER CONTROLS BY THE END OF THE WORKDAY.
- STOCKPILE PERIMETER CONTROLS CAN BE REMOVED ONCE ALL THE MATERIAL FROM THE STOCKPILE HAS BEEN USED.

STOCKPILE MANAGEMENT DETAIL

1 SCALE: N.T.S.



CF-1. PLASTIC MESH CONSTRUCTION FENCE

CONSTRUCTION FENCE INSTALLATION NOTES

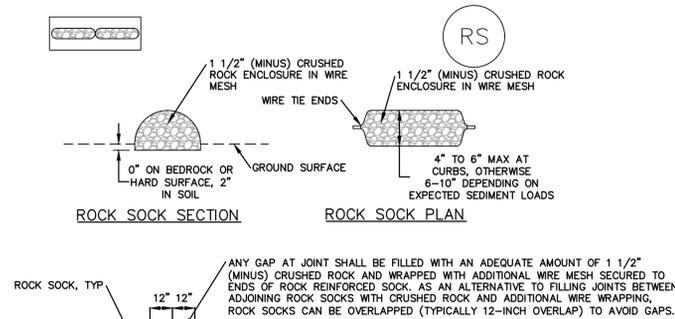
- SEE PLAN VIEW FOR:
 - LOCATION OF CONSTRUCTION FENCE.
- CONSTRUCTION FENCE SHOWN SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.
- CONSTRUCTION FENCE SHALL BE COMPOSED OF ORANGE, CONTRACTOR-GRADE MATERIAL THAT IS AT LEAST 4' HIGH. METAL POSTS SHOULD HAVE A PLASTIC CAP FOR SAFETY.
- STUDDED STEEL TEE POSTS SHALL BE UTILIZED TO SUPPORT THE CONSTRUCTION FENCE. MAXIMUM SPACING FOR STEEL TEE POSTS SHALL BE 10'.
- CONSTRUCTION FENCE SHALL BE SECURELY FASTENED TO THE TOP, MIDDLE, AND BOTTOM OF EACH POST.

CONSTRUCTION FENCE MAINTENANCE NOTES

- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- CONSTRUCTION FENCE SHALL BE REPAIRED OR REPLACED WHEN THERE ARE SIGNS OF DAMAGE SUCH AS RIPS OR SAGS. CONSTRUCTION FENCE IS TO REMAIN IN PLACE UNTIL THE UPSTREAM DISTURBED AREA IS STABILIZED AND APPROVED BY THE LOCAL JURISDICTION.
- WHEN CONSTRUCTION FENCES ARE REMOVED, ALL DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE FENCE SHALL BE COVERED WITH TOPSOIL, SEEDED AND MULCHED, OR OTHERWISE STABILIZED AS APPROVED BY LOCAL JURISDICTION.

CONSTRUCTION FENCE

2 SCALE: N.T.S.



ROCK SOCK PERIMETER CONTROL

ROCK SOCK JOINTING

| GRADATION TABLE | |
|-----------------|---|
| SIEVE SIZE | MASS PERCENT PASSING SQUARE MESH SIEVES |
| NO. 4 | 100 |
| 2" | 90 - 100 |
| 1 1/2" | 20 - 55 |
| 3/4" | 0 - 15 |
| 3/8" | 0 - 5 |

MATCHES SPECIFICATIONS FOR NO. 4 COARSE AGGREGATE FOR CONCRETE PER AASHTO M43. ALL ROCK SHALL BE FRACTURED FACE, ALL SIDES.

ROCK SOCK INSTALLATION NOTES

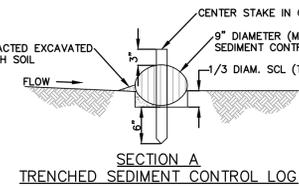
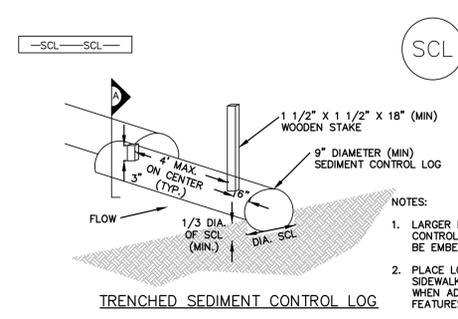
- SEE PLAN VIEW FOR:
 - LOCATION(S) OF ROCK SOCKS.
- CRUSHED ROCK SHALL BE 1 1/2" (MINUS) IN SIZE WITH A FRACTURED FACE (ALL SIDES) AND SHALL COMPLY WITH GRADATION SHOWN ON THIS SHEET (1 1/2" MINUS).
- WIRE MESH SHALL BE FABRICATED OF 10 GAUGE POULTRY MESH, OR EQUIVALENT, WITH A MAXIMUM OPENING OF 1/2". RECOMMENDED MINIMUM ROLL WIDTH OF 48".
- WIRE MESH SHALL BE SECURED USING "HOG RINGS" OR WIRE TIES AT 6" CENTERS ALONG ALL JOINTS AND AT 2" CENTERS ON ENDS OF SOCKS.
- SOME MUNICIPALITIES MAY ALLOW THE USE OF FILTER FABRIC AS AN ALTERNATIVE TO WIRE MESH FOR THE ROCK ENCLOSURE.

ROCK SOCK MAINTENANCE NOTES

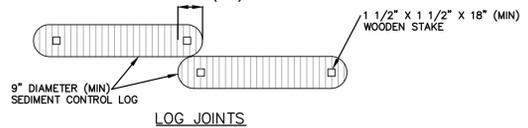
- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED, OR DAMAGED BEYOND REPAIR.
- SEDIMENT ACCUMULATED UPSTREAM OF ROCK SOCKS SHALL BE REMOVED AS NEEDED TO MAINTAIN FUNCTIONALITY OF THE BMP, TYPICALLY WHEN DEPTH OF ACCUMULATED SEDIMENTS IS APPROXIMATELY 1/2 OF THE HEIGHT OF THE ROCK SOCK.
- ROCK SOCKS ARE TO REMAIN IN PLACE UNTIL THE UPSTREAM DISTURBED AREA IS STABILIZED AND APPROVED BY THE LOCAL JURISDICTION.
- WHEN ROCK SOCKS ARE REMOVED, ALL DISTURBED AREAS SHALL BE COVERED WITH TOPSOIL, SEEDED AND MULCHED OR OTHERWISE STABILIZED AS APPROVED BY LOCAL JURISDICTION.

ROCK SOCK DETAIL

3 SCALE: N.T.S.



SECTION A TRENCHED SEDIMENT CONTROL LOG



LOG JOINTS

SCL-1. TRENCHED SEDIMENT CONTROL LOG

SEDIMENT CONTROL LOG INSTALLATION NOTES

- SEE PLAN VIEW FOR LOCATION AND LENGTH OF SEDIMENT CONTROL LOGS.
- SEDIMENT CONTROL LOGS THAT ACT AS A PERIMETER CONTROL SHALL BE INSTALLED PRIOR TO ANY UPGRADE LAND-DISTURBING ACTIVITIES.
- SEDIMENT CONTROL LOGS SHALL CONSIST OF STRAW, COMPOST, EXCELSIOR OR COCONUT FIBER, AND SHALL BE FREE OF ANY NOXIOUS WEED SEEDS OR DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
- SEDIMENT CONTROL LOGS MAY BE USED AS SMALL CHECK DAMS IN DITCHES AND SWALES. HOWEVER, THEY SHOULD NOT BE USED IN PERENNIAL STREAMS.
- IT IS RECOMMENDED THAT SEDIMENT CONTROL LOGS BE TRENCHED INTO THE GROUND TO A DEPTH OF APPROXIMATELY 1/3 OF THE DIAMETER OF THE LOG. IF TRENCHING TO THIS DEPTH IS NOT FEASIBLE AND/OR DESIRABLE (SHORT TERM INSTALLATION WITH DESIRE NOT TO DAMAGE LANDSCAPE) A LESSER TRENCHING DEPTH MAY BE ACCEPTABLE WITH MORE ROBUST STAKING. COMPOST LOGS THAT ARE 8 LB/FT DO NOT NEED TO BE TRENCHED.
- THE UPHILL SIDE OF THE SEDIMENT CONTROL LOG SHALL BE BACKFILLED WITH SOIL OR FILTER MATERIAL THAT IS FREE OF ROCKS AND DEBRIS. THE SOIL SHALL BE TIGHTLY COMPACTED INTO THE SHAPE OF A RIGHT TRIANGLE USING A SHOVEL OR WEIGHTED LAWN ROLLER OR BLOWN IN PLACE.
- FOLLOW MANUFACTURERS' GUIDANCE FOR STAKING. IF MANUFACTURERS' INSTRUCTIONS DO NOT SPECIFY SPACING, STAKES SHALL BE PLACED ON 4' CENTERS AND EMBEDDED A MINIMUM OF 6" INTO THE GROUND. 3" OF THE STAKE SHALL PROTRUDE FROM THE TOP OF THE LOG. STAKES THAT ARE BROKEN PRIOR TO INSTALLATION SHALL BE REPLACED. COMPOST LOGS SHOULD BE STAKED 10' ON CENTER.

SEDIMENT CONTROL LOG MAINTENANCE NOTES

- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- SEDIMENT ACCUMULATED UPSTREAM OF SEDIMENT CONTROL LOG SHALL BE REMOVED AS NEEDED TO MAINTAIN FUNCTIONALITY OF THE BMP, TYPICALLY WHEN DEPTH OF ACCUMULATED SEDIMENTS IS APPROXIMATELY 1/2 OF THE HEIGHT OF THE SEDIMENT CONTROL LOG.
- SEDIMENT CONTROL LOG SHALL BE REMOVED AT THE END OF CONSTRUCTION. COMPOST FROM COMPOST LOGS MAY BE LEFT IN PLACE AS LONG AS BAGS ARE REMOVED AND THE AREA SEEDED. IF DISTURBED AREAS EXIST AFTER REMOVAL, THEY SHALL BE COVERED WITH TOP SOIL, SEEDED AND MULCHED OR OTHERWISE STABILIZED IN A MANNER APPROVED BY THE LOCAL JURISDICTION.

SEDIMENT CONTROL LOG DETAIL

4 SCALE: N.T.S.

For Submittal

Lot 1 Erie Air Park Replat D

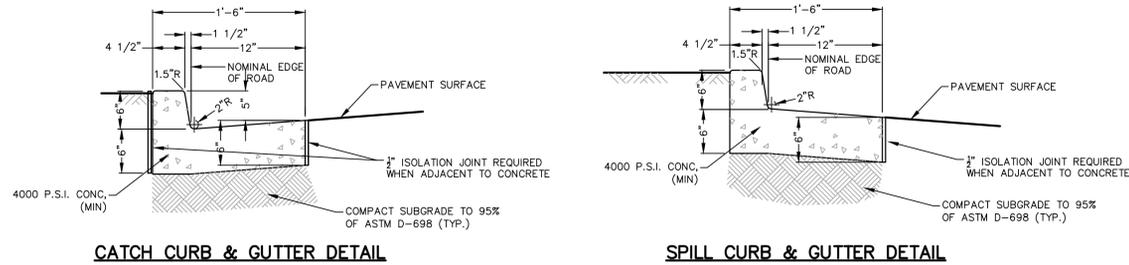
3020-3100 Airport Drive
Erie, Colorado 80516

| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

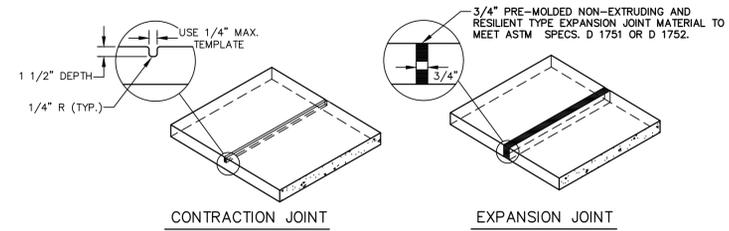
Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Details

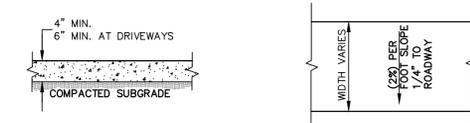
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- NOTE:**
1. ALL EXPOSED CONCRETE SHALL HAVE A BROOM FINISH
 2. EXPANSION JOINTS AT EACH END OF ALL CURB RETURNS AND 500' MAX. INTERVALS - SEE CURB & GUTTER EXPANSION JOINT DETAIL.
 3. CONTRACTION JOINTS AT 10' MAX. INTERVALS - SEE CURB & GUTTER CONTRACTION JOINT DETAIL



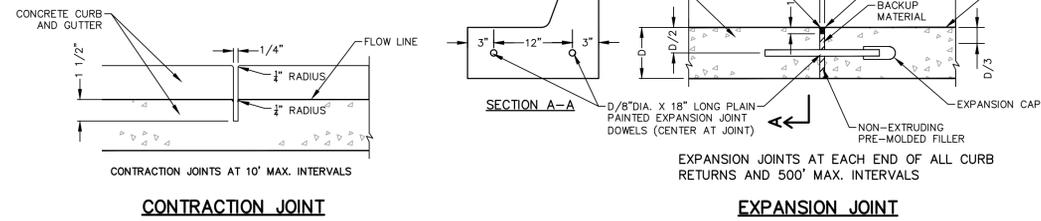
CONTRACTION JOINT EXPANSION JOINT



NOTE: ALL EXPOSED CONCRETE SHALL HAVE A BROOM FINISH. EXPANSION JOINTS AT ALL INTERSECTIONS WITH EXISTING CURB AND GUTTER, SIDEWALK, AT ALL RADIUS POINTS, AND AT 500 FT. MAX. INTERVALS. CONTRACTION JOINTS AT 5 FT. INTERVALS.

CONCRETE SIDEWALK DETAIL

SCALE: N.T.S.

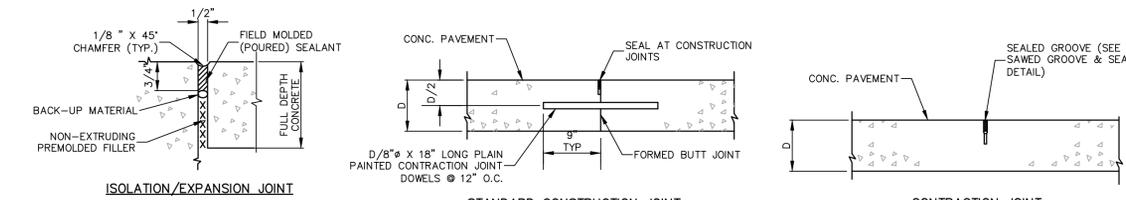


ISOLATION JOINTS WHERE CURB ABUTS STRUCTURES

CURB ISOLATION JOINT

CURB AND GUTTER DETAIL

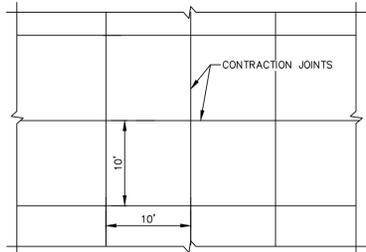
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ISOLATION/EXPANSION JOINT

STANDARD CONSTRUCTION JOINT

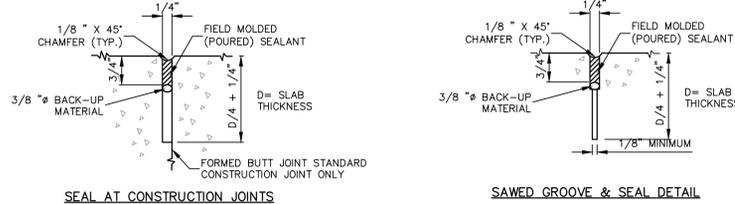
CONTRACTION JOINT



JOINT LAYOUT PLAN

- NOTE:**
1. JOINT LAYOUT PLAN REPRESENTS THE MAXIMUM JOINT SEPARATION. CONTRACTOR SHALL SUBMIT FINAL JOINT LAYOUT PLAN TO OWNER FOR APPROVAL PRIOR TO CONSTRUCTION.
 2. PROVIDE ISOLATION JOINT WHERE CONCRETE ABUT STRUCTURES.

NOTE:
1. CONSTRUCTION JOINTS SHALL BE USED WHEN OPERATIONS ARE SUSPENDED FOR 30 MINUTES OR LONGER. JOINTS SHALL ALIGN WITH CONTRACTION JOINT LAYOUT.



SEAL AT CONSTRUCTION JOINTS

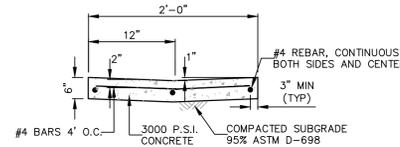
SAWED GROOVE & SEAL DETAIL

TYPICAL CONCRETE PAVEMENT JOINTS

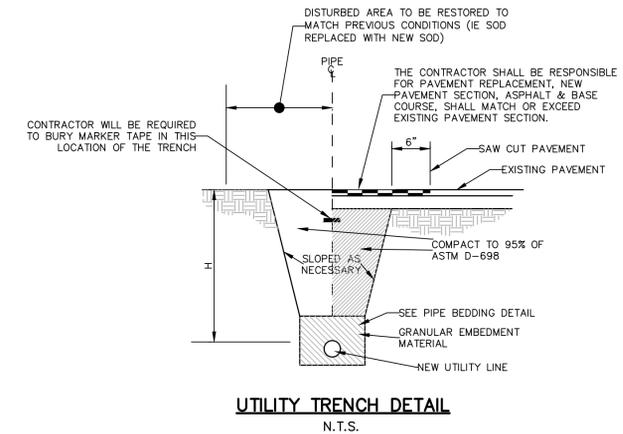
SCALE: N.T.S.

PIPE BOLLARD - 4"

SCALE: N.T.S.



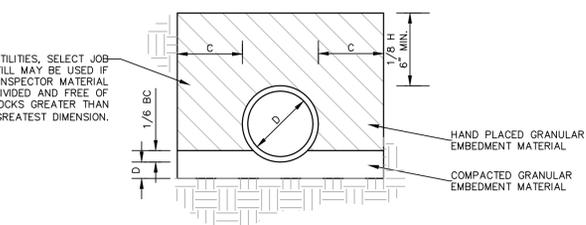
- NOTE:**
1. SEALED CONTRACTION JOINT REQUIRED EVERY 10 LINEAR FEET
 2. SEALED EXPANSION JOINT REQUIRED EVERY 100 LINEAR FEET
- 2" DRAINAGE PAN**
N.T.S.



UTILITY TRENCH DETAIL

N.T.S.

FOR RCP & DIP UTILITIES, SELECT JOB EXCAVATED BACKFILL MAY BE USED IF AUTHORIZED BY THE INSPECTOR MATERIAL MUST BE FINELY DIVIDED AND FREE OF ANY CLOD OR ROCKS GREATER THAN 3/4" IN GREATEST DIMENSION.



TRENCH WIDTH SPECIFICATIONS

| D | C | |
|--------------|------|------|
| | MIN. | MAX. |
| 0 - 18" | 6" | 12" |
| 20 - 28" | 9" | 12" |
| 30" & LARGER | 12" | 18" |

LEGEND

Bc = OUTSIDE DIAMETER
H = BACKFILL COVER ABOVE TOP OF PIPE
D = INSIDE DIAMETER
d = DEPTH OF BEDDING MATERIAL BELOW PIPE

DEPTH OF BEDDING MATERIAL BELOW PIPE

| D | IN SOIL | | IN ROCK | |
|---------------|----------|----------|----------|----------|
| | d (MIN.) | d (MIN.) | d (MIN.) | d (MIN.) |
| 27" & SMALLER | 3" | 6" | | |
| 30" TO 60" | 4" | 9" | | |
| 66" & LARGER | 6" | 12" | | |

PIPE BEDDING DETAIL

N.T.S.

| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Details

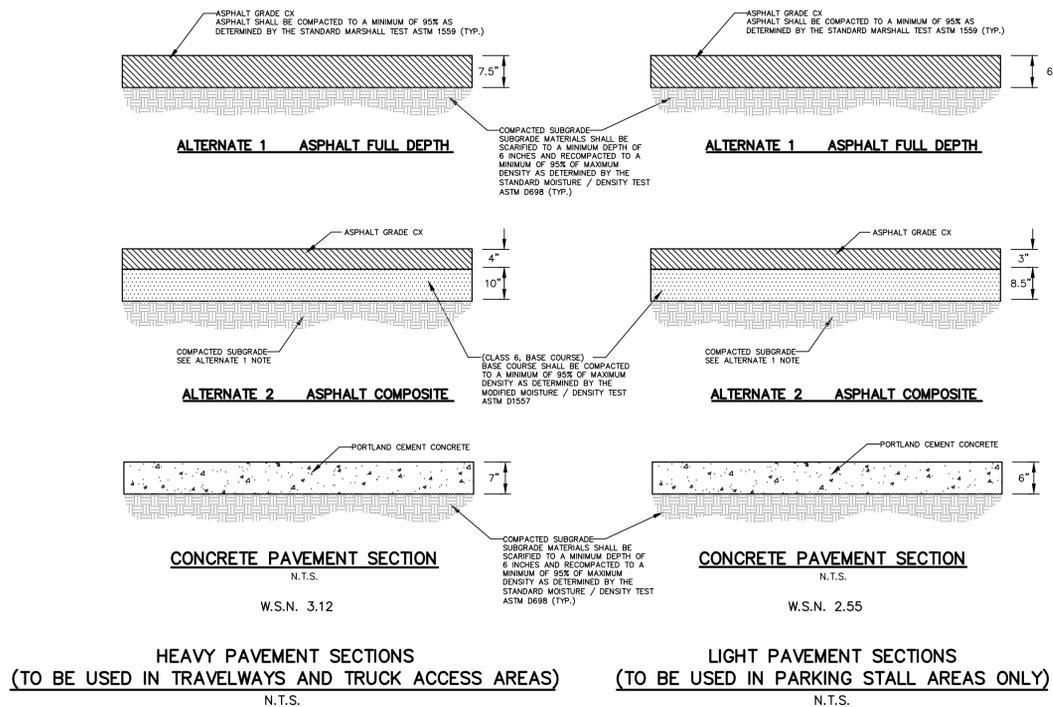
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PAVEMENT DESIGN ALTERNATIVES

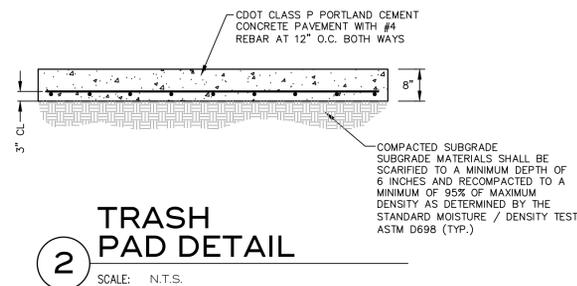
THE DESIGN PAVEMENT SECTIONS PROVIDED HEREIN ARE DESIGNED TO MEET A SPECIFIC WSN (WEIGHTED STRUCTURAL NUMBER), AS THE STRENGTH COEFFICIENTS OF THE ACTUAL MATERIALS TO BE USED IN THE CONSTRUCTION MAY BE DIFFERENT FROM THE ASSUMED STRENGTH COEFFICIENTS. ALTERNATE SECTIONS WHICH PROVIDE THE REQUIRED WSN WILL BE CONSIDERED. THE CONTRACTOR SHALL SUBMIT ALTERNATE DESIGN SECTIONS ALONG WITH DOCUMENTATION OF ACTUAL STRENGTH COEFFICIENTS TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.

NOTES: (APPLIES TO ALL SHEETS)

1. ALL ASPHALT PLACED SHALL BE COMPACTED TO A MINIMUM OF 95% AS DETERMINED BY THE MARSHALL TEST ASTM D1559 (TYP.)
2. MAX. ASPHALT LIFT THICKNESS = 4 INCHES
3. MIN. ASPHALT LIFT THICKNESS = 1 1/2 INCHES
4. ALL ASPHALT PAVING SHALL BE DONE IN ACCORDANCE WITH CDOT "SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".



1 PAVEMENT SECTIONS
 SCALE: N.T.S.



For
 Submittal

**Lot 1
 Erie Air Park
 Replat D**
 3020-3100 Airport Drive
 Erie, Colorado 80516

| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
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Civil Details

Sheet

STEEL GRATE QUANTITIES

| NO. | DESCRIPTION | LENGTH (L.F.) | WEIGHT (LBS.) |
|-------|--------------------|---------------|---------------|
| 1 | 54 x 7.7 BEAM | 42' | 7.96 |
| 2 | 3 1/2" x 1/2" FLAT | 285' | 2.38 |
| 3 | 2" x 1/2" FLAT | 285' | 1.33 |
| TOTAL | | | 11.67 |

QUANTITIES FOR ONE INLET

| H | CONCRETE (CU. YDS.) | STEEL (LBS.) | NO. OF STEPS |
|--------|---------------------|--------------|--------------|
| 2'-0" | 1.0 | 76 | 0 |
| 3'-0" | 2.1 | 81 | 1 |
| 4'-0" | 3.3 | 87 | 2 |
| 5'-0" | 4.5 | 93 | 3 |
| 6'-0" | 5.7 | 100 | 4 |
| 7'-0" | 7.0 | 107 | 5 |
| 8'-0" | 8.2 | 114 | 6 |
| 9'-0" | 9.5 | 121 | 7 |
| 10'-0" | 10.7 | 128 | 8 |

GENERAL NOTES

- INLET TYPE C IS NOT 40-20 RATED AND SHALL NOT BE PLACED IN PAVED ROADWAYS. THIS INLET SHALL BE USED ONLY OUTSIDE PAVED ROADWAYS.
- CONCRETE SHALL BE CLASS B. INLET MAY BE CAST-IN-PLACE OR PRECAST.
- REINFORCING BARS SHALL BE GRADE 60, EPOXY COATED, AND DEFORMED #4 AND SHALL HAVE A MIN. 2 INCH CLEARANCE CUT OR BEND AROUND PIPES AS REQUIRED.
- CONCRETE SLOPE AND DITCH PAVING SHALL BE IN ACCORDANCE WITH SECTION 507. REINFORCEMENT FOR CONCRETE SLOPE PAVING SHALL BE #4 @ 6" X 6" W/ 4" OR #4 @ 8" X 8" W/ 4" WEL.
- STRUCTURAL STEEL FOR GRATES AND GRATE INSTALLATION HARDWARE SHALL BE GALVANIZED AND SHALL BE IN ACCORDANCE WITH SUBSECTION 712.56.
- THE STANDARD INLET GRATES SHALL BE USED ON ALL TYPE C INLETS UNLESS CLOSE MESH INLET GRATES ARE SPECIFIED ON THE PLANS.
- CLOSE MESH GRATES ARE RECOMMENDED WHERE FEET TRAFFIC OR BICYCLE ROUTES ARE IN CLOSE PROXIMITY TO GRATE. THIS GRATE IS NOT ADA COMPLIANT OR BICYCLE FRIENDLY AND SHALL NOT BE PLACED DIRECTLY IN SIDEWALKS, CROSSWALKS OR SIDE PATHS.
- STEPS SHALL BE PROVIDED WHEN INLET DIMENSION "H" IS EQUAL TO OR GREATER THAN 3 FEET - 6 INCHES AND SHALL CONFORM TO AASHTO M 194.
- SEE STANDARD PLAN M-604-13 FOR REINFORCEMENT AROUND THE PIPE OPENING.
- ALL INLETS SHALL HAVE A 4 INCH DIA. METAL MESH WITH A "NO DUMPING GRANTS TO STREAM" MESSAGE ON IT. THE MESH SHALL HAVE A FISH SYMBOL WITH A BLUE BACKGROUND. IT SHALL BE FIRMLY ATTACHED TO THE TOP OF THE INLET WITH A PERMANENT FASTENER.

BAR LIST FOR H = 2 FT. - 6 IN. AND BENDING DIAGRAM

| MARK | NO. | REINFORCING | LENGTH |
|---------|-----|----------------|--------|
| NO. 402 | 2 | 2-3/4" #4-2" | 8'-0" |
| NO. 401 | 6 | 2-3/4" #4-8" | 8'-0" |
| NO. 402 | 4 | 1/2" #4-12'-4" | 12'-4" |

INLET, TYPE C

STANDARD PLAN NO. M-604-10
Standard Sheet No. 1 of 1

STEEL GRATE QUANTITIES

| NO. | DESCRIPTION | LENGTH (L.F.) | WEIGHT (LBS.) |
|-------|--------------------|---------------|---------------|
| 1 | 54 x 7.7 BEAM | 42' | 7.96 |
| 2 | 3 1/2" x 1/2" FLAT | 285' | 2.38 |
| 3 | 2" x 1/2" FLAT | 285' | 1.33 |
| TOTAL | | | 11.67 |

QUANTITIES FOR ONE INLET

| H | CONCRETE (CU. YDS.) | STEEL (LBS.) | NO. OF STEPS |
|--------|---------------------|--------------|--------------|
| 2'-0" | 1.0 | 76 | 0 |
| 3'-0" | 2.1 | 81 | 1 |
| 4'-0" | 3.3 | 87 | 2 |
| 5'-0" | 4.5 | 93 | 3 |
| 6'-0" | 5.7 | 100 | 4 |
| 7'-0" | 7.0 | 107 | 5 |
| 8'-0" | 8.2 | 114 | 6 |
| 9'-0" | 9.5 | 121 | 7 |
| 10'-0" | 10.7 | 128 | 8 |

GENERAL NOTES

- CONCRETE SHALL BE CLASS B. INLET MAY BE CAST-IN-PLACE OR PRECAST.
- CAST-IN-PLACE CONCRETE WALLS SHALL BE FORMED ON BOTH SIDES.
- EXPOSED CONCRETE CORNERS SHALL BE CHAMFERED 1/4" OF A INCH.
- REINFORCING BARS SHALL BE DEFORMED #4 AND SHALL HAVE A 2 INCH MINIMUM CLEARANCE. ALL REINFORCING BARS SHALL BE GRADE 60 AND EPOXY COATED.
- STEPS SHALL BE PROVIDED WHEN INLET DIMENSION "H" IS EQUAL TO OR GREATER THAN 3 FEET - 6 INCHES AND SHALL CONFORM TO AASHTO M 194.
- ALL GRATES AND FRAMES SHALL BE GRAY OR DUCTILE CAST IRON IN ACCORDANCE WITH SUBSECTION 712.06. GRATES AND FRAMES SHALL BE DESIGNED TO WITHSTAND 16.50 LOADS.
- STATION POINT IS AT THE CENTER OF THE INLET.
- GRATE SHALL HAVE "NO DUMP WASTE GRANTS TO STREAM" MESSAGE CAST ON SURFACE.

NO. 13 GRATING & FRAMES

PLAN VIEW
SECTION A-A
SECTION B-B
SECTION C-C
SECTION D-D
SECTION E-E
SECTION F-F

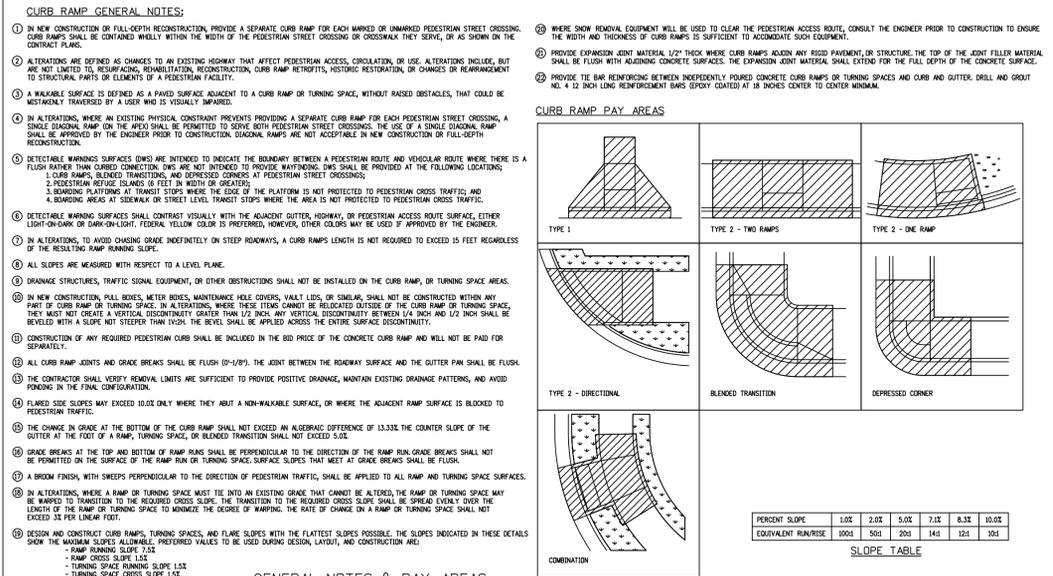
QUANTITIES FOR ONE INLET

| MARK | NO. | DIMENSIONS | LENGTH |
|---------|-----|-------------------|-----------|
| NO. 402 | 4 | 3'-0" x 2'-2" | 13'-4" |
| NO. 402 | 2 | 3'-0" x 2'-4 1/2" | 8'-0 1/2" |
| NO. 402 | 5 | 2'-1/2" x 2'-2" | 7'-0 1/2" |

ADD 6 IN. TO THIS DIMENSION FOR EACH 6 IN. INCREASE OF "H" OVER 3 FT. - 0 IN.
BAR LIST FOR H = 3 FT. - 0 IN.

CONCRETE INLET TYPE 13

STANDARD PLAN NO. M-604-13
Standard Sheet No. 1 of 1

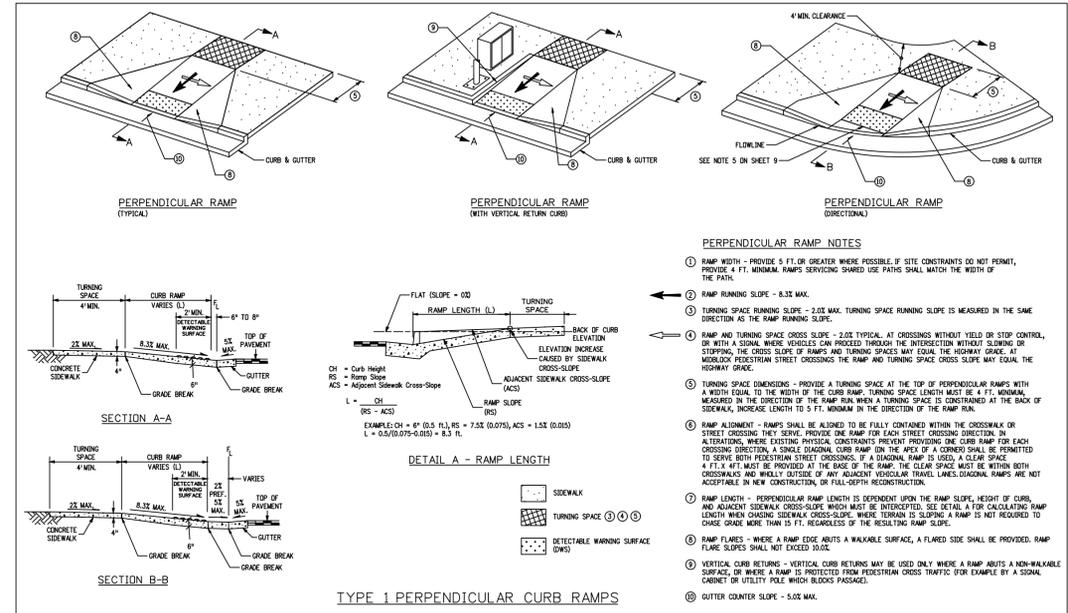


GENERAL NOTES & PAY AREAS

| Computer File Information | Sheet Revisions | Colorado Department of Transportation | STANDARD PLAN NO. |
|---|-----------------|---------------------------------------|----------------------------|
| Creation Date: 07/31/19 | Date: | 2829 West Howard Place | M-608-1 |
| Designer Initials: JBK | Comments: | COOT HQ, 3rd Floor | Standard Sheet No. 1 of 10 |
| Last Modification Date: 07/31/19 | | Denver, CO 80204 | Project Sheet Number: |
| Detailer Initials: LTA | | Phone: 303-757-9021 FAX: 303-757-9868 | |
| CAD Ver: MicroStation V8 Scale: Not to Scale Units: English | | Project Development Branch | |

SLOPE TABLE

| PERCENT SLOPE | 1:00 | 2:00 | 5:00 | 7:13 | 8:33 | 10:00 |
|---------------------|-------|------|------|------|------|-------|
| EQUIVALENT RUN/RISE | 100/1 | 50/1 | 20/1 | 14/1 | 12/1 | 10/1 |



PERPENDICULAR RAMP NOTES:

- RAMP WIDTH - PROVIDE 5 FT. OR GREATER WHERE POSSIBLE. IF SITE CONSTRAINTS DO NOT PERMIT, PROVIDE 4 FT. MINIMUM. RAMP SERVICES SHARED USE PATHS SHALL MATCH THE WIDTH OF THE PATH.
- RAMP RUNNING SLOPE - 8:33 MAX.
- TURNING SPACE RUNNING SLOPE - 2:00 MAX. TURNING SPACE RUNNING SLOPE IS MEASURED IN THE SAME DIRECTION AS THE RAMP RUNNING SLOPE.
- RAMP AND TURNING SPACE CROSS SLOPE - 2:00 TYPICAL. AT CROSSINGS WITHOUT YIELD OR STOP CONTROL, OR WITH A SIGNAL, WHERE VEHICLES CAN PROCEED THROUGH THE INTERSECTION WITHOUT SLOWING OR STOPPING, THE CROSS SLOPE OF RAMP AND TURNING SPACES MAY EQUAL THE HIGHWAY GRADE. AT MIDBLOCK PEDESTRIAN STREET CROSSINGS THE RAMP AND TURNING SPACE CROSS SLOPE MAY EQUAL THE HIGHWAY GRADE.
- TURNING SPACE DIMENSIONS - PROVIDE A TURNING SPACE AT THE TOP OF PERPENDICULAR RAMP WITH A WIDTH EQUAL TO THE WIDTH OF THE CURB RAMP. TURNING SPACE LENGTH MUST BE 4 FT. MINIMUM, MEASURED IN THE DIRECTION OF THE RAMP RUN WHEN A TURNING SPACE IS CONSTRAINED AT THE BACK OF SIDEWALK, INCREASE LENGTH TO 5 FT. MINIMUM IN THE DIRECTION OF THE RAMP RUN.
- RAMP ALIGNMENT - RAMP SHALL BE ALIGNED TO BE FULLY CONTAINED WITHIN THE CROSSWALK OR STREET CROSSING THEY SERVE. PROVIDE ONE RAMP FOR EACH STREET CROSSING DIRECTION. IN ALTERATIONS, WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT PROVIDING ONE CURB RAMP FOR EACH CROSSING DIRECTION, A SINGLE DIAGONAL RAMP ON THE AREA OF A CORNER SHALL BE PERMITTED TO SERVE BOTH PEDESTRIAN STREET CROSSINGS, IF A DIAGONAL RAMP IS USED, A CLEAR SPACE 4 FT. x 4 FT. MUST BE PROVIDED AT THE BASE OF THE RAMP. THE CLEAR SPACE MUST BE WITHIN BOTH CROSSWALKS AND FULLY OUTSIDE OF ANY ADJACENT VEHICULAR TRAVEL LANES. DIAGONAL RAMP IS NOT ACCEPTABLE IN NEW CONSTRUCTION OR FULL-DEPTH RECONSTRUCTION.
- RAMP LENGTH - PERPENDICULAR RAMP LENGTH IS DEPENDENT UPON THE RAMP SLOPE, WIDTH OF CURB, AND ADJACENT SIDEWALK CROSS-SLOPE WHICH MUST BE INTERCEPTED. SEE DETAIL A FOR CALCULATING RAMP LENGTH WHEN CHANGING SIDEWALK CROSS-SLOPE. WHERE TERRAIN IS SLOPING, A RAMP IS NOT REQUIRED TO CHASE GRADE MORE THAN 15 FT. REGARDLESS OF THE RESULTING RAMP SLOPE.
- RAMP FLARES - WHERE A RAMP EDGE ABUTS A WALKABLE SURFACE, A FLARED SIDE SHALL BE PROVIDED. RAMP FLARE SLOPES SHALL NOT EXCEED 10:00.
- VERTICAL CURB RESTRICTION - VERTICAL CURB RESTRICTIONS MAY BE USED ONLY WHERE A RAMP ABUTS A NON-WALKABLE SURFACE, OR WHERE A RAMP IS PROTECTED FROM PEDESTRIAN CROSS TRAFFIC (FOR EXAMPLE BY A SIGNAL, CABINET OR UTILITY POLE WHICH BLOCKS PASSAGE).
- CUTTER COUNTER SLOPE - 5:00 MAX.

Computer File Information

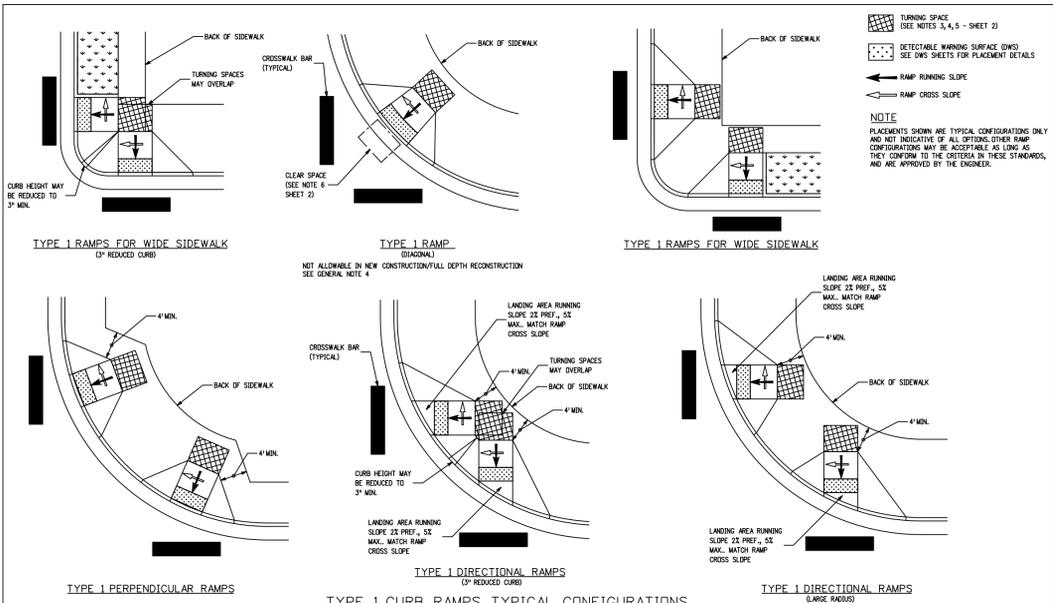
Sheet Revisions

Colorado Department of Transportation

CURB RAMP

STANDARD PLAN NO. M-608-1

Standard Sheet No. 2 of 10



Computer File Information

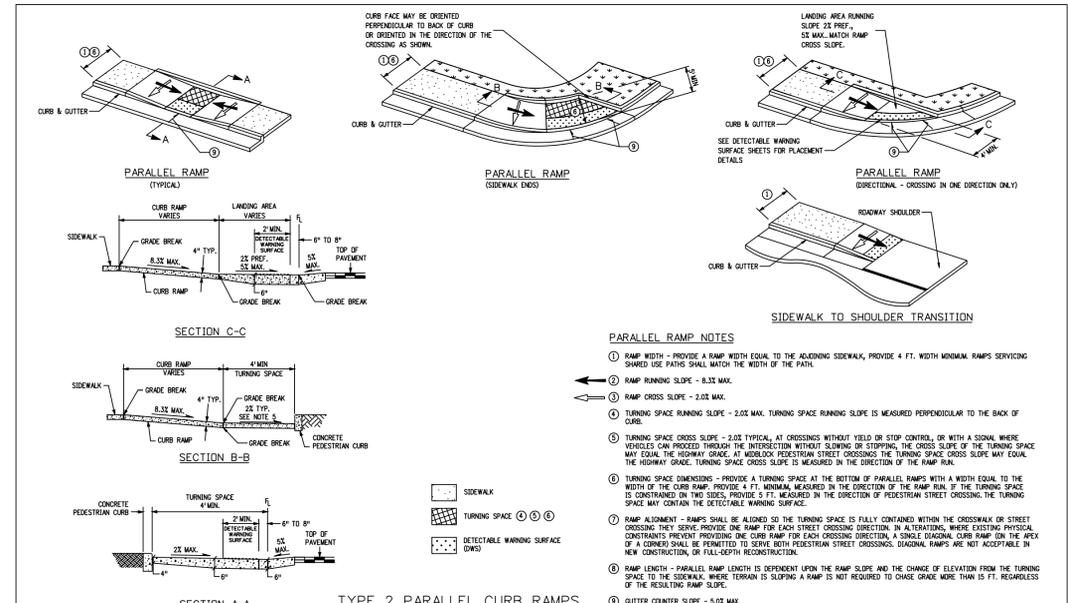
Sheet Revisions

Colorado Department of Transportation

CURB RAMP

STANDARD PLAN NO. M-608-1

Standard Sheet No. 3 of 10



PARALLEL RAMP NOTES:

- RAMP WIDTH - PROVIDE A RAMP WIDTH EQUAL TO THE ADJACENT SIDEWALK, PROVIDE 4 FT. WIDTH MINIMUM. RAMP SERVICES SHARED USE PATHS SHALL MATCH THE WIDTH OF THE PATH.
- RAMP RUNNING SLOPE - 8:33 MAX.
- RAMP CROSS SLOPE - 2:00 MAX.
- TURNING SPACE RUNNING SLOPE - 2:00 MAX. TURNING SPACE RUNNING SLOPE IS MEASURED PERPENDICULAR TO THE BACK OF CURB.
- TURNING SPACE CROSS SLOPE - 2:00 TYPICAL. AT CROSSINGS WITHOUT YIELD OR STOP CONTROL, OR WITH A SIGNAL, WHERE VEHICLES CAN PROCEED THROUGH THE INTERSECTION WITHOUT SLOWING OR STOPPING, THE CROSS SLOPE OF THE TURNING SPACE MAY EQUAL THE HIGHWAY GRADE. AT MIDBLOCK PEDESTRIAN STREET CROSSINGS THE TURNING SPACE CROSS SLOPE MAY EQUAL THE HIGHWAY GRADE. TURNING SPACE CROSS SLOPE IS MEASURED IN THE DIRECTION OF THE RAMP RUN.
- TURNING SPACE DIMENSIONS - PROVIDE A TURNING SPACE AT THE BOTTOM OF PARALLEL RAMP WITH A WIDTH EQUAL TO THE WIDTH OF THE CURB RAMP. PROVIDE 4 FT. MINIMUM MEASURED IN THE DIRECTION OF THE RAMP RUN. IF THE TURNING SPACE IS CONSTRAINED ON TWO SIDES, PROVIDE 5 FT. MEASURED IN THE DIRECTION OF PEDESTRIAN STREET CROSSING. THE TURNING SPACE MAY CONTAIN THE DETECTABLE WARNING SURFACE.
- RAMP ALIGNMENT - RAMP SHALL BE ALIGNED SO THE TURNING SPACE IS FULLY CONTAINED WITHIN THE CROSSWALK OR STREET CROSSING THEY SERVE. PROVIDE ONE RAMP FOR EACH STREET CROSSING DIRECTION. IN ALTERATIONS, WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT PROVIDING ONE CURB RAMP FOR EACH CROSSING DIRECTION, A SINGLE DIAGONAL CURB RAMP ON THE AREA OF A CORNER SHALL BE PERMITTED TO SERVE BOTH PEDESTRIAN STREET CROSSINGS. DIAGONAL RAMP IS NOT ACCEPTABLE IN NEW CONSTRUCTION OR FULL-DEPTH RECONSTRUCTION.
- RAMP LENGTH - PARALLEL RAMP LENGTH IS DEPENDENT UPON THE RAMP SLOPE AND THE CHANGE OF ELEVATION FROM THE TURNING SPACE TO THE SIDEWALK. WHERE TERRAIN IS SLOPING, A RAMP IS NOT REQUIRED TO CHASE GRADE MORE THAN 15 FT. REGARDLESS OF THE RESULTING RAMP SLOPE.
- CUTTER COUNTER SLOPE - 5:00 MAX.

Computer File Information

Sheet Revisions

Colorado Department of Transportation

CURB RAMP

STANDARD PLAN NO. M-608-1

Standard Sheet No. 4 of 10

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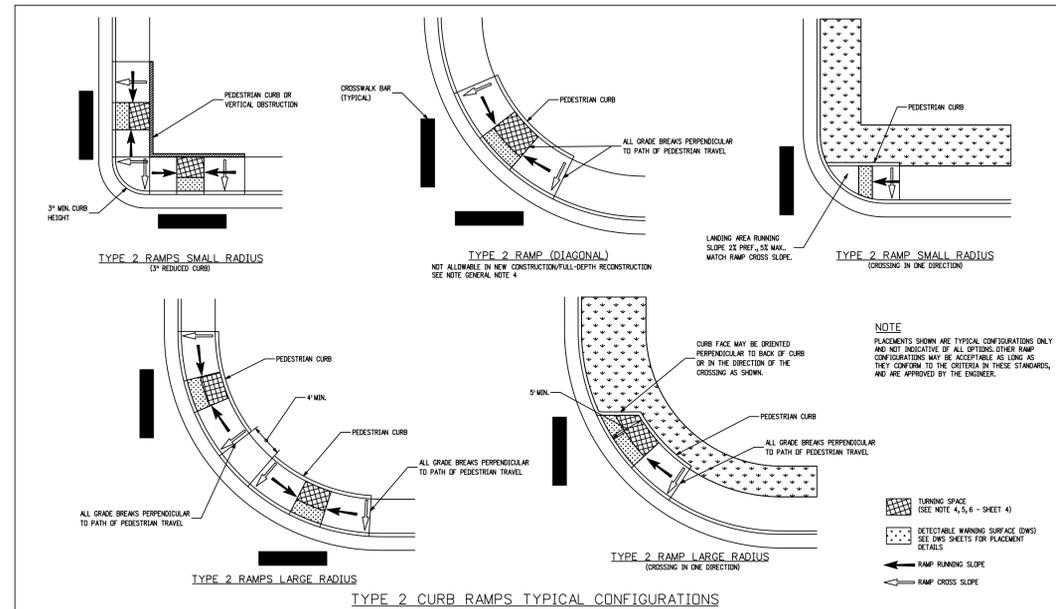
**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

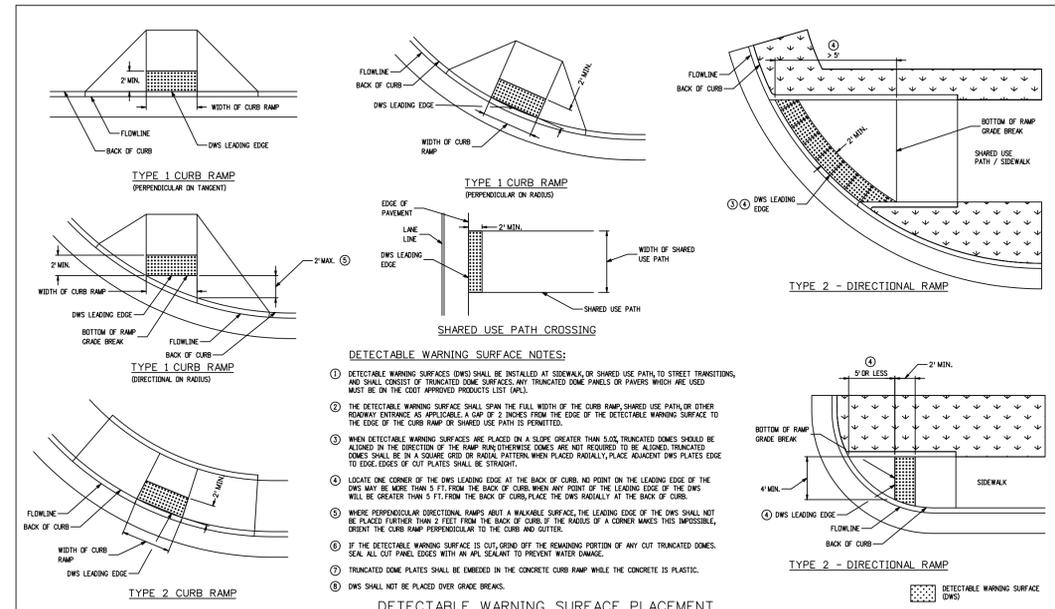
Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Details

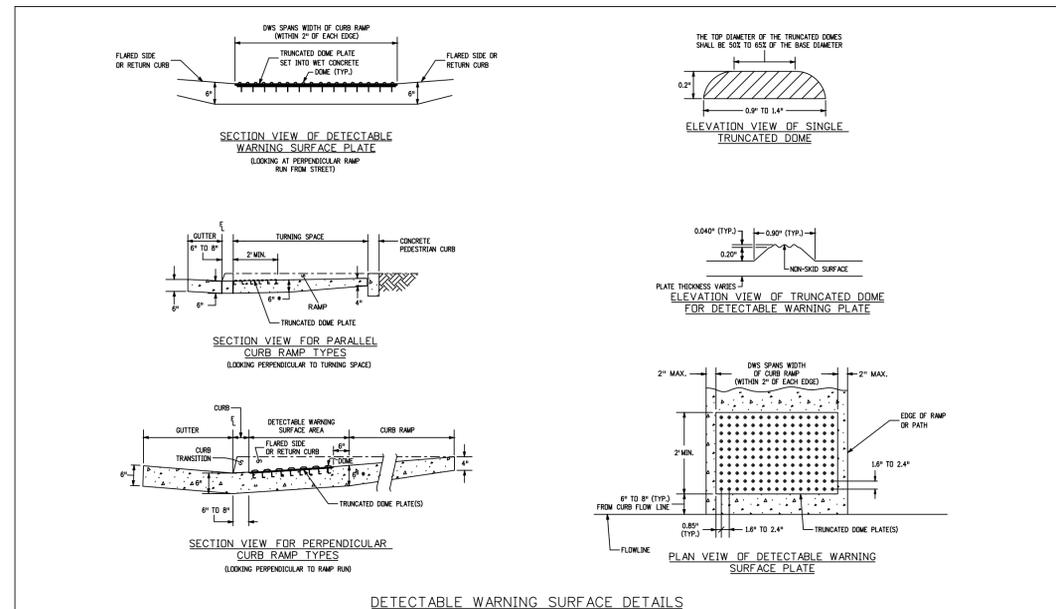
For
Submittal



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| Computer File Information | | Sheet Revisions | | Colorado Department of Transportation | | STANDARD PLAN NO. | |
| Creation Date: 07/31/19 | | Date: | Comments: | 2829 West Howard Place | | M-608-1 | |
| Designer Initials: JBK | (E3C) | | | COOT Hq, 3rd Floor | | Standard Sheet No. 5 of 10 | |
| Last Modification Date: 07/31/19 | (E3C) | | | Denver, CO 80204 | | Project Sheet Number: | |
| Detailer Initials: LTA | (E3C) | | | Phone: 303-757-9021 FAX: 303-757-9868 | | | |
| CAD Ver.: MicroStation V8 | Scale: Not to Scale | Units: English | (E3C) | Project Development Branch | JBK | | |



| | | | | | | | |
|----------------------------------|---------------------|-----------------|-----------|---------------------------------------|-----|----------------------------|--|
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| Designer Initials: JBK | (E3C) | | | COOT Hq, 3rd Floor | | Standard Sheet No. 9 of 10 | |
| Last Modification Date: 07/31/19 | (E3C) | | | Denver, CO 80204 | | Project Sheet Number: | |
| Detailer Initials: LTA | (E3C) | | | Phone: 303-757-9021 FAX: 303-757-9868 | | | |
| CAD Ver.: MicroStation V8 | Scale: Not to Scale | Units: English | (E3C) | Project Development Branch | JBK | | |



| | | | | | | | |
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| Designer Initials: JBK | (E3C) | | | COOT Hq, 3rd Floor | | Standard Sheet No. 10 of 10 | |
| Last Modification Date: 07/31/19 | (E3C) | | | Denver, CO 80204 | | Project Sheet Number: | |
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**Lot 1
Erie Air Park
Replat D**
 3020-3100 Airport Drive
 Erie, Colorado 80516

| # | Date | Description |
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| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

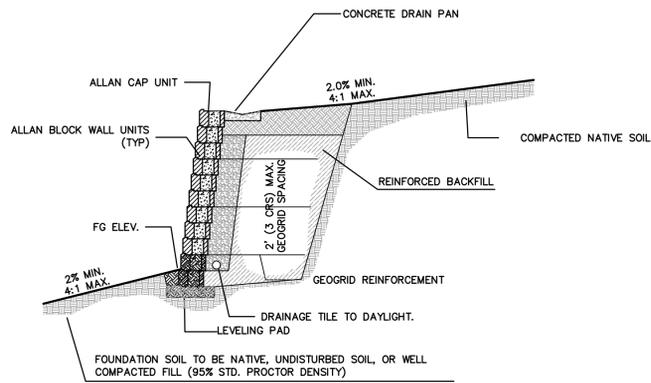
Project No: 23126A
 By: JAS/DPA
 File: 23126A-1

Civil Details

Sheet

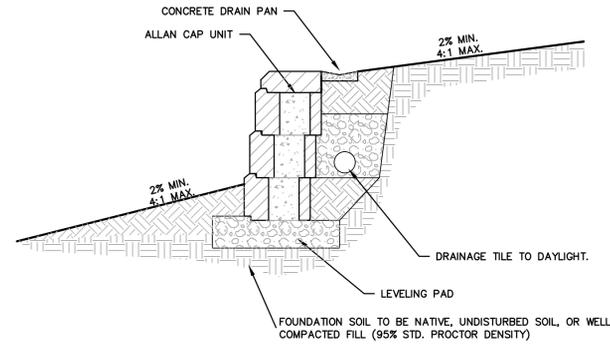
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For
Submittal

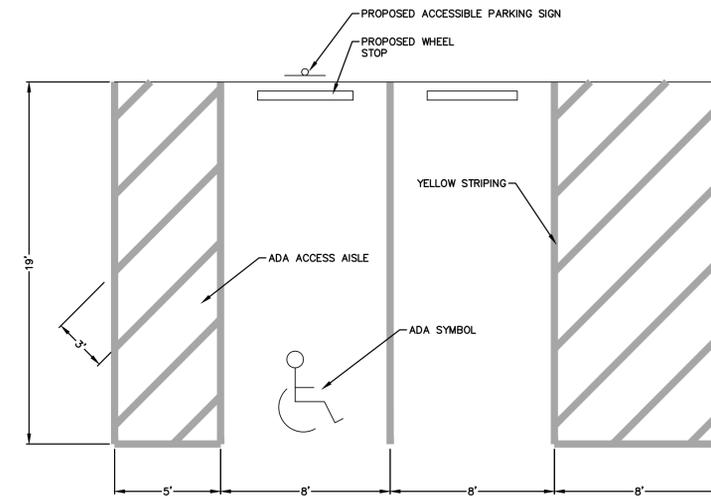


*ALL WALL UNITS SHALL BE ALLAN CLASSIC (ALLAN BLOCK) UNITS AS PROVIDED BY BASALITE CONCRETE PRODUCTS OR APPROVED EQUAL. COLOR SHALL BE TAN BLEND.

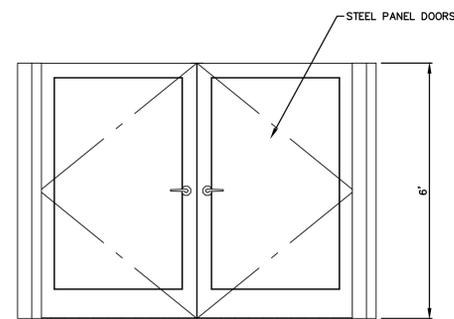
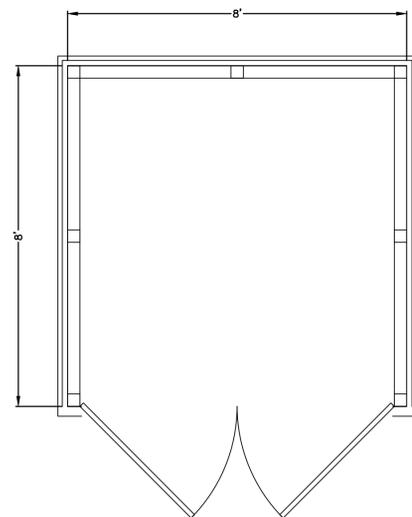
1 RETAINING WALL DETAIL
SCALE: N.T.S.



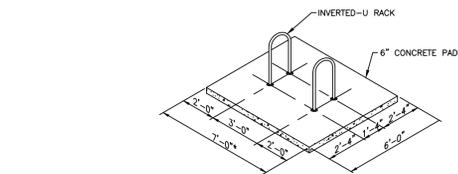
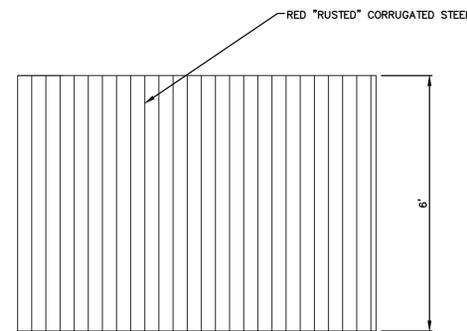
2 RETAINING WALL DETAIL
SCALE: N.T.S.



3 ACCESSIBLE PARKING DETAIL
SCALE: N.T.S.

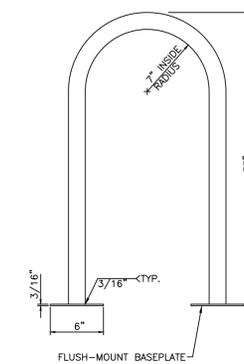


4 TRASH ENCLOSURE DETAIL
SCALE: N.T.S.

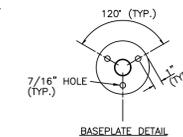


CONCRETE SLAB

- NOTES:
- HEIGHT-33" FROM THE GROUND
 - CONTINUOUS BEND INSIDE RADIUS=7"
- MATERIALS AND CONSTRUCTION:
- MINIMUM OR 1 1/4" SCHEDULE 40 STEEL PIPE (1 5/8" OUTSIDE DIAMETER)
 - MAXIMUM 1 1/2" SCHEDULE 40 STEEL PIPE (2" OUTSIDE DIAMETER)
 - SOLID ONE-PIECE CONSTRUCTION; CONTINUOUS BEND; LEGS 14"-18" APART
 - GALVANIZED WITH BLACK POWDER COAT FINISH
 - FLUSH MOUNTED WITH WELDED BASE PLATES (6" DIAMETER, 3/16" THICK BASE PLATE). HIDDEN OR VANDAL-RESISTANT FASTENERS (SCREWS OR EXPANSION BOLTS)



INVERTED U RACK



BASEPLATE DETAIL

5 BIKE RACK DETAIL
SCALE: N.T.S.

Lot 1
Erie Air Park
Replat D

3020-3100 Airport Drive
Erie, Colorado 80516

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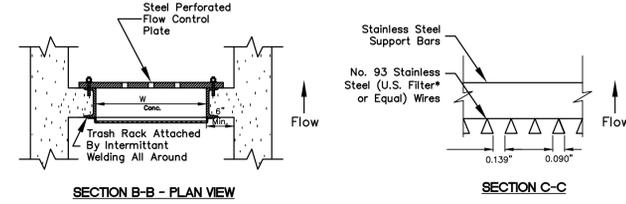
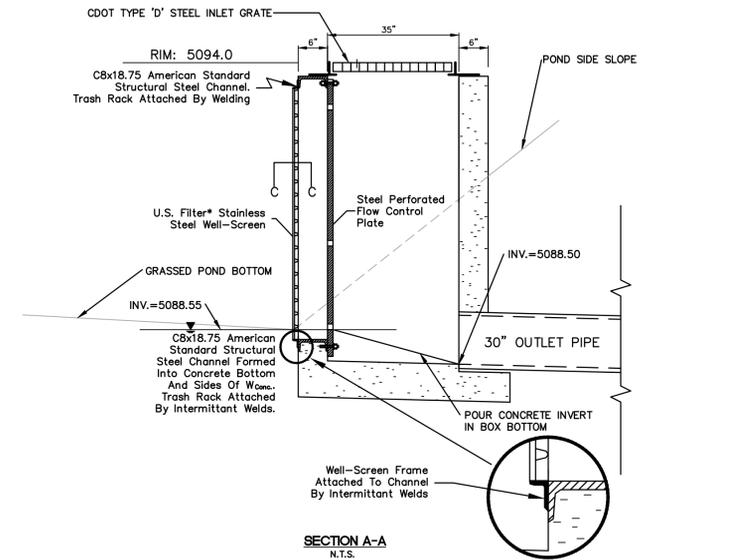
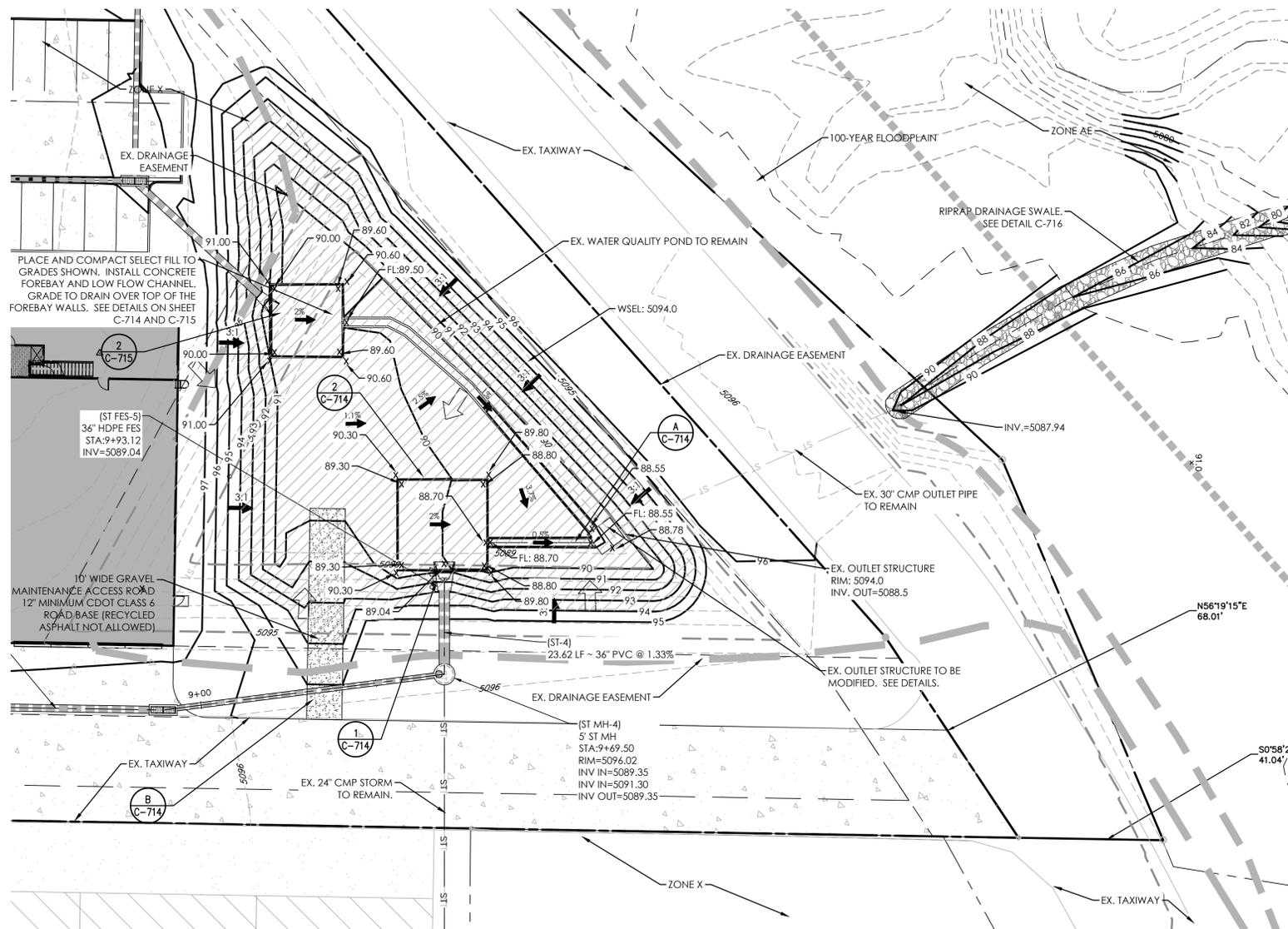
Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Details

Sheet

C-712

For
Submittal

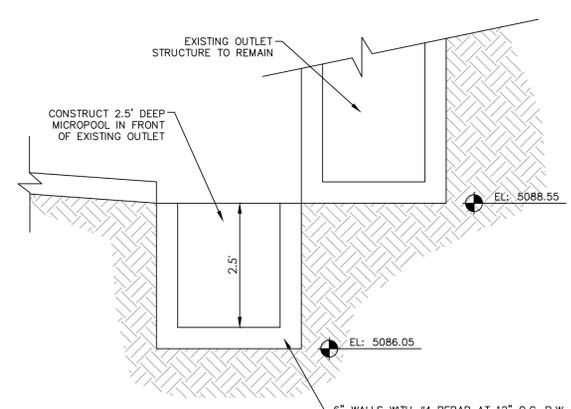


- ALL OUTLET PLATE OPENINGS ARE CIRCULAR.
- MAXIMUM DIAMETER OF OPENING = 2 INCHES.
*U.S. FILTER, ST. PAUL, MINNESOTA, USA
- TRASH RACK SHALL BE US FILTER (OR APPROVED EQUAL) STAINLESS STEEL WELL-SCREEN WITH #93 WEE WIRE, 0.139" OPENING BETWEEN WIRES, #156 VEE SUPPORT RODS ON 3/4" ON-CENTER SPACING, TOTAL RACK THICKNESS OF 0.655" AND 3/4" X 1.0" FLAT BAR WELDED CARBON STEEL FRAME.

CIRCULAR OPENINGS ONLY
 $R \text{ VALUE} = \frac{(\text{NET OPEN AREA})}{(\text{GROSS RACK AREA})} = 0.60$

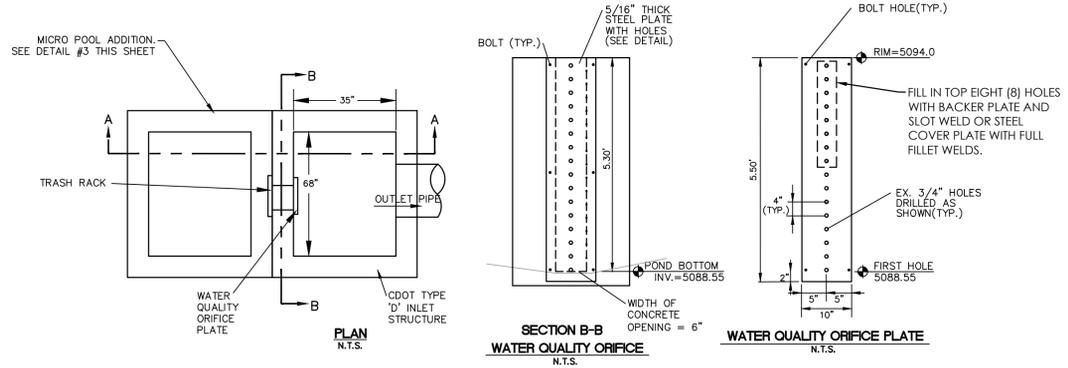
1 POND DETAIL
SCALE: 1" = 20'

1 EXISTING TRASH RACK DETAIL
SCALE: N.T.S.



3 MICROPOOL DETAIL
SCALE: N.T.S.

| WATER QUALITY POND SUMMARY | | | | |
|----------------------------|----------------------|----------------------|------------------------------|--------------------|
| STAGE | VOLUME REQUIRED (CF) | VOLUME PROVIDED (CF) | WATER SURFACE ELEVATION (FT) | RELEASE RATE (CFS) |
| WQCV | 23,392 | 36,475 | 5094.0 | N/A |
| EURV | N/A | N/A | N/A | N/A |
| 100-YR | N/A | N/A | N/A | N/A |



2 EXISTING POND OUTLET DETAIL
SCALE: N.T.S.

**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Details

Sheet

C-713

INSPECTION AND MAINTENANCE REQUIREMENTS AT SPECIFIC EDB FEATURES

| | SEDIMENT REMOVAL | MOWING/WEED CONTROL | TRASH/DEBRIS REMOVAL | EROSION | OVERGROWN VEGETATION REMOVAL | STANDING WATER (MOSQUITO/ALGAE CONTROL) | STRUCTURE REPAIR |
|---------------|------------------|---------------------|----------------------|---------|------------------------------|---|------------------|
| INFLOW POINTS | X | | X | | | | X |
| FOREBAY | X | | X | | | | X |
| LOW-FLOW | X | | X | X | X | | X |
| BOTTOM STAGE | X | X | X | X | X | X | |
| MICROPOOL | X | | X | | X | X | X |
| OUTLET WORKS | X | | X | | | | X |
| SPILLWAY | | | X | X | X | | X |
| UPPER STAGE | | | X | X | | | |
| EMBANKMENT | | X | | X | X | | |

SUMMARY OF ROUTINE MAINTENANCE ACTIVITIES FOR AN EDB

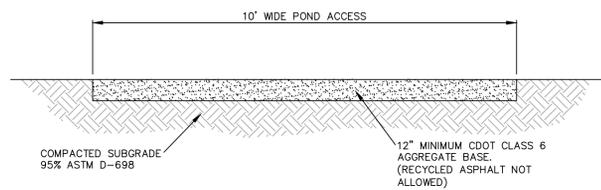
| MAINTENANCE ACTIVITY | MINIMUM FREQUENCY | INDICATION ACTION IS NEEDED | MAINTENANCE ACTION |
|-----------------------|--|--|--|
| MOWING | TWICE ANNUALLY | EXCESSIVE GRASS HEIGHT/AESTHETICS | MOW GRASS TO A HEIGHT OF 4" TO 6" |
| TRASH/DEBRIS REMOVAL | TWICE ANNUALLY | TRASH AND DEBRIS IN POND | REMOVE AND DISPOSE OF TRASH AND DEBRIS |
| OUTLET WORKS CLEANING | AS NEEDED; AFTER SIGNIFICANT RAIN EVENTS; TWICE ANNUALLY | CLOGGED OUTLET STRUCTURE; PONDING WATER | REMOVE AND DISPOSE OF DEBRIS/TRASH/SEDIMENT TO ALLOW OUTLET STRUCTURE TO FUNCTION PROPERLY |
| WEED CONTROL | MINIMUM TWICE ANNUALLY | NOXIOUS WEEDS; UNWANTED VEGETATION | TREAT WITH HERBICIDE OR HAND PULL; CONSULT WITH COUNTY WEED SPECIALIST |
| MOSQUITO TREATMENT | AS NEEDED | STANDING WATER; MOSQUITO HABITAT | TREAT WITH EPA APPROVED CHEMICALS |
| ALGAE TREATMENT | AS NEEDED | STANDING WATER; ALGAE GROWTH/GREEN COLOR | TREAT WITH EPA APPROVED CHEMICALS |

**SUMMARY OF MINOR ROUTINE MAINTENANCE ACTIVITIES FOR AN EDB
(THESE ACTIVITIES REQUIRE PRIOR CONSULTATION WITH THE TOWN)**

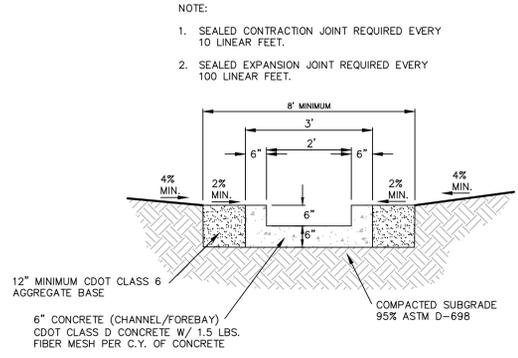
| MAINTENANCE ACTIVITY | MINIMUM FREQUENCY | INDICATION ACTION IS NEEDED | MAINTENANCE ACTION |
|----------------------------------|---|--|--|
| SEDIMENT REMOVAL | AS NEEDED; TYPICALLY EVERY 1 TO 2 YEARS | MUST REMOVE SEDIMENT WHEN IT REACHES 3/4 OF FOREBAY/MICROPOOL VOLUME | REMOVE AND DISPOSE OF SEDIMENT |
| EROSION REPAIR | AS NEEDED; BASED UPON INSPECTION | RILL/GULLY EROSION ON SIDE SLOPES; TRICKLE CHANNEL, AND OTHER AREAS | REPAIR ERODED AREAS AND REVEGETATE; ADDRESS SOURCE OF EROSION. |
| VEGETATION REMOVAL/TREE THINNING | AS NEEDED; BASED UPON INSPECTION | LARGE TREES/WOOD VEGETATION IN LOWER CHAMBER OF POND | REMOVE VEGETATION; RESTORE GRADE AND SURFACE |
| DRAIN CLEANING/JET VAC | AS NEEDED; BASED UPON INSPECTION | SEDIMENT BUILD-UP/NON DRAINING SYSTEM | CLEAN DRAINS; JET VAC IF NEEDED |

**SUMMARY OF MAJOR ROUTINE MAINTENANCE ACTIVITIES FOR AN EDB
(THESE ACTIVITIES REQUIRE PRIOR CONSULTATION WITH THE TOWN)**

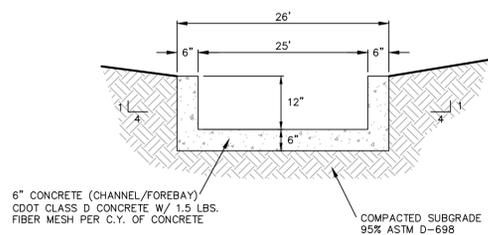
| MAINTENANCE ACTIVITY | MINIMUM FREQUENCY | INDICATION ACTION IS NEEDED | MAINTENANCE ACTION |
|------------------------|----------------------------------|--|---|
| MAJOR SEDIMENT REMOVAL | AS NEEDED; BASED UPON INSPECTION | LARGE QUANTITIES OF SEDIMENT; REDUCED POND CAPACITY | REMOVE AND DISPOSE OF SEDIMENT; REPAIR VEGETATION AS NEEDED |
| MAJOR EROSION REPAIR | AS NEEDED; BASED UPON INSPECTION | SEVERE EROSION INCLUDING GULLING, EXCESSIVE SOIL DISPLACEMENT AREAS OF SETTLEMENT, HOLES | REPAIR EROSION; FIND CAUSE OF PROBLEM AND ADDRESS TO AVOID FUTURE EROSION |
| STRUCTURAL REPAIR | AS NEEDED; BASED UPON INSPECTION | DETERIORATION AND/OR DAMAGE TO STRUCTURAL COMPONENTS (I.E. BROKEN CONCRETE, DAMAGED PIPES, DAMAGED OUTLET WORKS) | STRUCTURAL REPAIR TO RESTORE THE STRUCTURE TO ITS ORIGINAL DESIGN |



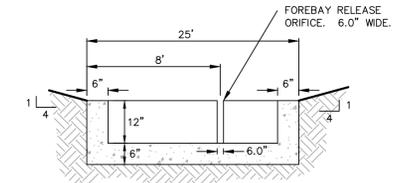
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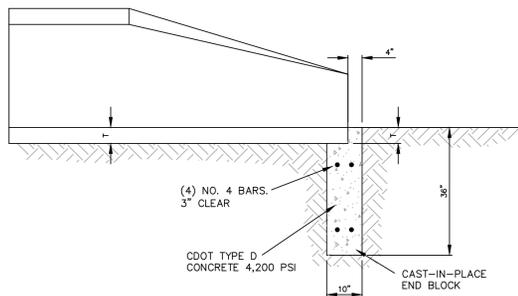
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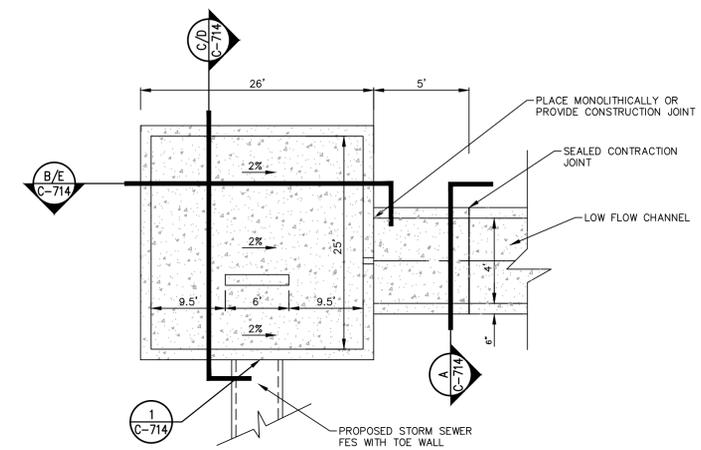
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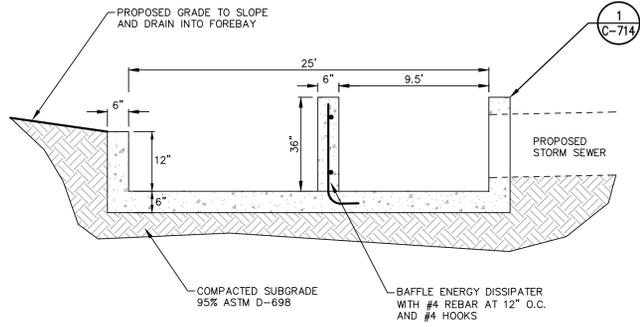
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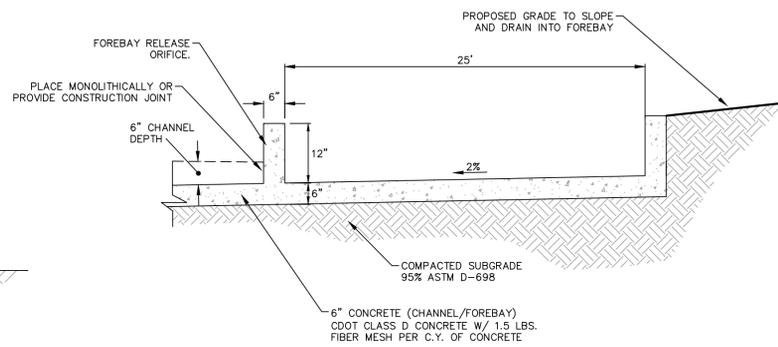
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2 FOREBAY TRANSITION DETAIL
SCALE: N.T.S.



D FOREBAY SECTION
SCALE: N.T.S.



E FOREBAY SECTION
SCALE: N.T.S.

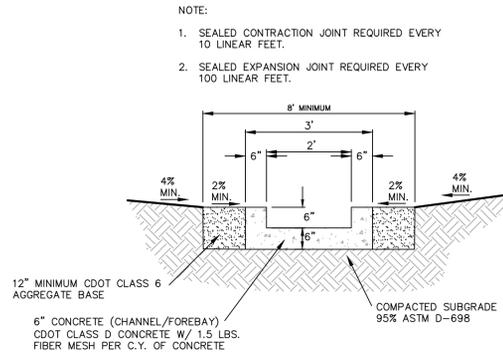
| # | Date | Description |
|---|----------|-------------------|
| 1 | 05/22/23 | Initial Submittal |
| 2 | 07/28/23 | SIP Submittal |
| 3 | 12/08/23 | Town Comments |
| 4 | 06/14/24 | Town Comments |

Project No: 23126A
By: JAS/DPA
File: 23126A-1

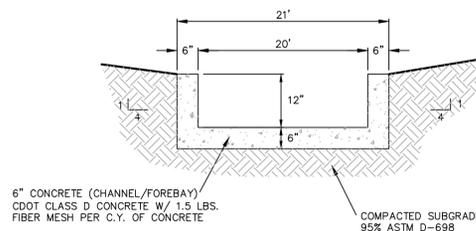
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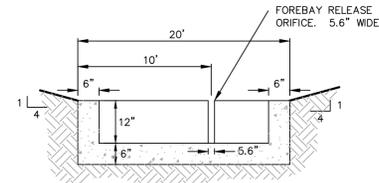
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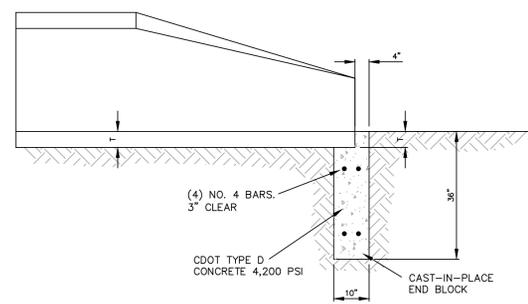
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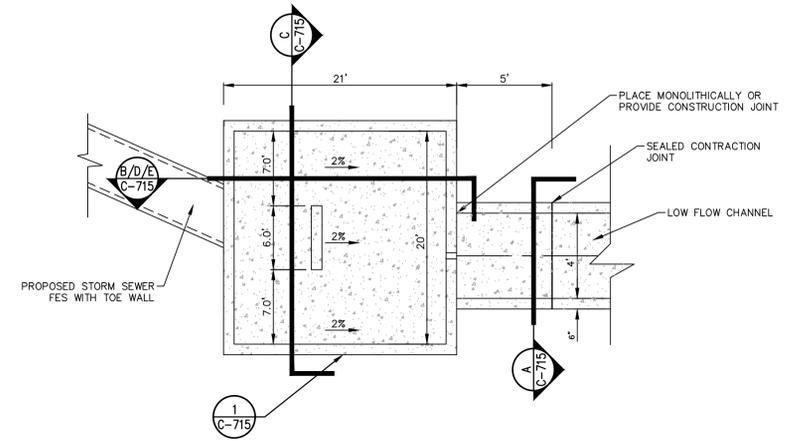
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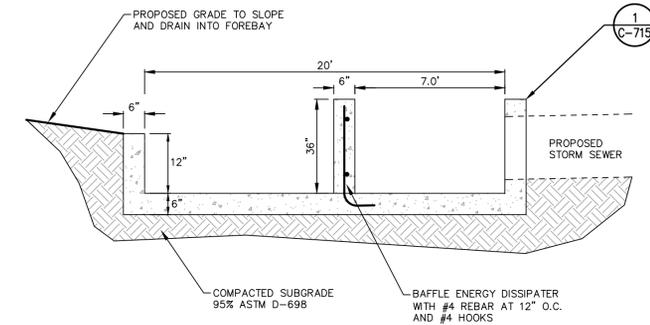
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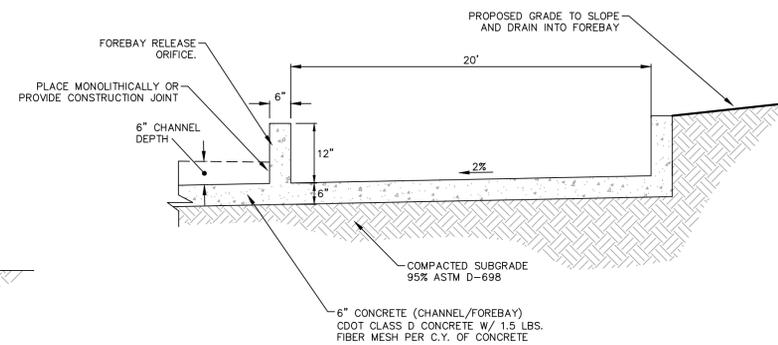
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 SCALE: N.T.S.



2 FOREBAY TRANSITION DETAIL
 SCALE: N.T.S.



D FOREBAY SECTION
 SCALE: N.T.S.



E FOREBAY SECTION
 SCALE: N.T.S.

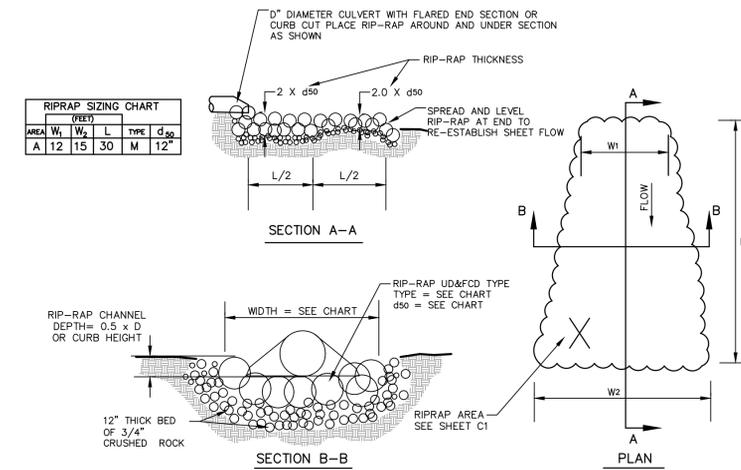
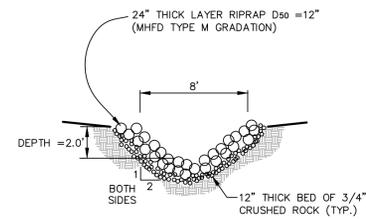
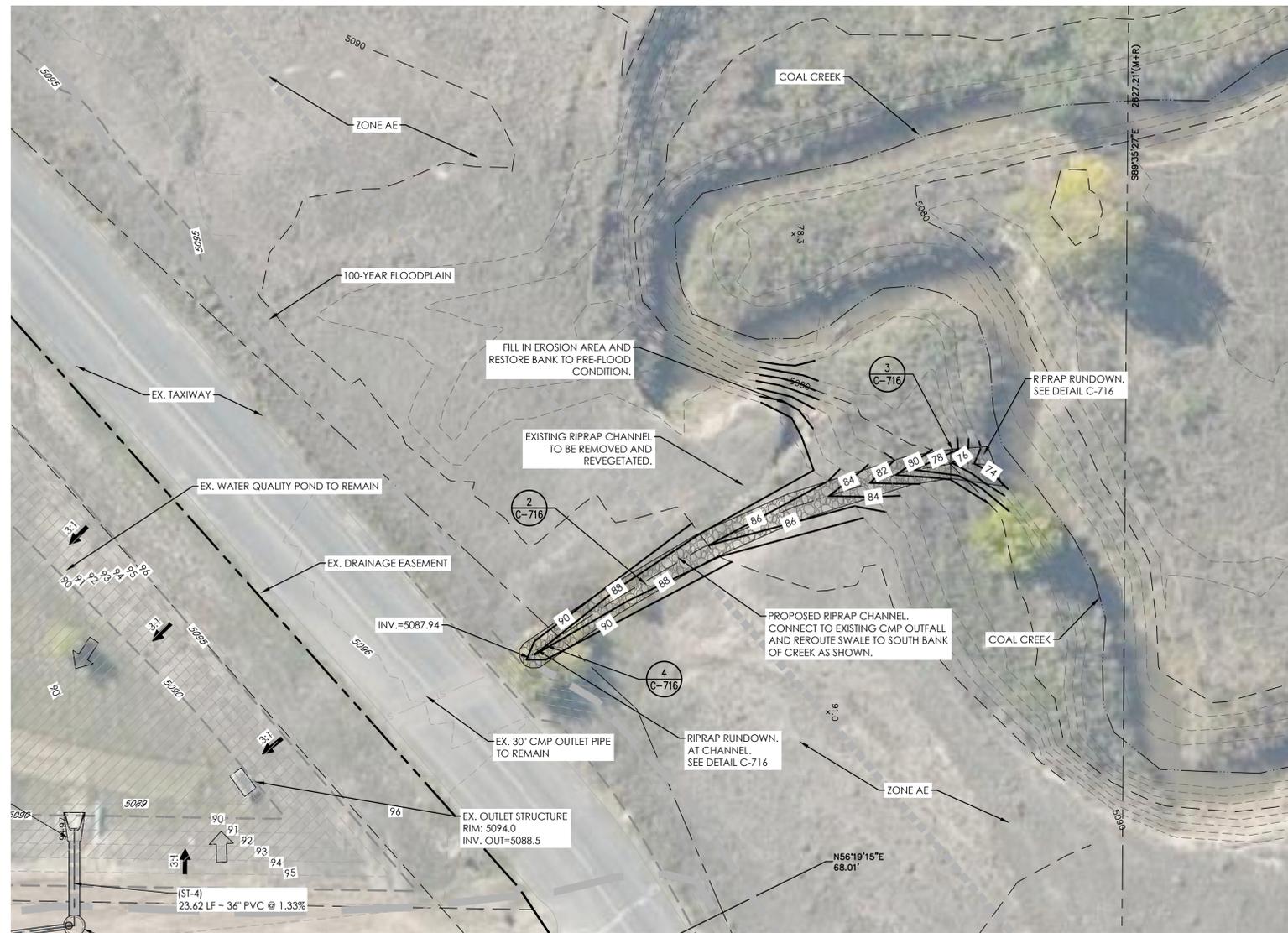
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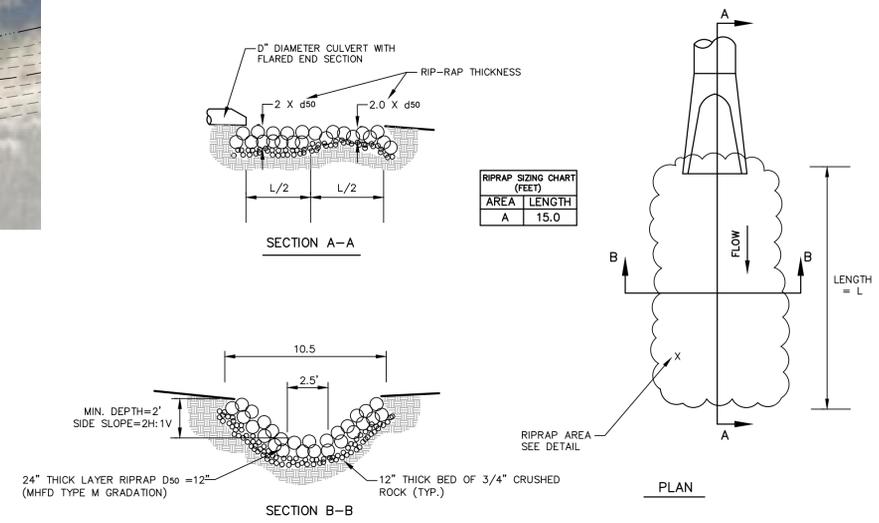
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- NOTES:
- STONE SHALL BE PLACED HOMOGENEOUSLY AND TO THE FULL COURSE THICKNESS IN ONE OPERATION; DISPLACEMENT OF UNDERLYING MATERIAL SHALL BE AVOIDED.
 - QUALITY, AND PLACEMENT OF RIPRAP SHALL CONFORM TO SECTION 506 OF CDOT'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
 - 3/4" CRUSHED ROCK SHALL CONFORM TO CDOT CLASS A FILTER MATERIAL (SECTION 703.09 CLASS A), PER MHFD REQUIREMENTS.



For
Submittal

**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

| # | Date | Description |
|---|----------|-------------------|
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