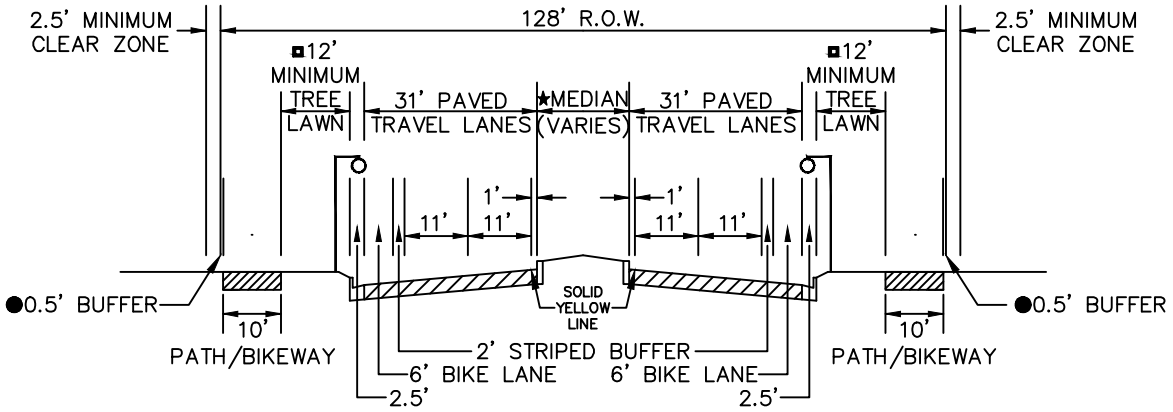
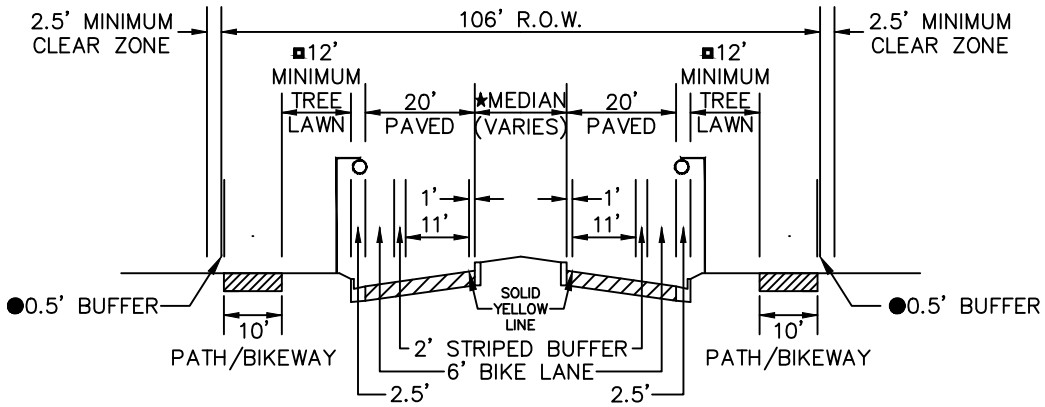
INDEX OF DRAWINGS
STREETS

DRAWING NO.	DATE	TITLE
ST1	10/2024	ARTERIAL
ST2	10/2024	ARTERIAL W/ CYCLE TRACK
ST3	10/2024	MAJOR COLLECTORS
ST4	10/2024	MINOR COLLECTORS
ST5	10/2024	VISIBILITY / SIGHT DISTANCE
ST6	10/2024	LOCAL STREETS
ST7	11/2019	PAVEMENT PHASING–NEW ROADS
ST8	10/2024	RURAL STREET
ST9	11/2019	DRIVEWAY APPROACHES FOR ROADS
ST10	01/2014	TRENCH AND CURB PATCH
ST11	01/2011	STRUCTURE PATCH
ST12	01/2011	PORTLAND CEMENT REPLACEMENT
ST13	06/2019	90° TURN – LOCAL ACCESS STREETS
ST14	01/2011	CUL–DE–SACS
ST15A	09/2017	GROUND MOUNT STREET NAME SIGN INSTALLATION
ST15B	06/2018	ROAD AND STREET NAME SIGNS
ST15C	01/2011	PRIVATE STREET SIGN
ST16	09/2022	4" PERFORATED CURB DRAIN
ST17A	09/2022	4" PERFORATED MEDIAN CURB DRAIN FOR CENTER PLANTING
ST17B	09/2022	4" PERFORATED MEDIAN CURB DRAIN FOR EDGE PLANTING
ST18	03/2020	CURB DRAIN OUTLET TREATMENT
ST19	01/2011	TYPICAL STREET UTILITY LOCATION
ST20	10/2024	ALLEY
ST21	10/2024	BIKE FACILITY
ST22	10/2024	ARTERIAL LEFT TURNS



PRINCIPAL ARTERIAL/4-LANE MINOR ARTERIAL

(< 28,000 AADT)



2-LANE MINOR ARTERIAL

(< 12,000 AADT)

▫TREE LAWN MEASURED FROM BACK OF CURB.

★MEDIAN WIDTH VARIES. R.O.W. SHOWN WITH 16' MEDIAN. MEDIAN WIDTH AND TREATMENT TO CONFORM WITH TOWN MEDIAN POLICY.

●0.5' MINIMUM BUFFER BETWEEN EDGE OF R.O.W. AND EDGE OF PATH/SIDEWALK.

CROSS-SECTION SELECTION MUST BE APPROVED BY TOWN ENGINEER. DESIGN SPEED OF PRINCIPAL ARTERIALS SHALL BE 40 MPH. DESIGN SPEED OF MINOR ARTERIALS SHALL BE 35 MPH. DESIGN SPEED WILL BE REDUCED TO 30 MPH BASED ON OPERATIONAL AND SAFETY ANALYSIS FROM THE TRANSPORTATION TEAM.

NO SINGLE FAMILY RESIDENTIAL FRONTAGE OR DRIVEWAY ACCESS

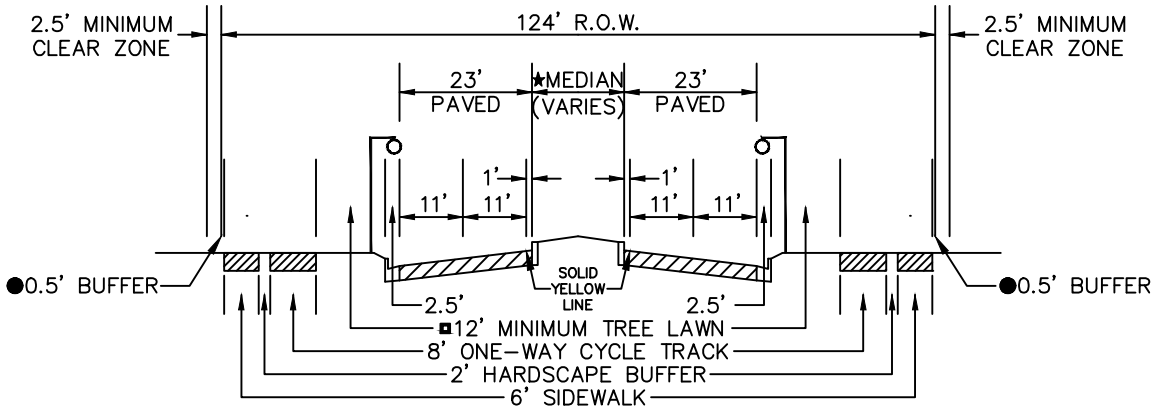
TRAFFIC CALMING DEVICES NOT ALLOWED.

The Town of ERIE COLORADO

DRAWING TITLE: ARTERIAL

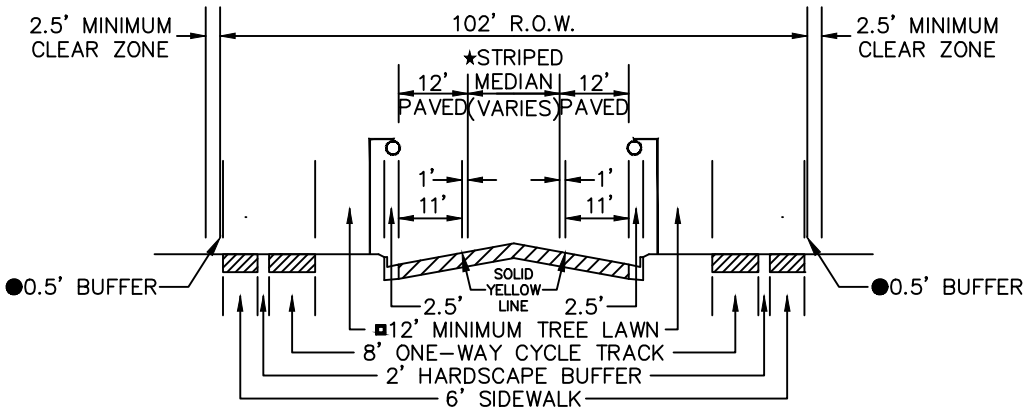
DRAWING NUMBER: ST1

DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024



PRINCIPAL ARTERIAL/4-LANE MINOR ARTERIAL W/ CYCLE TRACK

(< 28,000 AADT)



2-LANE MINOR ARTERIAL W/ CYCLE TRACK

(< 12,000 AADT)

◘TREE LAWN MEASURED FROM BACK OF CURB.

★MEDIAN WIDTH VARIES. R.O.W. SHOWN WITH 16' MEDIAN. MEDIAN WIDTH AND TREATMENT TO CONFORM WITH TOWN MEDIAN POLICY. ON 2-LANE MINOR ARTERIAL W/ CYCLE TRACK, SPOT RAISED MEDIANS CAN BE USED FOR ACCESS MANAGEMENT AND TO ALLOW PEDESTRIAN REFUGE MEDIANS.

●0.5' MINIMUM BUFFER BETWEEN EDGE OF R.O.W. AND EDGE OF PATH/SIDEWALK.

CROSS-SECTION SELECTION MUST BE APPROVED BY TOWN ENGINEER. DESIGN SPEED OF PRINCIPAL ARTERIALS SHALL BE 40 MPH. DESIGN SPEED OF MINOR ARTERIALS SHALL BE 35 MPH. DESIGN SPEED WILL BE REDUCED TO 30 MPH BASED ON OPERATIONAL AND SAFETY ANALYSIS FROM THE TRANSPORTATION TEAM.

NO SINGLE FAMILY RESIDENTIAL FRONTAGE OR DRIVEWAY ACCESS

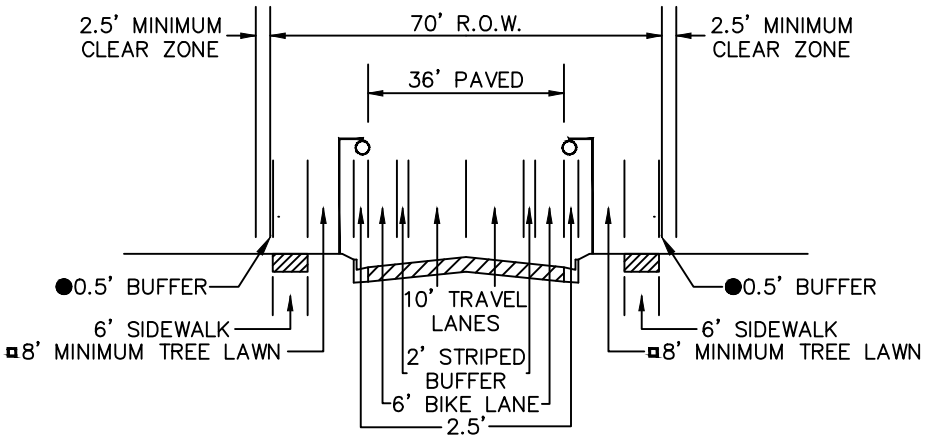
TRAFFIC CALMING DEVICES NOT ALLOWED.

The Town of ERIE COLORADO

DRAWING TITLE: ARTERIAL W/ CYCLE TRACK

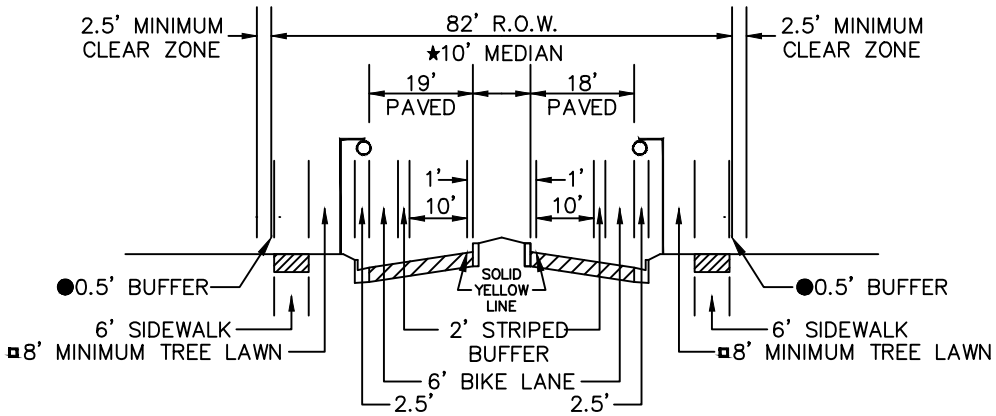
DRAWING NUMBER: ST2

DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024



MAJOR COLLECTOR

(< 9,000 AADT)



MAJOR COLLECTOR W/ MEDIAN

(< 9,000 AADT)

◘TREE LAWN MEASURED FROM BACK OF CURB.

●0.5' MINIMUM BUFFER BETWEEN EDGE OF R.O.W. AND EDGE OF SIDEWALK.

★MEDIAN ENDS WHEN LEFT TURN LANE REQUIRED. TURN LANES WILL BE REQUIRED AS DETERMINED BY A TRAFFIC STUDY.

NO SINGLE FAMILY RESIDENTIAL FRONTAGE OR DRIVEWAY ACCESS. LIMITED AND RESTRICTED DRIVEWAY ACCESS FOR ALL OTHER LAND USES.

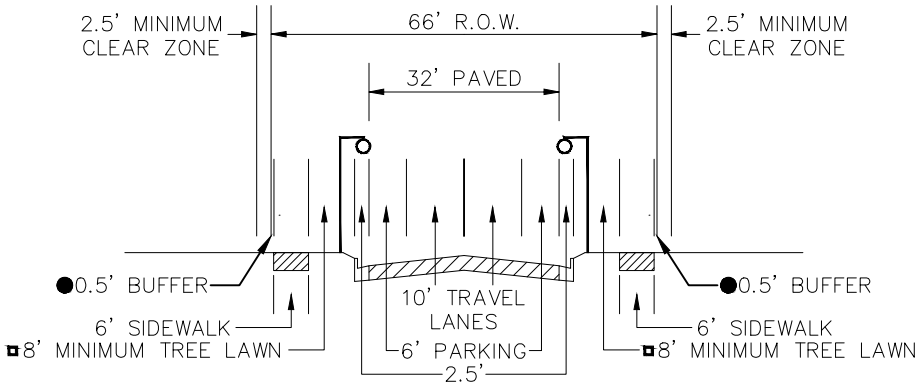
TRAFFIC CALMING DEVICES NOT ALLOWED.

The Town of ERIE COLORADO

DRAWING TITLE: MAJOR COLLECTOR

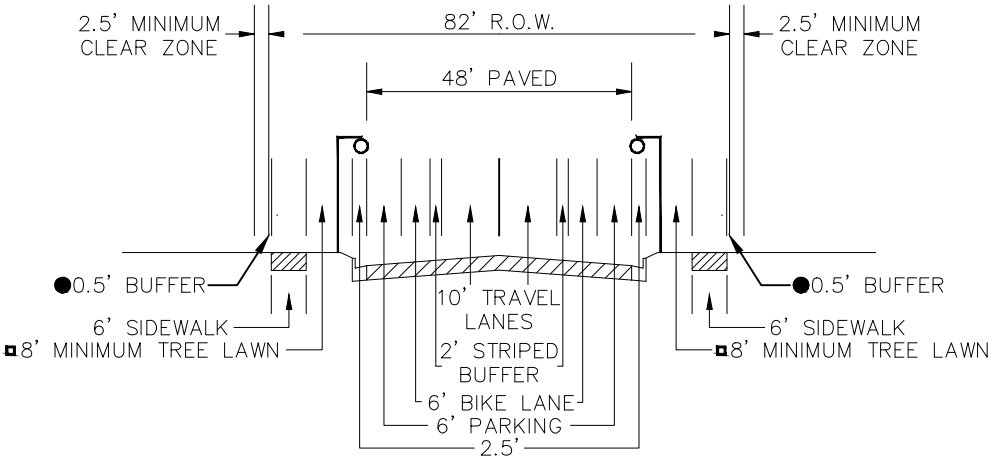
DRAWING NUMBER: ST3

DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024



MINOR COLLECTOR

(< 3,000 AADT)



MINOR COLLECTOR W/ BIKE FACILITY

(< 6,000 AADT)

◘TREE LAWN/LANDSCAPE BUFFER MEASURED FROM BACK OF CURB.

●0.5' MINIMUM BUFFER BETWEEN EDGE OF R.O.W. AND EDGE OF SIDEWALK.

LIMITED AND RESTRICTED DRIVEWAY ACCESS AS SPECIFIED IN SECTION 500

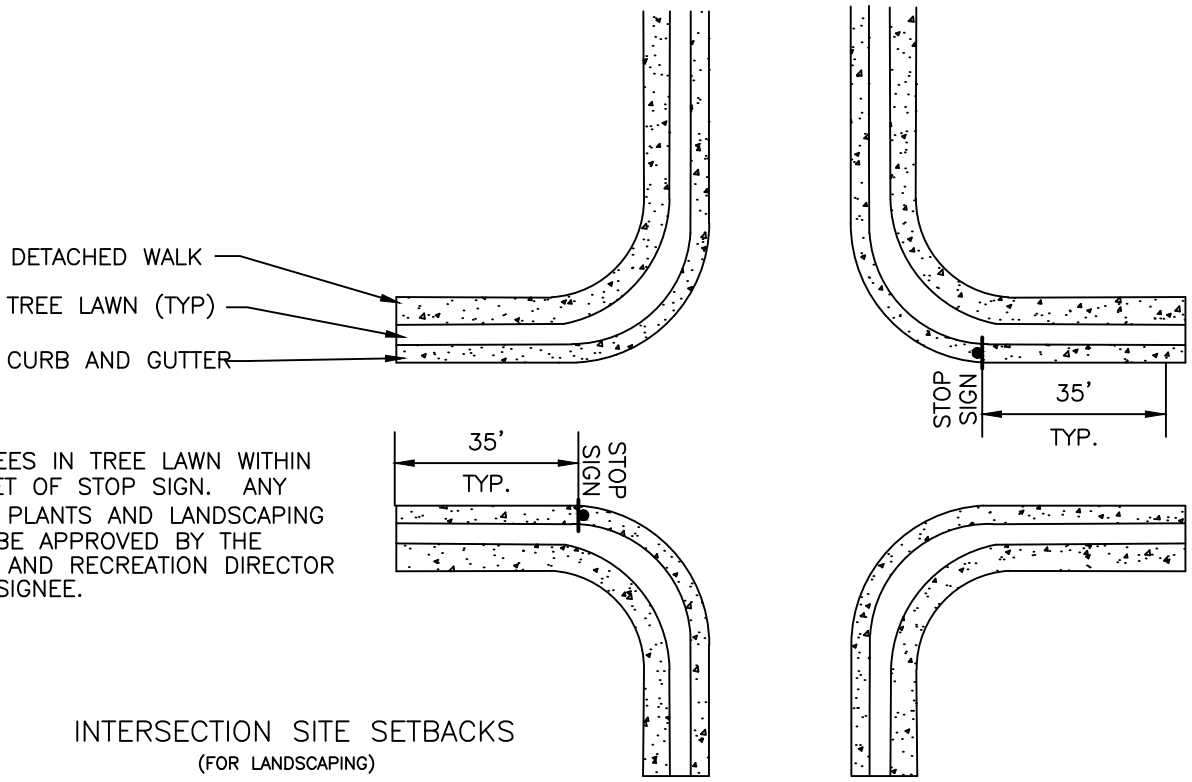
TRAFFIC CALMING DEVICES ALLOWED AS SPECIFIED IN SECTION 500

The Town of ERIE COLORADO

DRAWING TITLE: MINOR COLLECTORS

DRAWING NUMBER: ST4

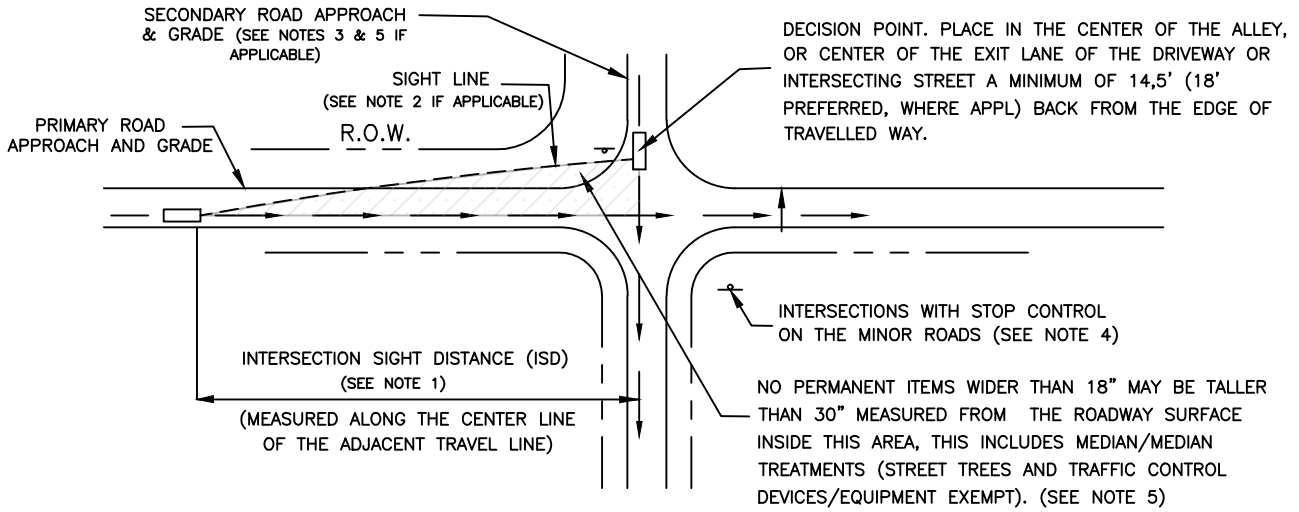
DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024



NO TREES IN TREE LAWN WITHIN 35 FEET OF STOP SIGN. ANY OTHER PLANTS AND LANDSCAPING MUST BE APPROVED BY THE PARKS AND RECREATION DIRECTOR OR DESIGNEE.

INTERSECTION SITE SETBACKS
(FOR LANDSCAPING)

INTERSECTION SIGHT LINES



MINIMUM SIGHT DISTANCE FOR STOPPED VEHICLES (FEET) (SEE NOTE 3)

DESIGN SPEED (MPH)	VIEWING APPROACHING TRAFFIC FROM BOTH THE LEFT AND RIGHT (AASHTO CASE B1)	VIEWING APPROACHING TRAFFIC FROM THE LEFT (AASHTO CASE B2)
25	280	240
30	335	290
35	390	335
40	445	380
45	500	430

NOTES:

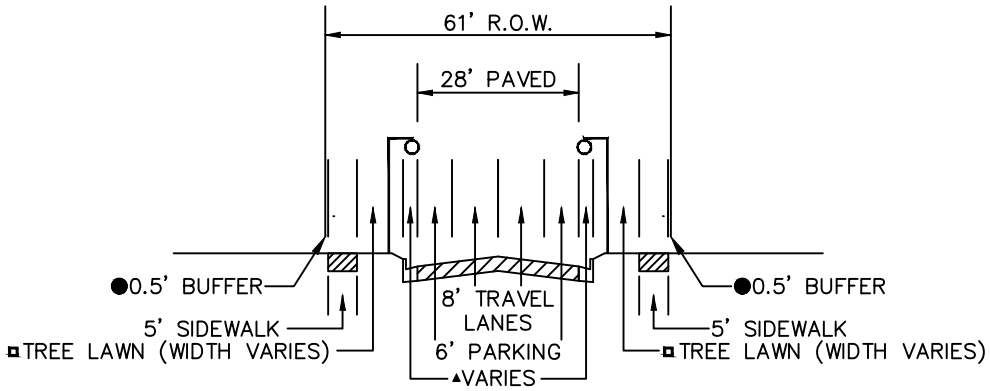
1. ADEQUATE SIGHT DISTANCE MUST BE PROVIDED ALONG THE ENTIRE ROADWAY ALIGNMENT AT EACH DRIVEWAY, ALLEY, AND INTERSECTION UNLESS A VARIANCE IS GRANTED BY THE TOWN ENGINEER.
2. IF THE SIGHT LINE EXTENDS ONTO PRIVATE PROPERTY, THEN THE BUILDINGS/ ON-SITE APPURTENANCES MUST BE PROPERLY CHAMFERED/ SETBACK. THIS IS SUBJECT TO PUBLIC WORKS REVIEW ON A CASE-BY-CASE BASIS.
3. DISTANCES SHOWN ARE FOR A STOPPED PASSENGER CAR TO TURN ONTO A TWO-LANE PRIMARY ROAD WITH NO MEDIAM AND GRADES 3% OR LESS. FOR OTHER CONDITIONS (I.E. DIFFERENT DESIGN VEHICLES, ADDITIONAL LANE) THEN REFER TO THE AASHTO GREEN BOOK (CURRENT EDITION).
4. FOR INTERSECTIONS WITH TRAFFIC SIGNAL CONTROL, ALL-WAY STOP CONTROL, PERMISSIVE RIGHT OR LEFT TURN MOVEMENTS; REFER TO AASHTO CASES D, E, B2 OR F RESPECTIVELY.
5. TREES LOCATED WITHIN THE VISIBILITY TRIANGLE MUST BE TRIMMED AT THE TRUNK TO AT LEAST 8' ABOVE THE LEVEL OF THE GROUND SURFACE, PROVIDED THAT SUCH TREES ARE SPACED SO THAT TRUNKS DO NOT OBSTRUCT VISION. STREET TREE SELECTION IN SIGHT TRIANGLE SHALL BE SUBJECT TO THE APPROVAL OF THE TOWN OF ERIE PARKS & RECREATION DEPARTMENT AND WILL PRIMARILY BE RESERVED FOR CANOPY TREE SPECIES. (REFER TO SECTION 582)

The Town of ERIE COLORADO

DRAWING TITLE: VISIBILITY / SIGHT DISTANCE

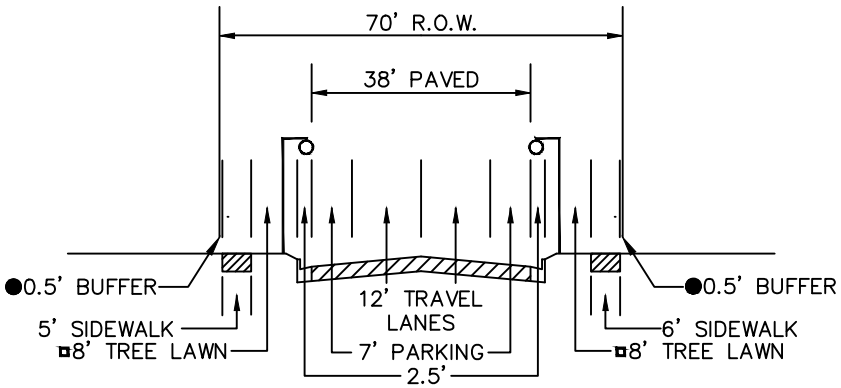
DRAWING NUMBER: ST5

DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024



LOCAL STREET

(< 1,000 AADT)



INDUSTRIAL LOCAL STREET

(< 1,000 AADT)

▲WITH STANDARD CURB AND GUTTER, WIDTH OF CURB AND GUTTER IS 2.5' AND STREET LAWN IS 8.5'. WITH ROLLOVER CURB, WIDTH OF CURB AND GUTTER IS 3' AND TREE LAWN IS 8'. IN URBAN CONTEXTS ROLLOVER CURB IS NOT ALLOWED.

▫TREE LAWN TO BE 8' MINIMUM FROM BACK OF CURB

●0.5' MINIMUM BUFFER BETWEEN EDGE OF R.O.W. AND EDGE OF PATH

VERTICAL CURB SHALL BE USED WHEN NO SINGLE FAMILY, DUPLEX, OR TOWNHOUSES ARE PRESENT, OR IF ALLEY LOADED.

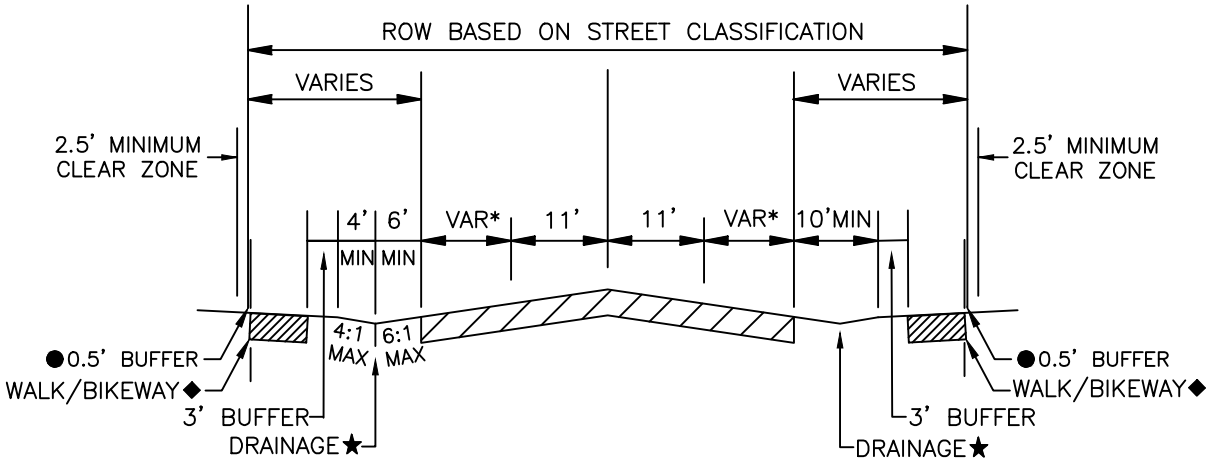
TRAFFIC CALMING DEVICES ALLOWED AS SPECIFIED IN SECTION 500

The Town of ERIE COLORADO

DRAWING TITLE: LOCAL STREETS

DRAWING NUMBER: ST6

DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024



RURAL STREET

(< 6,000 AADT)

(6,000 – 12,000 AADT)✚

RURAL STREET SECTION TO BE USED UPON TOWN APPROVAL

✚6,000 – 12,000 AADT TO INCLUDE ADDITIONAL SAFETY FEATURES INCLUDING A MEDIAN BUFFER ZONE, SHOULDER BUFFER ZONES, RUMBLE STRIPS, AUXILIARY LANES AND OTHER TREATMENTS TO BALANCE ACCESS AND MOBILITY.

*SHOULDER WIDTH VARIES DEPENDING ON DESIGN SPEED:
6' WIDTH – 30 MPH OR LESS
8' WIDTH – 35 MPH OR HIGHER

◆WALK/BIKEWAY
–WIDTH VARIES DEPENDING ON DESIGN SPEED:
6' WIDTH – 35 MPH OR LESS
10' WIDTH – 40 MPH OR HIGHER

–WALK, WHERE REQUIRED, TO BE PROVIDED IN RIGHT OF WAY
–WALK SHOULD HAVE A 2% MAX CROSS SLOPE TOWARD THE DRAINAGE DITCH

★DRAINAGE DITCH TO BE ENGINEERED, CULVERTS MAY BE REQUIRED AT CROSS STREETS AND DRIVEWAYS

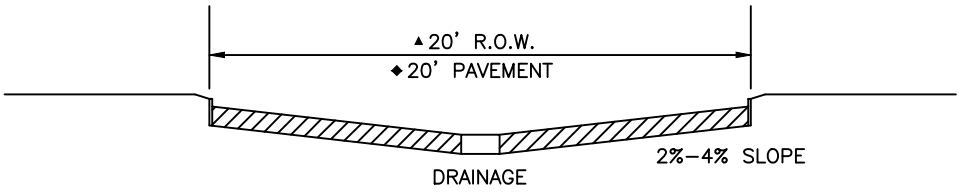
●0.5' MINIMUM BUFFER BETWEEN EDGE OF R.O.W. AND EDGE OF PATH/SIDEWALK.

LEFT TURN AND RIGHT TURN LANES MAY BE REQUIRED AT INTERSECTIONS

TRAFFIC CALMING DEVICES NOT ALLOWED.

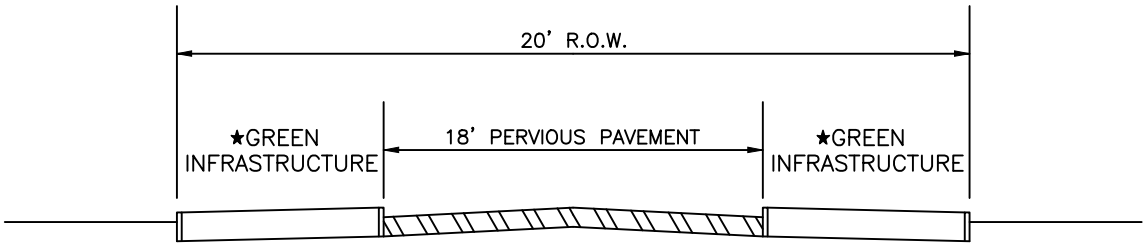
The Town of ERIE COLORADO

DRAWING TITLE: RURAL STREET
DRAWING NUMBER: ST8
DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024



TYPICAL ALLEY

(< 500 AADT)



GREEN ALLEY

(< 500 AADT)

◆20' PAVEMENT WIDTH REQUIRED WHERE EMERGENCY ACCESS IS NEEDED. 18' PAVEMENT WIDTH MAY BE CONSIDERED WHERE NO EMERGENCY ACCESS IS NEEDED.

▲REQUIRED R.O.W. WIDTH MAY CHANGE BASED ON UTILITY EASEMENT REQUIREMENTS.

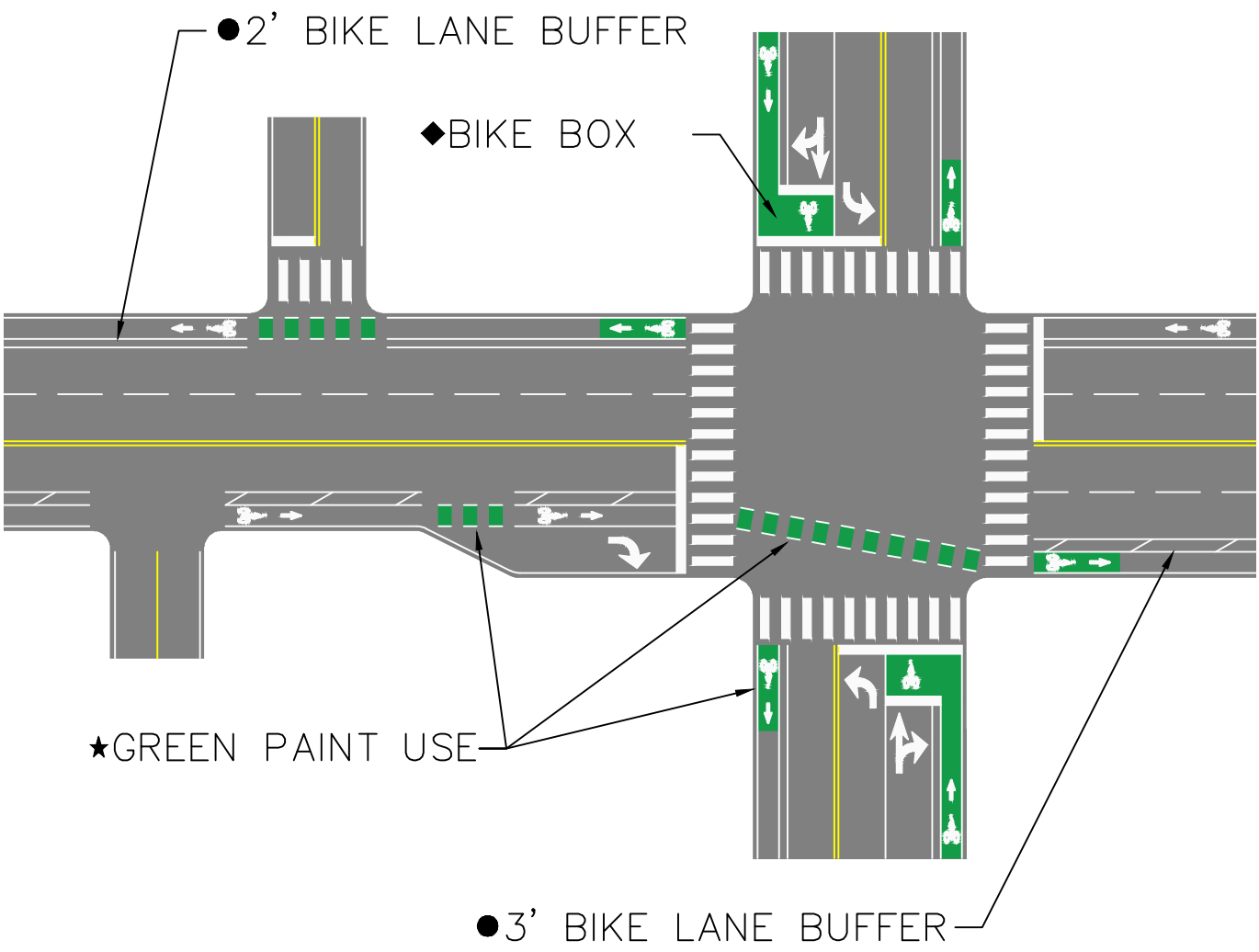
★GREEN INFRASTRUCTURE TREATMENTS MUST BE FULLY ENGINEERED AND APPROVED BY TOWN ENGINEER.

The Town of ERIE COLORADO

DRAWING TITLE: ALLEY

DRAWING NUMBER: ST20

DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024



◆SEE SECTION 521.04 OF TOWN STREET CONSTRUCTION FOR SITUATIONS WHEN A BICYCLE BOX MAY BE APPROPRIATE. THE BICYCLE BOX SHALL BE AT LEAST 10' BETWEEN THE INTERSECTION STOP LINE AND THE ADVANCE STOP LINE.

●1.5' MINIMUM BUFFER BETWEEN BIKE LANE AND VEHICLE LANE. NO CROSS-HATCH FOR 1.5'-2.5' BUFFER. DIAGONAL CROSS-HATCH WITH 40' SPACING FOR 3' OR GREATER BUFFER.

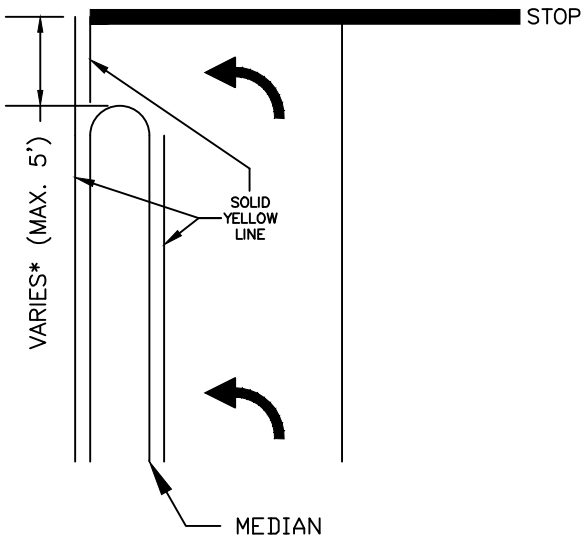
★GREEN PAINT TO BE USED BETWEEN LONGITUDINAL DASHED WHITE LINES DENOTING CONFLICT ZONE WITH VEHICLES: ENTRANCE TO RIGHT TURN POCKET, CROSSINGS AT ANY ARTERIAL-ARTERIAL INTERSECTION OR MAJOR COLLECTOR-ARTERIAL INTERSECTION, ACROSS INTERSECTIONS WHERE THERE IS CHANGE IN HORIZONTAL ALIGNMENT OF BIKE LANE THROUGH INTERSECTION.
GREEN PAINT TO BE USED TO ENHANCE VISIBILITY OF BIKE FACILITY: FIRST 8'-20' OF BIKE LANE ON FAR SIDE OF INTERSECTION, WITHIN A BIKE BOX AND 20' OF BIKE LANE BEFORE A BIKE BOX.

The Town of ERIE COLORADO

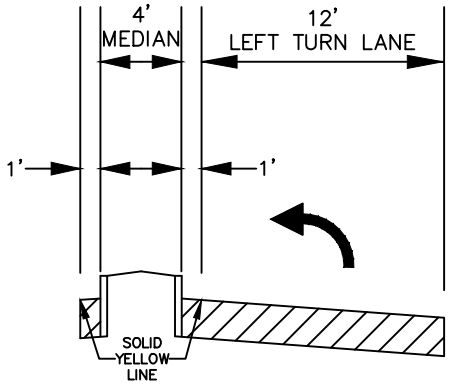
DRAWING TITLE: BIKE FACILITY

DRAWING NUMBER: ST21

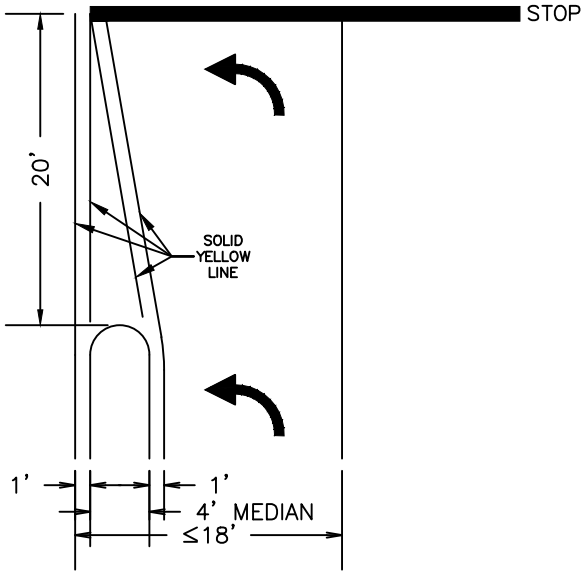
DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024



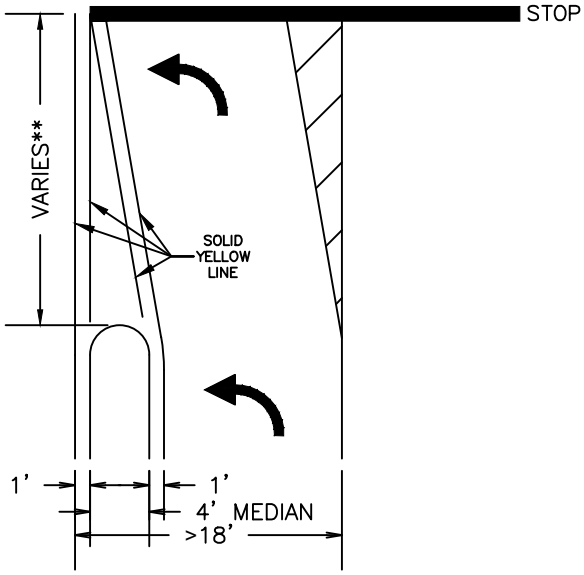
TYPICAL LEFT TURN LANE◘



TYPICAL MEDIAN W/ LEFT TURN LANE



POSITIVE OFFSET W/O TAPER★



POSITIVE OFFSET W/ TAPER●

*SET AS THE MINIMUM DISTANCE DETERMINED USING TURNING TEMPLATES DURING DESIGN.
**CALCULATED BASED ON TRANSITION TAPER FORMULA.
◘APPLIES TO PROTECTED ARTERIAL LEFT TURNS AND ARTERIAL LEFT TURNS WITHOUT AN OPPOSING LEFT TURN.
★APPLIES TO PERMISSIVE ARTERIAL LEFT TURNS WITH AN OPPOSING LEFT TURN LANE WHERE TURN LANE, MEDIAN, AND MEDIAN APRONS ARE LESS THAN OR EQUAL TO 18'.
●APPLIES TO PERMISSIVE ARTERIAL LEFT TURNS WITH AN OPPOSING LEFT TURN LANE WHERE TURN LANE, MEDIAN, AND MEDIAN APRONS ARE GREATER THAN 18'.

The Town of ERIE COLORADO

DRAWING TITLE: ARTERIAL LEFT TURNS
DRAWING NUMBER: ST22
DRAWN BY: A. HARMANN APPROVED BY: D. PASIC DATE: 10/2024

INDEX OF DRAWINGS
CURB/GUTTER & SIDEWALKS

DRAWING NO.	DATE	TITLE
SW1	01/2010	CURB AND GUTTER JOINT DETAIL
SW2	03/2016	MONOLITHIC INTEGRAL CURBWALK
SW3	06/2010	CONCRETE CROSS PAN
SW4A	03/2023	DRIVE CUT–DETACHED WALK
SW4B	03/2023	DRIVE CUT–ATTACHED WALK
SW5	10/2024	CURB RAMP TYPE 1 ATTACHED WALK
SW6	10/2024	CURB RAMP TYPE 2 ATTACHED WALK
SW7A	10/2024	ADDITIONAL CURB RAMP PLACEMENT OPTIONS
SW7B	10/2024	ADDITIONAL CURB RAMP PLACEMENT OPTIONS
SW8	10/2024	CURB RAMP TYPE 4 DETACHED SIDEWALK
SW9	10/2024	CURB RAMP TYPE 3 DETACHED SIDEWALK
SW10A	08/2018	CURB RAMP MID BLOCK TYPE 1 DETACHED SIDEWALK
SW10B	08/2018	CURB RAMP MID BLOCK TYPE 3 DETACHED SIDEWALK
SW10C	08/2018	CURB RAMP MID BLOCK TYPE 2 ATTACHED SIDEWALK
SW11	03/2016	MOUNTABLE CURB SECTION
SW12	05/2019	VERTICAL CURB SECTION
SW13A	11/2019	6" VERTICAL CURB, GUTTER AND DETACHED SIDEWALK
SW13B	11/2019	6" MOUNTABLE CURB, GUTTER AND DETACHED SIDEWALK
SW14		THIS PAGE HAS BEEN LEFT BLANK INTENTIONALLY

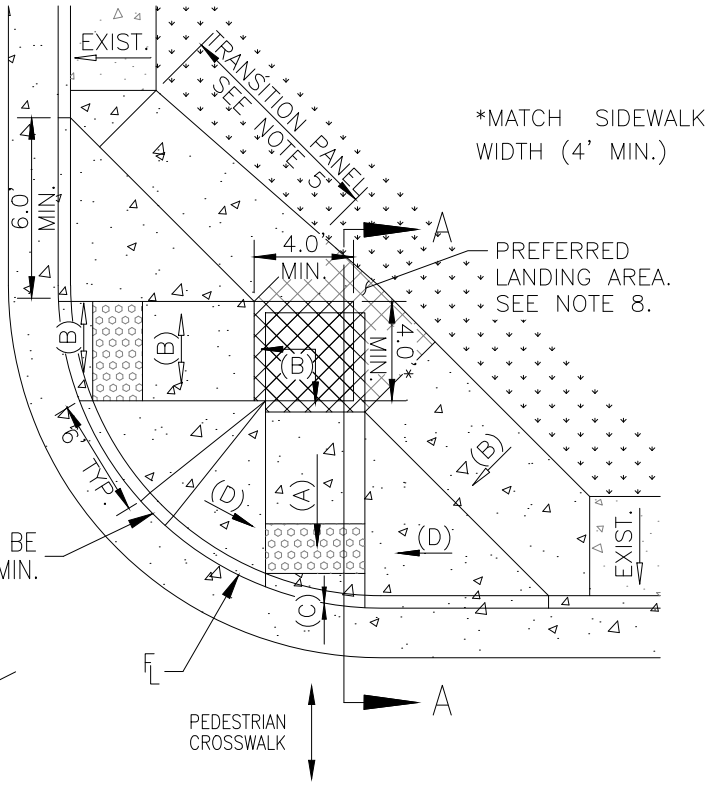
LEGEND

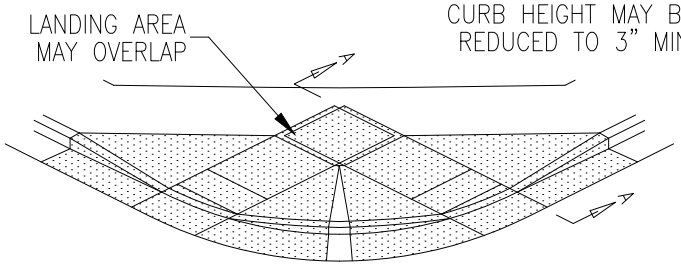
- CURB RAMP PAY AREA
- DETECTABLE WARNING SURFACE
- LANDING AREA
- LANDSCAPE AREA
- RUNNING SLOPE
- CROSS SLOPE

SLOPE LEGEND

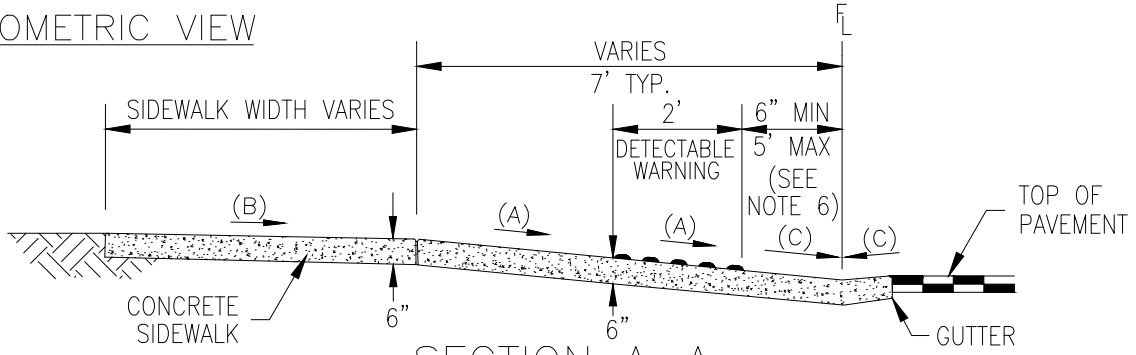
(A) 7.8% PREF. (12:1 MAX.)
(B) 1.5% PREF. (48:1 MAX.)
(C) 4.5% PREF. (20:1 MAX.)
(D) 9.5% PREF. (10:1 MAX.)

PEDESTRIAN CROSSWALK





ISOMETRIC VIEW



SECTION A–A

NOTES:

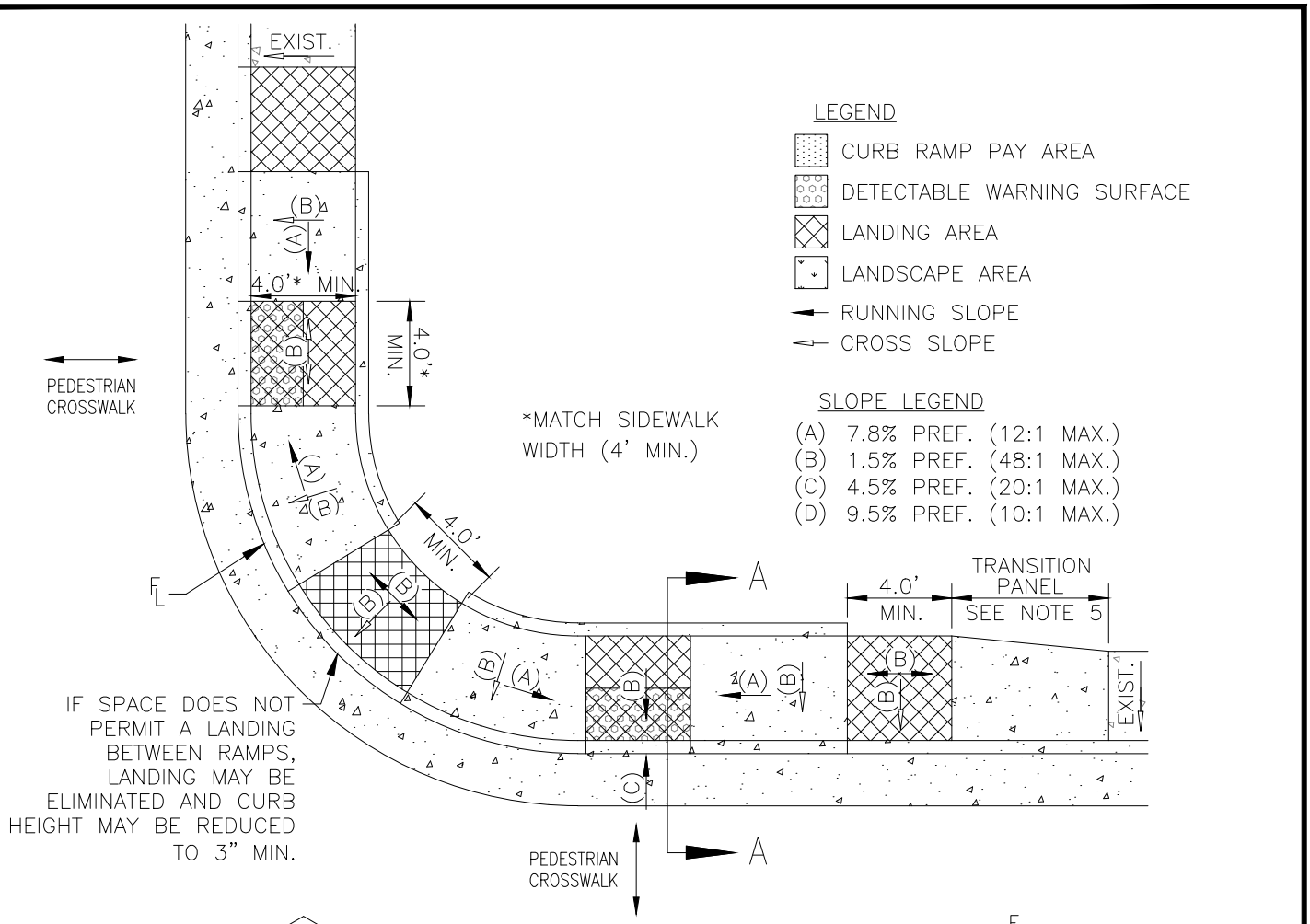
1. CURB RAMPS SHALL BE PROVIDED AT ALL CORNERS OF STREET INTERSECTIONS AND AT "T" INTERSECTIONS WHERE THERE IS EXISTING OR PROPOSED SIDEWALK AND CURB.
2. CURB RAMP SURFACE SHALL HAVE A COARSE BROOM FINISH PERPENDICULAR TO THE DIRECTION OF TRAVEL. THE RAMP AREA SHALL RECEIVE A COARSER BRUSH TREATMENT THAN THE SIDEWALK.
3. CURB RAMPS SHALL BE POURED MONOLITHICALLY WITH THE CURB, GUTTER AND APRON.
4. CURB RAMP DIMENSIONS SHALL BE SPECIFIED ON THE CONSTRUCTIONS PLANS.
5. SIDEWALK TRANSITIONS SHALL BE 6' MINIMUM AND 15' MAXIMUM. IF A TRANSITION HAS REACHED 15' IN LENGTH AND GRADE HAS NOT YET MATCHED EXISTING, RUNNING SLOPE MAY EXCEED 12:1. SIDEWALK TRANSITION PANELS SHALL HAVE A HORIZONTAL TAPER OF 10:1 PREFERRED AND 3:1 MINIMUM. SIDEWALK TRANSITION PANEL SLOPE TRANSITION SHALL BE 0.5%/FT MAXIMUM. A 4'X4' MINIMUM LANDING AREA SHALL BE PROVIDED AT THE TOP OF A SIDEWALK TRANSITION PANEL IF RUNNING SLOPE IS GREATER THAN 5% AND IF THE TRANSITION PANEL ABUTS A CHANGE IN DIRECTION.
6. IF THE SPACE BETWEEN THE FLOWLINE AND DETECTABLE WARNING SURFACE EXCEEDS 5', THE DETECTABLE WARNING SURFACE SHALL BE PLACED RADIALLY ALONG THE FLOWLINE.
7. WINGED CURB RAMPS, LIKE CURB RAMP TYPE 4, ARE PREFERRED WHERE PEDESTRIAN ACTIVITY IS LIKELY ADJACENT TO THE CURB RAMP AND THERE IS NO OBSTACLE.
8. ALL GRADE BREAKS SHALL BE PERPENDICULAR TO THE DIRECTION OF TRAVEL.
9. DETECTABLE WARNING SURFACES SHALL BE PER CDOT STANDARD PLAN M–608–1.

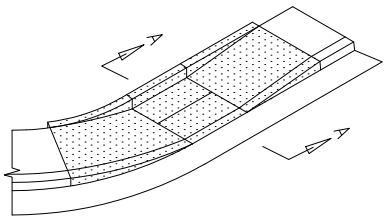
The Town of ERIE COLORADO

DRAWING TITLE: CURB RAMP TYPE 1 ATTACHED WALK

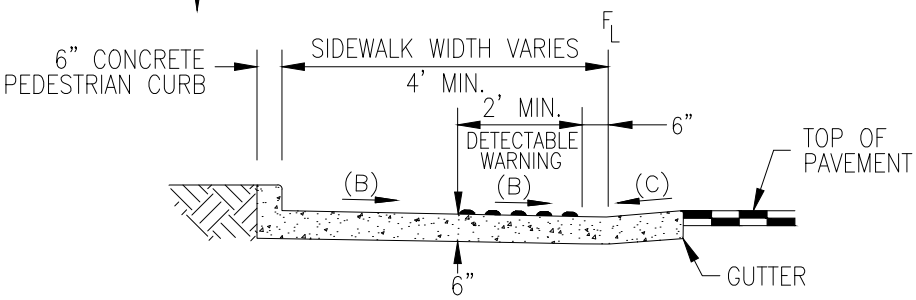
DRAWING NUMBER: SW5

DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 10/2024





ISOMETRIC VIEW



SECTION A–A

NOTES:

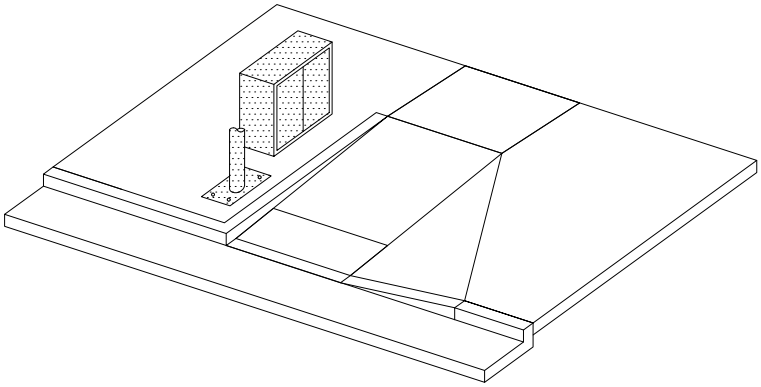
1. CURB RAMPS SHALL BE PROVIDED AT ALL CORNERS OF STREET INTERSECTIONS AND AT "T" INTERSECTIONS WHERE THERE IS EXISTING OR PROPOSED SIDEWALK AND CURB.
2. CURB RAMP SURFACE SHALL HAVE A COARSE BROOM FINISH PERPENDICULAR TO THE DIRECTION OF TRAVEL. THE RAMP AREA SHALL RECEIVE A COARSER BRUSH TREATMENT THAN THE SIDEWALK.
3. CURB RAMPS SHALL BE POURED MONOLITHICALLY WITH THE CURB, GUTTER AND APRON.
4. CURB RAMP DIMENSIONS SHALL BE SPECIFIED ON THE CONSTRUCTIONS PLANS.
5. SIDEWALK TRANSITIONS SHALL BE 6' MINIMUM AND 15' MAXIMUM. IF A TRANSITION HAS REACHED 15' IN LENGTH AND GRADE HAS NOT YET MATCHED EXISTING, RUNNING SLOPE MAY EXCEED 12:1. SIDEWALK TRANSITION PANELS SHALL HAVE A HORIZONTAL TAPER OF 10:1 PREFERRED AND 3:1 MINIMUM. SIDEWALK TRANSITION PANEL SLOPE TRANSITION SHALL BE 0.5%/FT MAXIMUM.
6. CONSTRUCTION OF THE CONCRETE PEDESTRIAN CURB TO BE INCLUDED IN THE COST OF THE CURB RAMP.
7. ALL GRADE BREAKS SHALL BE PERPENDICULAR TO THE DIRECTION OF TRAVEL.
8. DETECTABLE WARNING SURFACES SHALL BE PER CDOT STANDARD PLAN M–608–1.

The Town of ERIE COLORADO

DRAWING TITLE: CURB RAMP TYPE 2 ATTACHED WALK

DRAWING NUMBER: SW6

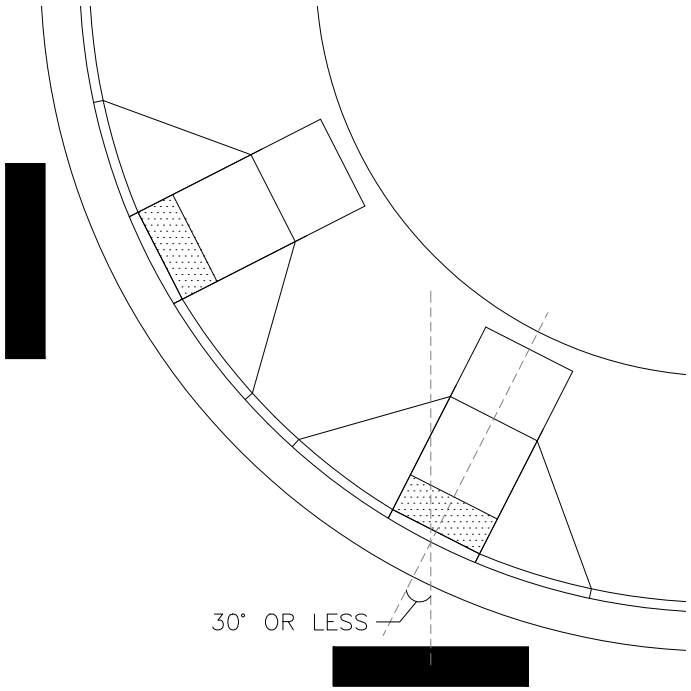
DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 10/2024



FLARED RAMP CONSTRAINED BY AN OBJECT

NOTES:

IF AN OBSTACLE IS CONSTRAINING A FLARED CURB RAMP SUCH THAT AN ADA COMPLIANT FLARE CANNOT BE INSTALLED, A PEDESTRIAN CURB MAY BE INSTALLED ALONG THAT SIDE OF THE RAMP. THE COST OF THE PEDESTRIAN CURB SHALL BE INCLUDED IN THE CURB RAMP. THE REMAINDER OF THE RAMP SHALL BE IN ACCORDANCE WITH DRAWING NUMBERS SW5 AND/OR SW8.



PERPENDICULAR FLARED RAMP

NOTES:

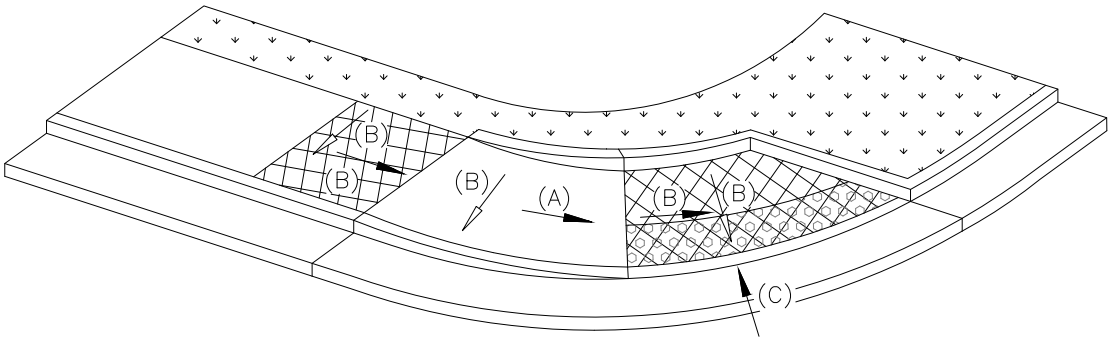
IN THE CASE THAT THE DISTANCE BETWEEN THE DETECTABLE WARNING SURFACE AND THE FLOWLINE EXCEEDS 5', CURB RAMPS MAY BE PLACED PERPENDICULAR TO THE FLOWLINE AS AN ALTERNATIVE TO RADIALLY PLACED DETECTABLE WARNING SURFACES. PERPENDICULAR FLARED CURB RAMPS MUST BE PLACED SO THAT THE PEDESTRIAN CIRCULATION PATH DOES NOT DEFLECT MORE THAN 30° FROM THE CROSSWALK.

The Town of ERIE COLORADO

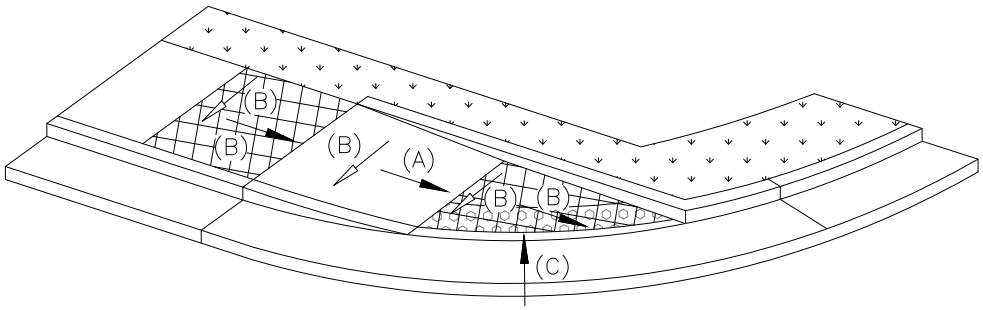
DRAWING TITLE: ADDITIONAL CURB RAMP PLACEMENT OPTIONS

DRAWING NUMBER: SW7A

DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 10/2024



PARALLEL CURB RAMP (SIDEWALK ENDS)(ALT A)



PARALLEL CURB RAMP (SIDEWALK ENDS)(ALT B)

LEGEND

- CURB RAMP PAY AREA
- DETECTABLE WARNING SURFACE
- LANDING AREA
- LANDSCAPE AREA
- RUNNING SLOPE
- CROSS SLOPE

SLOPE LEGEND

(A) 7.8% PREF. (12:1 MAX.)
(B) 1.5% PREF. (48:1 MAX.)
(C) 4.5% PREF. (20:1 MAX.)
(D) 9.5% PREF. (10:1 MAX.)

NOTES:

LANDING AREA, TRANSITION PANELS, AND MINIMUM WIDTHS SHALL BE IN ACCORDANCE WITH DRAWING NUMBERS SW6 AND SW9.

The Town of ERIE COLORADO

DRAWING TITLE: ADDITIONAL CURB RAMP PLACEMENT OPTIONS

DRAWING NUMBER: SW7B

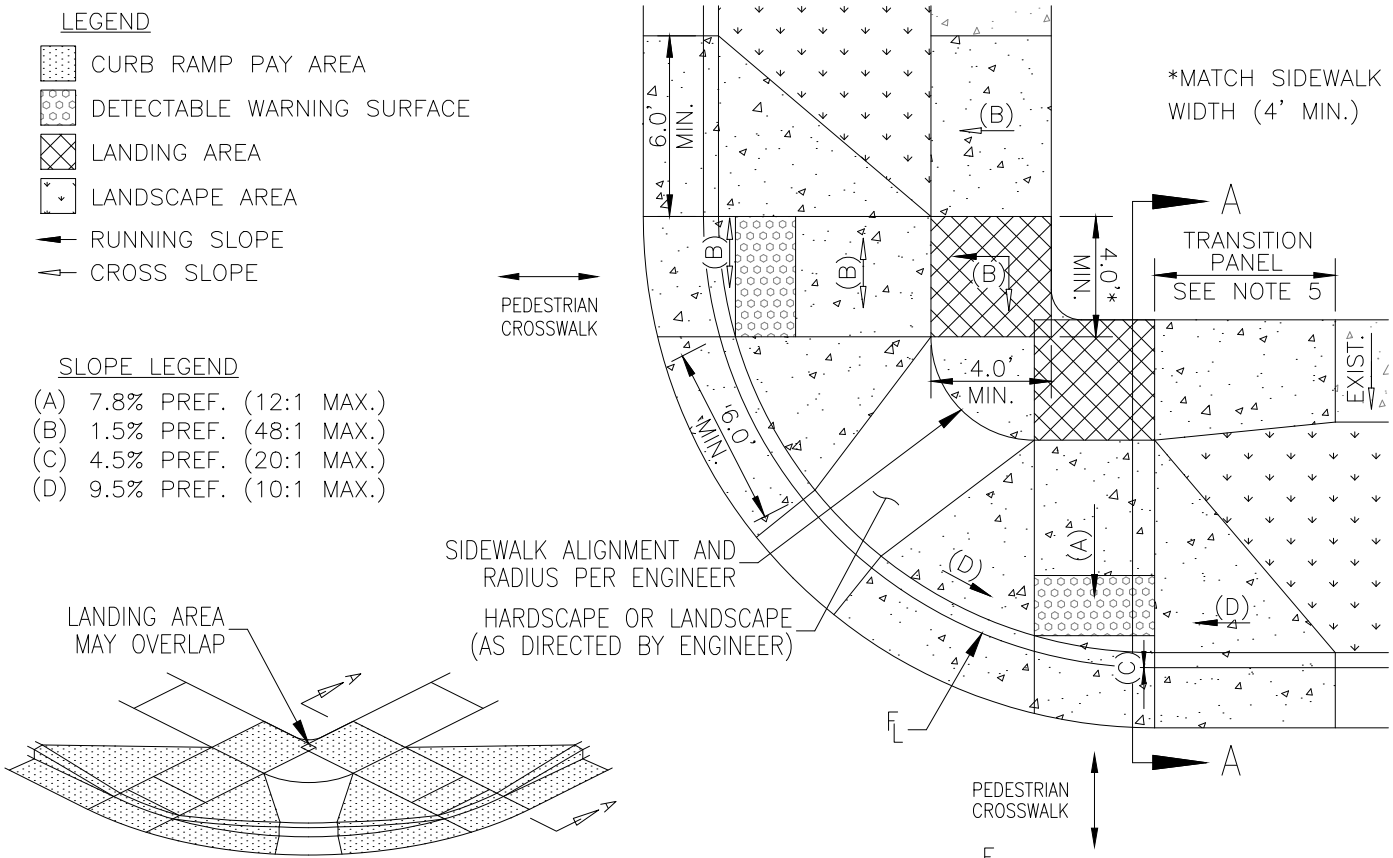
DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 10/2024

LEGEND

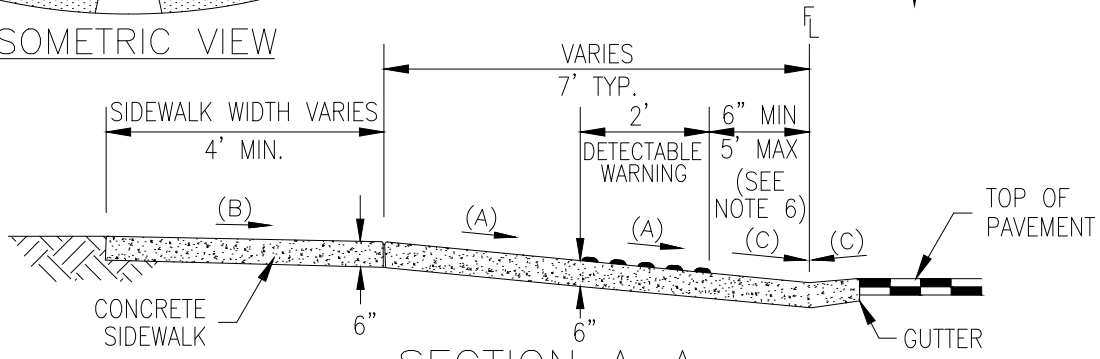
- CURB RAMP PAY AREA
- DETECTABLE WARNING SURFACE
- LANDING AREA
- LANDSCAPE AREA
- RUNNING SLOPE
- CROSS SLOPE

SLOPE LEGEND

(A) 7.8% PREF. (12:1 MAX.)
(B) 1.5% PREF. (48:1 MAX.)
(C) 4.5% PREF. (20:1 MAX.)
(D) 9.5% PREF. (10:1 MAX.)



ISOMETRIC VIEW



SECTION A–A

NOTES:

1. CURB RAMPS SHALL BE PROVIDED AT ALL CORNERS OF STREET INTERSECTIONS AND AT "T" INTERSECTIONS WHERE THERE IS EXISTING OR PROPOSED SIDEWALK AND CURB.
2. CURB RAMP SURFACE SHALL HAVE A COARSE BROOM FINISH PERPENDICULAR TO THE DIRECTION OF TRAVEL. THE RAMP AREA SHALL RECEIVE A COARSER BRUSH TREATMENT THAN THE SIDEWALK.
3. CURB RAMPS SHALL BE POURED MONOLITHICALLY WITH THE CURB, GUTTER AND APRON.
4. CURB RAMP DIMENSIONS SHALL BE SPECIFIED ON THE CONSTRUCTIONS PLANS.
5. SIDEWALK TRANSITIONS SHALL BE 6' MINIMUM AND 15' MAXIMUM. IF A TRANSITION HAS REACHED 15' IN LENGTH AND GRADE HAS NOT YET MATCHED EXISTING, RUNNING SLOPE MAY EXCEED 12:1. SIDEWALK TRANSITION PANELS SHALL HAVE A HORIZONTAL TAPER OF 10:1 PREFERRED AND 3:1 MINIMUM. SIDEWALK TRANSITION PANEL SLOPE TRANSITION SHALL BE 0.5%/FT MAXIMUM.
6. IF THE SPACE BETWEEN THE FLOWLINE AND DETECTABLE WARNING SURFACE EXCEEDS 5', THE DETECTABLE WARNING SURFACE SHALL BE PLACED RADIALLY ALONG THE FLOWLINE.
7. ALL GRADE BREAKS SHALL BE PERPENDICULAR TO THE DIRECTION OF TRAVEL.
8. DETECTABLE WARNING SURFACES SHALL BE PER CDOT STANDARD PLAN M–608–1.

The Town of ERIE COLORADO

DRAWING TITLE: CURB RAMP TYPE 4 DETACHED SIDEWALK

DRAWING NUMBER: SW8

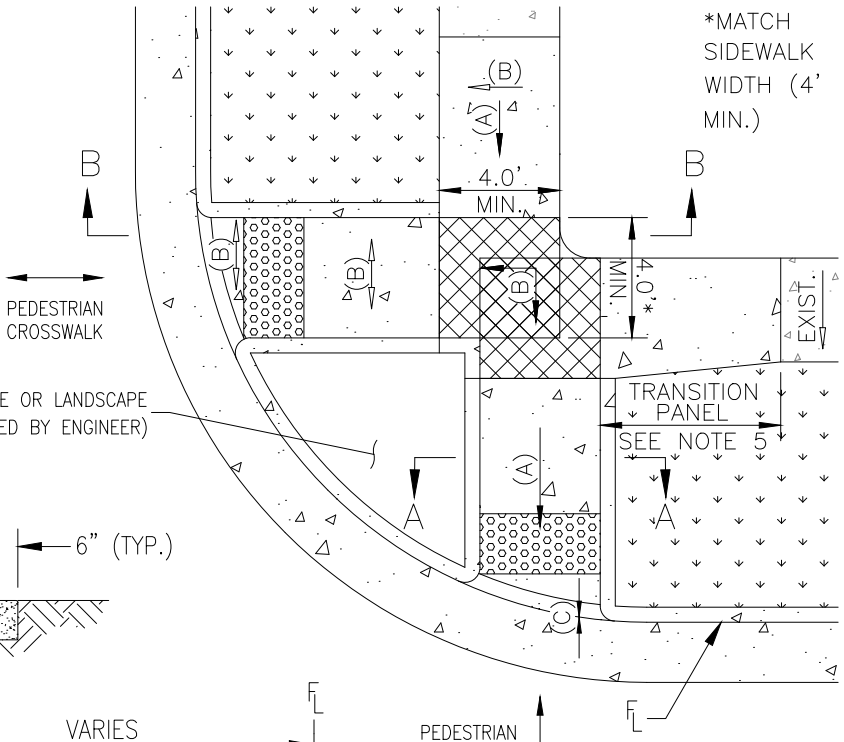
DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 10/2024

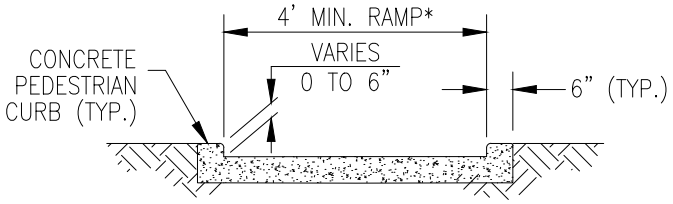
LEGEND

- CURB RAMP PAY AREA
- DETECTABLE WARNING SURFACE
- LANDING AREA
- LANDSCAPE AREA
- RUNNING SLOPE
- CROSS SLOPE

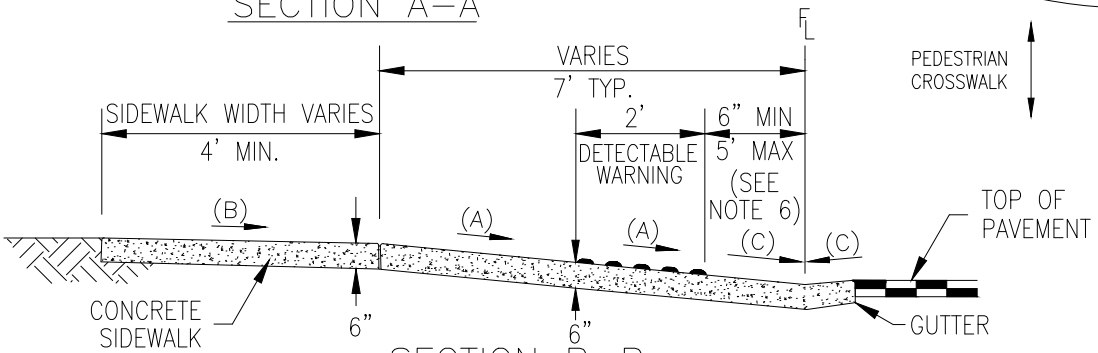
SLOPE LEGEND

(A) 7.8% PREF. (12:1 MAX.)
(B) 1.5% PREF. (48:1 MAX.)
(C) 4.5% PREF. (20:1 MAX.)
(D) 9.5% PREF. (10:1 MAX.)

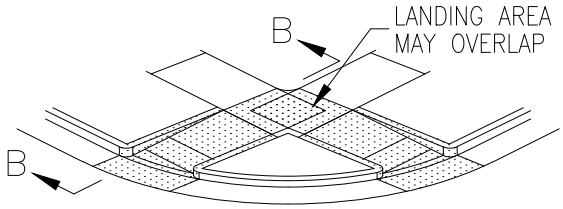




SECTION A–A



SECTION B–B



ISOMETRIC VIEW

NOTES:

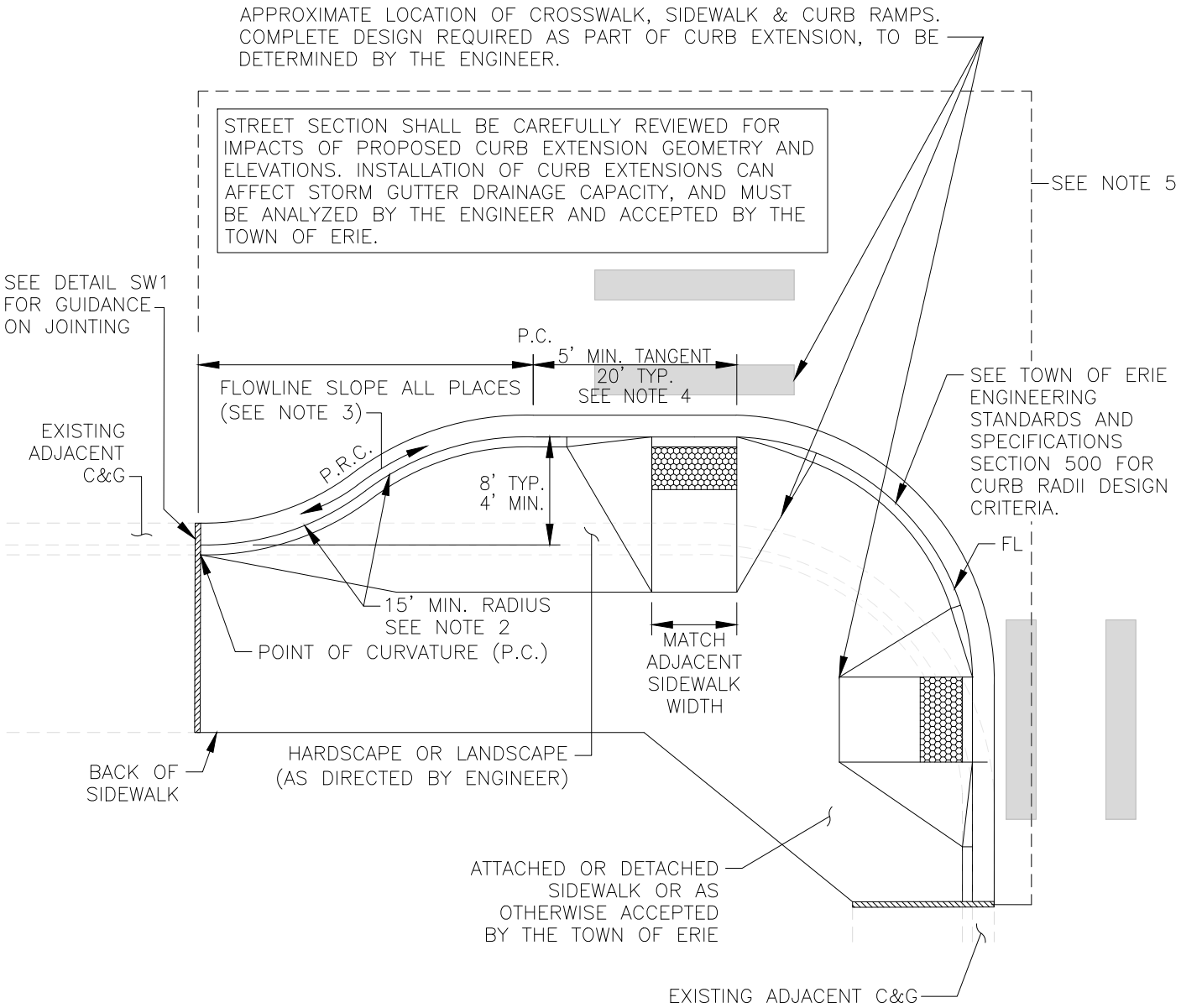
1. CURB RAMPS SHALL BE PROVIDED AT ALL CORNERS OF STREET INTERSECTIONS AND AT "T" INTERSECTIONS WHERE THERE IS EXISTING OR PROPOSED SIDEWALK AND CURB.
2. CURB RAMP SURFACE SHALL HAVE A COARSE BROOM FINISH PERPENDICULAR TO THE DIRECTION OF TRAVEL. THE RAMP AREA SHALL RECEIVE A COARSER BRUSH TREATMENT THAN THE SIDEWALK.
3. CURB RAMPS SHALL BE POURED MONOLITHICALLY WITH THE CURB, GUTTER AND APRON.
4. CURB RAMP DIMENSIONS SHALL BE SPECIFIED ON THE CONSTRUCTIONS PLANS.
5. SIDEWALK TRANSITIONS SHALL BE 6' MINIMUM AND 15' MAXIMUM. IF A TRANSITION HAS REACHED 15' IN LENGTH AND GRADE HAS NOT YET MATCHED EXISTING, RUNNING SLOPE MAY EXCEED 12:1. SIDEWALK TRANSITION PANELS SHALL HAVE A HORIZONTAL TAPER OF 10:1 PREFERRED AND 3:1 MINIMUM. SIDEWALK TRANSITION PANEL SLOPE TRANSITION SHALL BE 0.5%/FT MAXIMUM.
6. IF THE SPACE BETWEEN THE FLOWLINE AND DETECTABLE WARNING SURFACE EXCEEDS 5', THE DETECTABLE WARNING SURFACE SHALL BE PLACED RADIALLY ALONG THE FLOWLINE.
7. CONSTRUCTION OF THE CONCRETE PEDESTRIAN CURB SHALL BE INCLUDED IN THE COST OF THE CURB RAMP.
8. WINGED CURB RAMPS, LIKE CURB RAMP TYPE 4, ARE PREFERRED WHERE PEDESTRIAN ACTIVITY IS LIKELY ADJACENT TO THE CURB RAMP AND THERE IS NO OBSTACLE.
9. ALL GRADE BREAKS SHALL BE PERPENDICULAR TO THE DIRECTION OF TRAVEL.
10. DETECTABLE WARNING SURFACES SHALL BE PER CDOT STANDARD PLAN M–608–1.

The Town of ERIE COLORADO

DRAWING TITLE: CURB RAMP TYPE 3 DETACHED SIDEWALK

DRAWING NUMBER: SW9

DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 10/2024



NOTES:

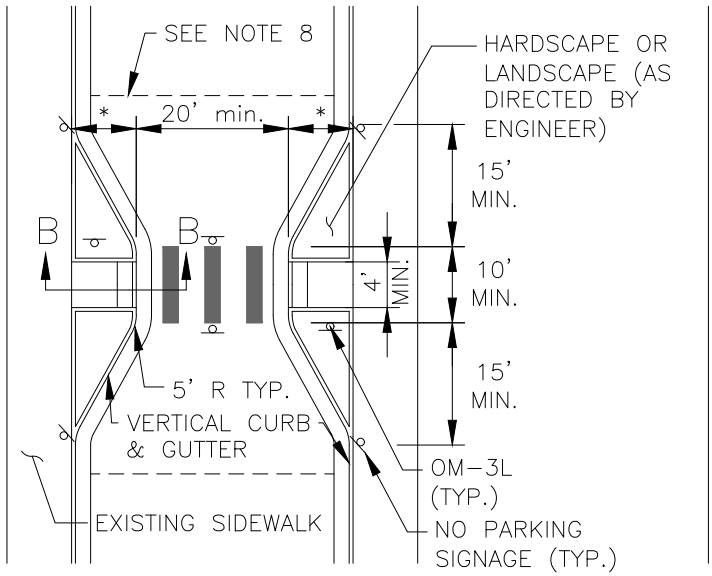
1. RETROFIT CONDITION FOR CURB EXTENSION IN ONE DIRECTION IS SHOWN. CURB EXTENSION MAY BE APPLIED TO BOTH SIDES OF CORNER AND IN NEW DESIGN WITH THE SAME REQUIREMENTS AS SHOWN.
2. THE REVERSE CURVES AT THE END OF THE CURB EXTENSION SHALL BE TANGENT TO EACH OTHER, AND EACH CURVE SHALL BE TANGENT WITH THE CURB LINE CONTINUING IN EACH DIRECTION.
3. PROVIDE POSITIVE 0.7% PREFERRED, 0.5% MIN, SLOPE ALONG THE FLOWLINE OF THE CURB EXTENSION. NEW INLETS AND STORM DRAIN MAY BE REQUIRED.
4. IF THERE IS AN EXISTING OR PROPOSED BUS STOP OR DRIVEWAY AT THE CORNER, THE LENGTH OF THE TANGENT SHOULD BE INCREASED TO ACCOMMODATE THE FULL LENGTH OF THE BUS STOP/DRIVEWAY.
5. LIMITS OF STREET CUT AND PATCHING SHALL BE SET TO PROVIDE AN ADA ACCESSIBLE PEDESTRIAN ACCESS ROUTE (PAR) WITHIN THE CROSSWALK THAT DOES NOT RESULT IN ANY PAR DESIGN ELEMENTS BEING EXCEEDED. IN SOME CASES, RECONSTRUCTION OF ENTIRE STREET FROM CURB TO CURB MAY BE REQUIRED.

The Town of ERIE COLORADO

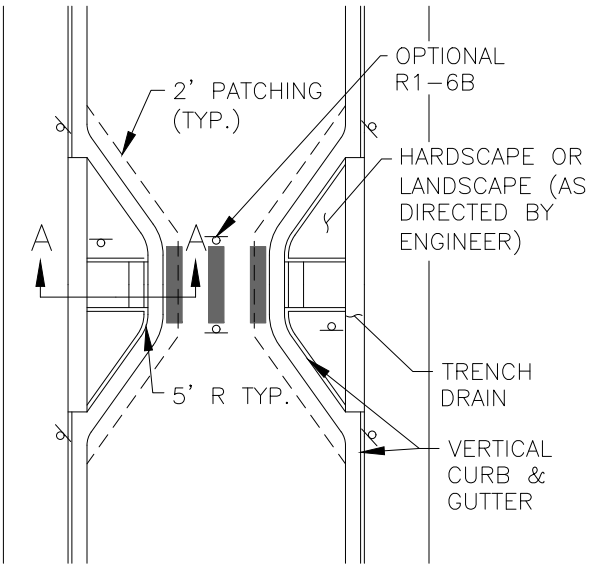
DRAWING TITLE: CONCRETE CURB EXTENSION

DRAWING NUMBER: SM1

DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 02/2025

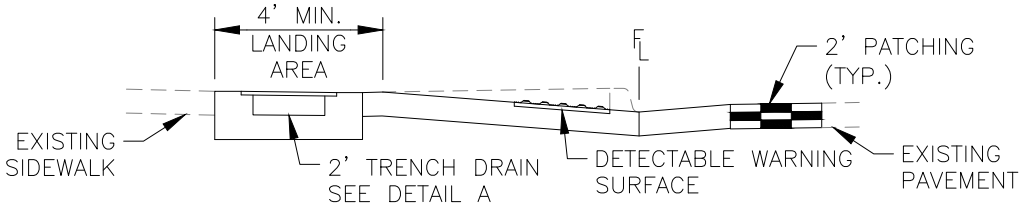


MIDBLOCK CURB EXTENSION

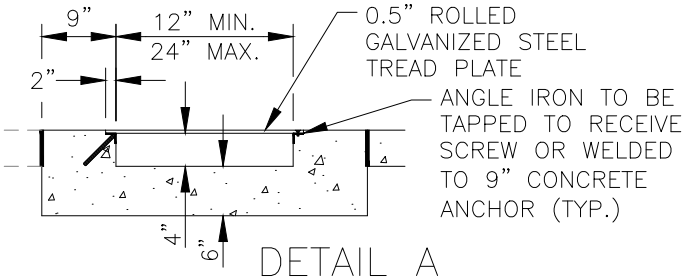


MIDBLOCK CURB EXTENSION (TRENCH DRAIN)

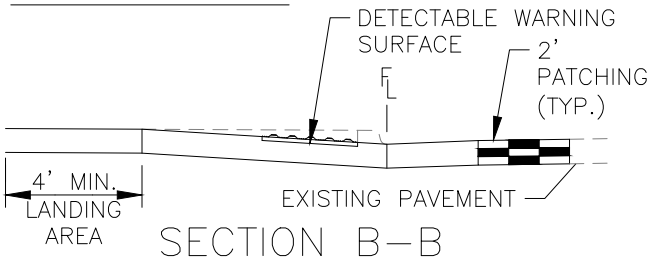
*MUST ACCOMODATE CURB RAMP LENGTH. 4' MIN.



SECTION A-A



DETAIL A



SECTION B-B

NOTES:

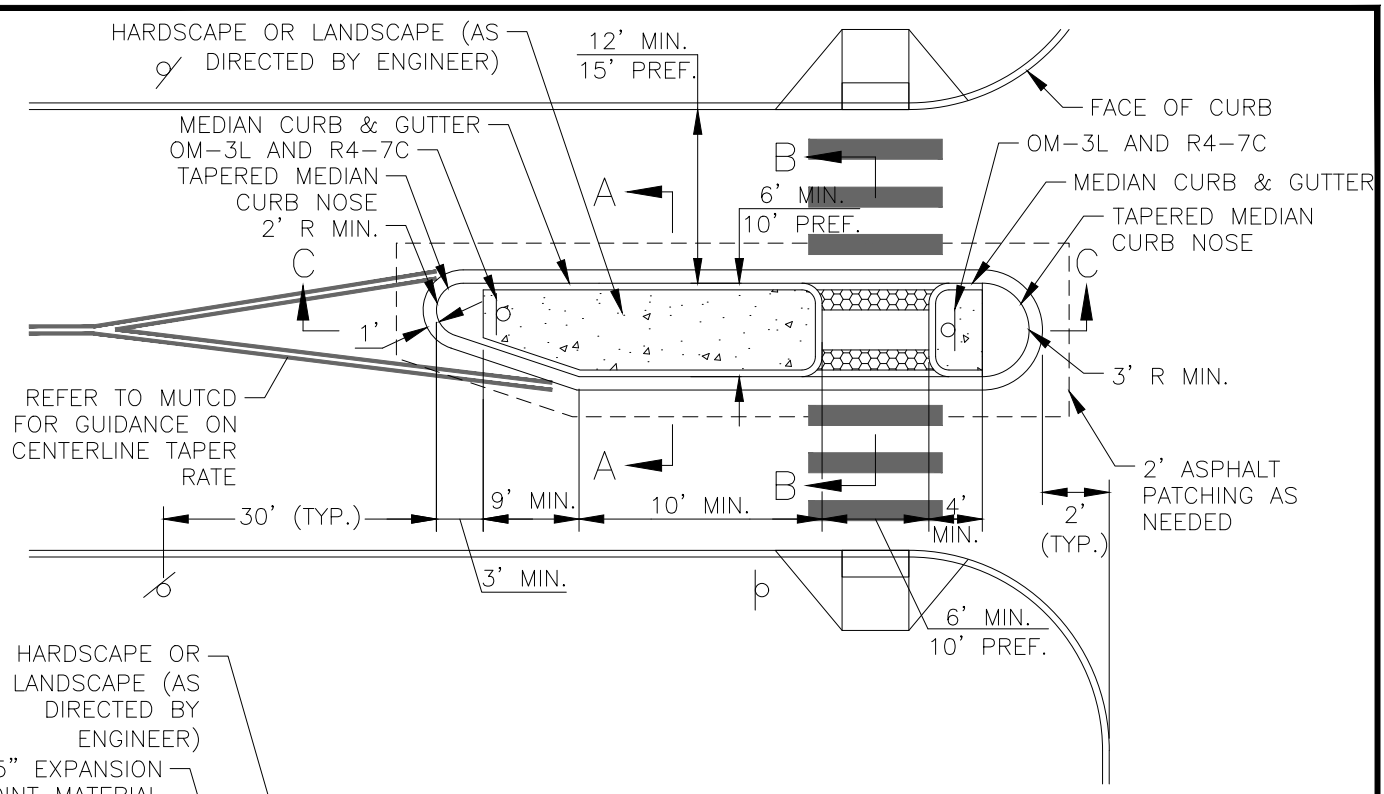
1. SEE DETAIL SW9 FOR CURB RAMP REQUIREMENTS.
2. WITHOUT TOWN OF ERIE ACCEPTANCE, MIDBLOCK CURB EXTENSIONS SHALL NOT BE INSTALLED WITHIN:
   - 20' OF RESIDENTIAL DRIVEWAYS OR ALLEY CURB CUTS.
   - 100' OF AN INTERSECTION
   - 10' OF FIRE HYDRANTS.
3. STREET SECTION SHALL BE CAREFULLY REVIEWED FOR IMPACTS OF PROPOSED CURB EXTENSION GEOMETRY AND ELEVATIONS. INSTALLATION OF CURB EXTENSIONS CAN AFFECT STORM GUTTER DRAINAGE CAPACITY, AND MUST BE ANALYZED BY THE ENGINEER AND ACCEPTED BY THE TOWN OF ERIE.
4. PROVIDE POSITIVE 0.7% PREFERRED, 0.5% MIN, SLOPE ALONG THE FLOWLINE OF THE MIDBLOCK CURB EXTENSION. NEW INLETS AND STORM DRAIN MAY BE REQUIRED.
5. MIDBLOCK CURB EXTENSIONS MAY BE LANDSCAPED WITH ACCEPTANCE FROM TOWN OF ERIE. PLANTINGS SHALL BE KEPT UNDER 3' IN HEIGHT AND NOT OBSTRUCT THE VIEW OF CROSSING PEDESTRIANS OR DOWNSTREAM VEHICLES.
6. MIDBLOCK CURB EXTENSION (TRENCH DRAIN) MAY ONLY BE INSTALLED WITH TOWN OF ERIE ACCEPTANCE AND MAINTENANCE PLAN DETERMINED.
7. RAISED CROSSWALK MAY BE CONSIDERED IF DRAINAGE IS ADDRESSED. RAISED CROSSWALK GEOMETRY AND SIGNAGE SHALL BE ACCEPTED BY THE TOWN OF ERIE.
8. LIMITS OF STREET CUT AND PATCHING SHALL BE SET TO PROVIDE AN ADA ACCESSIBLE PEDESTRIAN ACCESS ROUTE (PAR) WITHIN THE CROSSWALK THAT DOES NOT RESULT IN ANY PAR DESIGN ELEMENTS BEING EXCEEDED. IN SOME CASES, RECONSTRUCTION OF ENTIRE STREET FROM CURB TO CURB MAY BE REQUIRED.

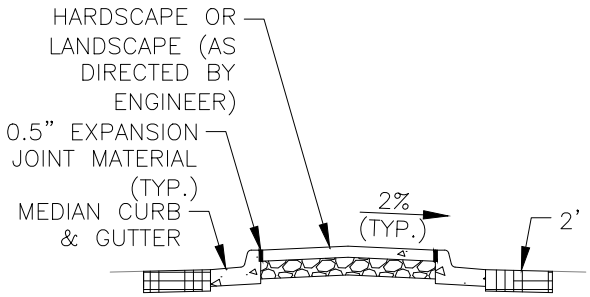
The Town of ERIE COLORADO

DRAWING TITLE: MIDBLOCK CURB EXTENSION

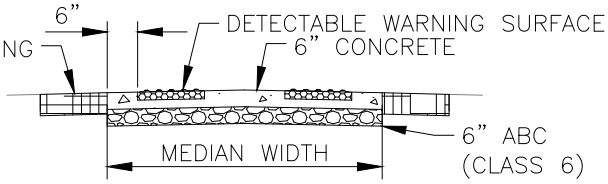
DRAWING NUMBER: SM2

DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 02/2025

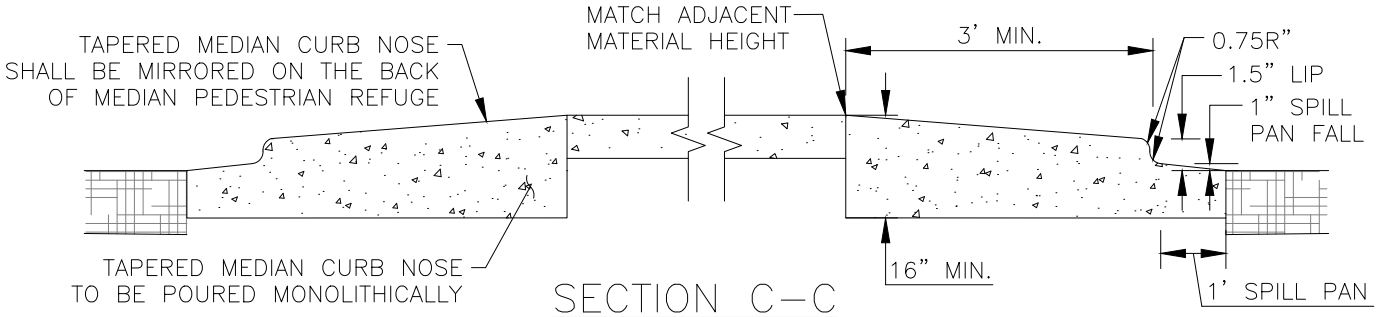




SECTION A-A



SECTION B-B



SECTION C-C

NOTES:

1. WITHIN MEDIAN PEDESTRAIN REFUGE, NO SLOPE SHALL EXCEED 1.5% IN ANY DIRECTION, INCLUSIVE OF THE DETECTABLE WARNING SURFACE AREAS AND THE SPACE BETWEEN.
2. INSTALL 4" PVC SOCKETS FOR SIGNAGE.
3. BEGIN MEDIAN NOSE AT 2' SETBACK FROM CONTINUATION OF FLOWLINES ACROSS SIDE STREET AS SHOWN.
4. PEDESTRIAN REFUGE WIDTH SHALL MATCH THE MEDIAN WIDTH. THE MINIMUM MEDIAN WIDTH IS 6'. THE MINIMUM RAMP WIDTH IS 6' OR SHALL MATCH THE ADJACENT CURB RAMPS AND SIDEWALK/PATH WIDTH.
5. DETECTABLE WARNING SURFACES SHALL BE INCLUDED ON BOTH SIDES OF THE MEDIAN.
6. REFER TO THE MUTCD FOR GUIDANCE ON SIGNING AND STRIPING.
7. FOR A MIDBLOCK MEDIAN REFUGE ISLAND, MIRROR THE APPROACH SIDE TREATMENT (NON-INTERSECTION SIDE). IN SITING A MIDBLOCK PEDESTRIAN REFUGE, CONSIDER EXISTING LIGHTING, ADDITION OF IMPROVED CROSSING SIGNAGE AND SIGNALIZATION.
8. ANY LANE TAPER SHALL MEET MUTCD REQUIREMENTS BASED ON DESIGN SPEED.
9. MEDIAN NOSE MAY BE EXCLUDED WITH TOWN OF ERIE ACCEPTANCE.

The Town of ERIE COLORADO

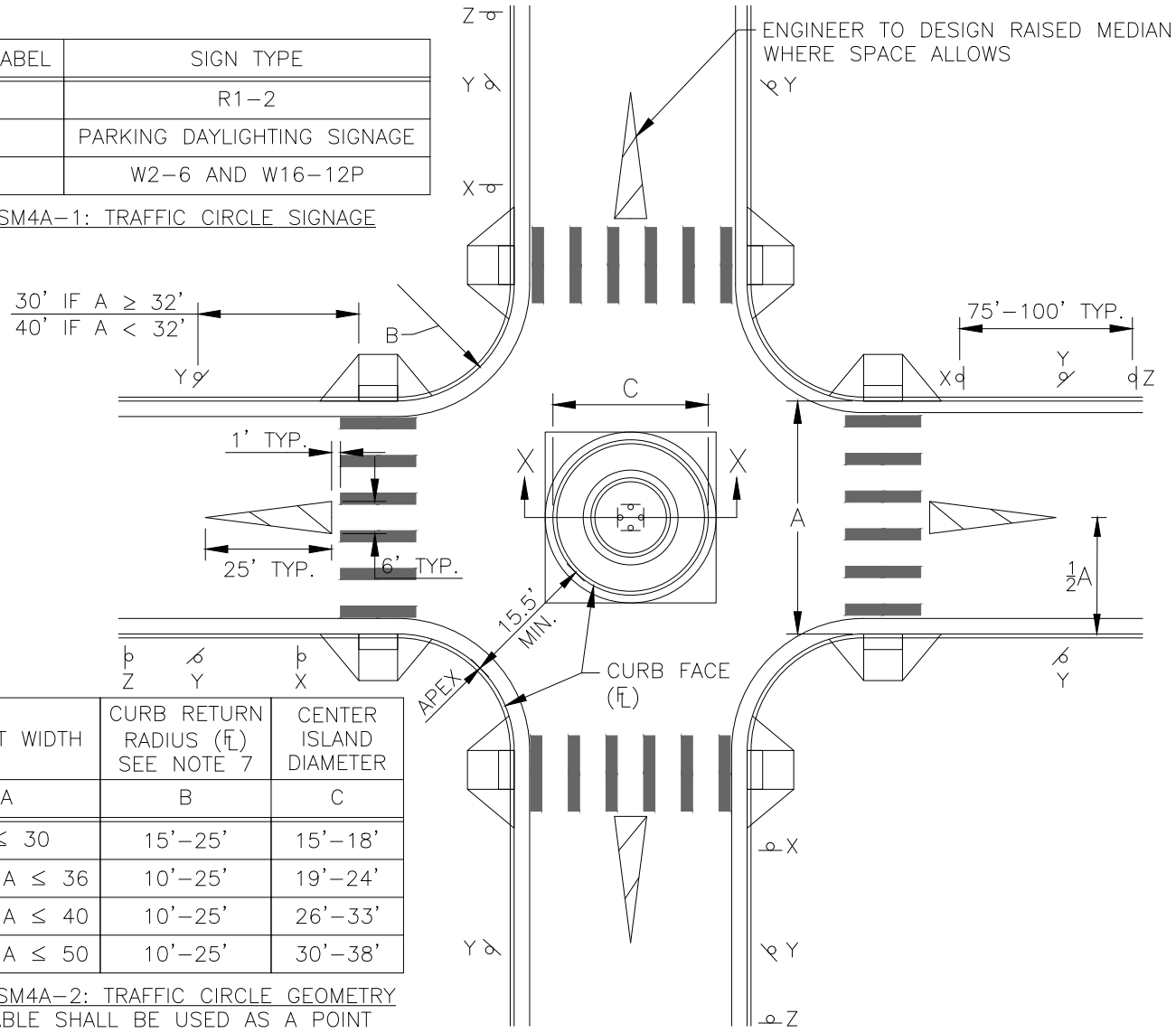
DRAWING TITLE: MEDIAN PEDESTRIAN REFUGE

DRAWING NUMBER: SM3

DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 02/2025

SIGN LABEL	SIGN TYPE
X	R1–2
Y	PARKING DAYLIGHTING SIGNAGE
Z	W2–6 AND W16–12P

TABLE SM4A–1: TRAFFIC CIRCLE SIGNAGE



STREET WIDTH	CURB RETURN RADIUS (℄) SEE NOTE 7	CENTER ISLAND DIAMETER
A	B	C
A ≤ 30	15'–25'	15'–18'
30' < A ≤ 36	10'–25'	19'–24'
36' < A ≤ 40	10'–25'	26'–33'
40' < A ≤ 50	10'–25'	30'–38'

TABLE SM4A–2: TRAFFIC CIRCLE GEOMETRY
THIS TABLE SHALL BE USED AS A POINT OF REFERENCE. DESIGNERS SHALL CONFIRM THAT THE DESIGN VEHICLE IS NOT HINDERED BY THE INSTALLATION OF A TRAFFIC CIRCLE.

NEIGHBORHOOD TRAFFIC CIRCLE

NOTES:

1. TRAFFIC CIRCLES SHALL NOT BE INSTALLED WHERE THE PARALLEL DESIGN VEHICLE PATH ENCROACHES INTO THE CENTERLINE OF A VALLEY GUTTER (IF PRESENT).
2. TRAFFIC CIRCLES SHALL NOT BE INSTALLED WITH DIAGONAL OR APEX PEDESTRIAN CURB RAMPS.
3. WHERE THE INTERSECTING STREETS HAVE DIFFERING WIDTHS, OR WHERE THE INTERSECTING STREETS ARE OFFSET, AN OVAL–SHAPED CENTER ISLAND CAN BE USED; CURB EXTENSIONS MAY ALSO BE USED TO NARROW ONE OF THE INTERSECTING STREETS.
4. THE AREA BETWEEN THE APEX OF THE CURB RETURN AND THE TRAFFIC CIRCLE SHALL BE DESIGNED TO A MAXIMUM SLOPE OF 5%. IN RETROFIT SITUATIONS, THE SLOPE SHALL BE A MAXIMUM OF 12%.
5. IF APEX INLETS ARE PRESENT, THE 15.5' CURB FACE TO TRAFFIC CIRCLE MEASUREMENT SHALL BE INCREASED TO 17.5' MINIMUM.
6. THIS DETAIL SHOULD NOT BE USED ON STREETS WIDER THAN 50' OR STREETS HAVING MORE THAN ONE THROUGH LANE IN ANY DIRECTION SERVED BY THE TRAFFIC CIRCLE.
7. REFER TO SECTION 500 OF THE TOWN OF ERIE ENGINEERING STANDARDS AND SPECIFICATIONS FOR DESIGN VEHICLE INFORMATION.
8. IF A TRAFFIC CIRCLE SERVES A BIKE ROUTE, BIKES AND TRAVEL LANES SHOULD BE MERGED PRIOR TO THE INTERSECTION AND DETAILED ENGINEERING SHOULD BE COMPLETED FOR TOWN OF ERIE ACCEPTANCE.

CONTINUED ON SM4B...

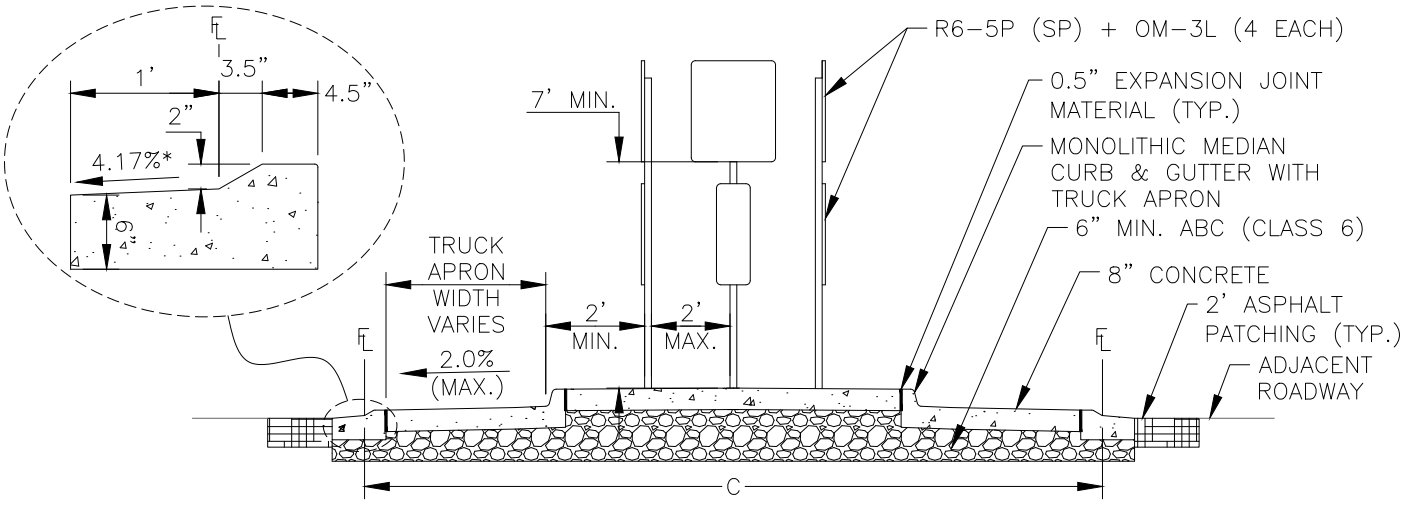
The Town of ERIE COLORADO

DRAWING TITLE: NEIGHBORHOOD TRAFFIC CIRCLE

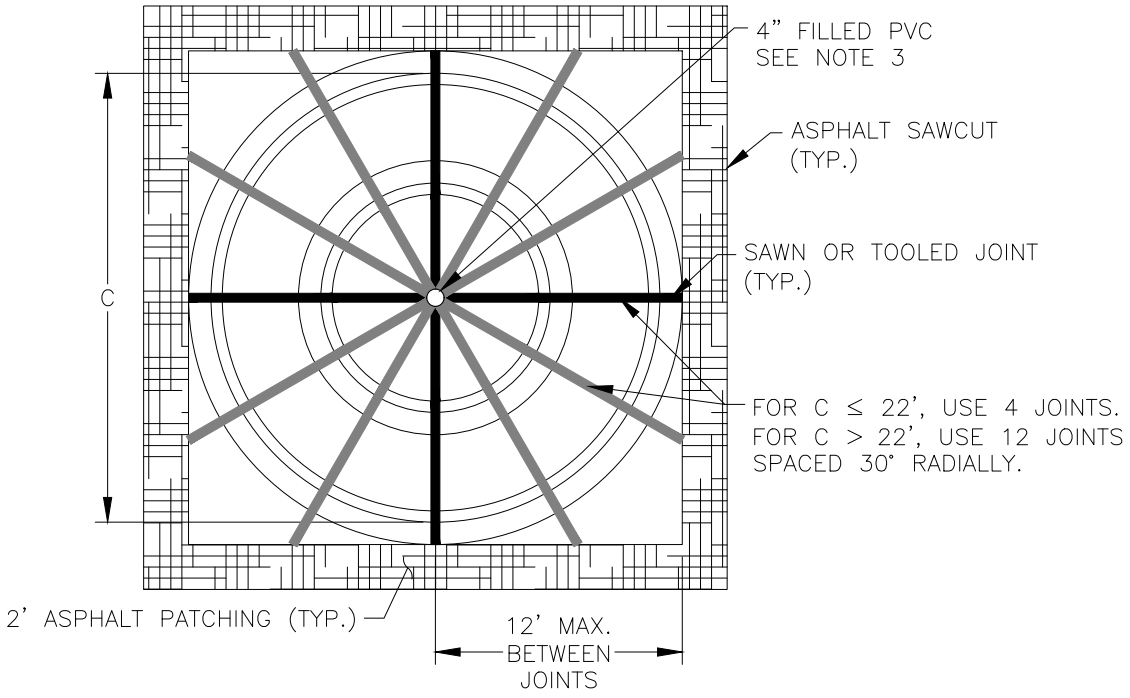
DRAWING NUMBER: SM4A

DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 02/2025

...CONTINUED FROM SM4A



SECTION X-X



TYPICAL JOINTING PLAN

NOTES:

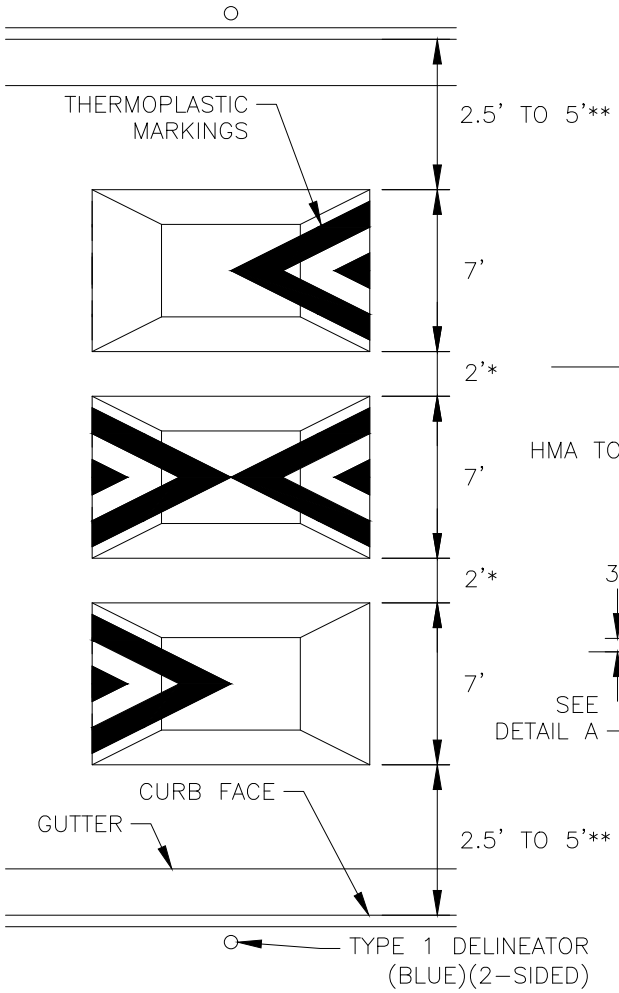
1. TRUCK APRONS SHOULD BE APPLIED AS NEEDED GIVEN THE DESIGN AND CONTROL VEHICLES SELECTED FOR THE LARGER INTERSECTING STREET.
2. TRAFFIC CIRCLES MAY BE LANDSCAPED WITH ACCEPTANCE FROM THE TOWN OF ERIE. LANDSCAPING SHALL FOLLOW THE TOWN OF ERIE ENGINEERING STANDARDS AND SPECIFICATIONS SECTION 1200.
3. WHERE JOINTS MEET AT AN ACUTE ANGLE AND AT THE CENTER OF THE CIRCLE, A 4" PVC PIPE SHALL BE INSTALLED AS A TERMINATION CORE TO PREVENT CRACKING. THE INSIDE OF THE PVC PIPE SHALL BE COATED WITH BOND BREAKER AND FILLED WITH NON-SHRINK GROUT.

The Town of ERIE COLORADO

DRAWING TITLE: NEIGHBORHOOD TRAFFIC CIRCLE

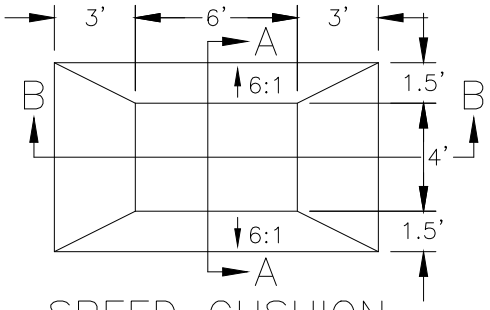
DRAWING NUMBER: SM4B

DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 02/2025

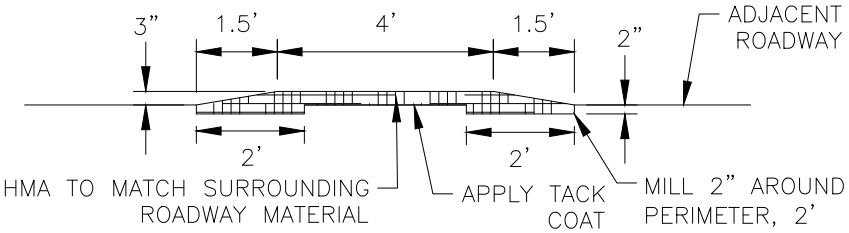


PLACEMENT DETAIL

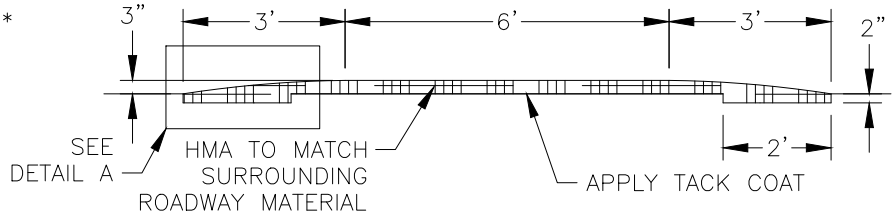
*SEE NOTE 3
**SEE NOTE 4



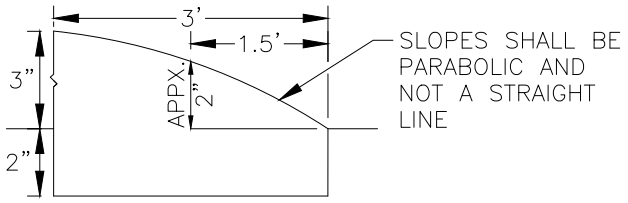
SPEED CUSHION



SECTION A-A



SECTION B-B



DETAIL A

NOTES:

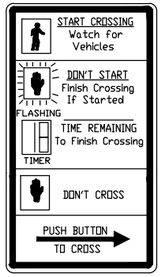
1. DETAIL SHOWS SPEED CUSHIONS ADDED TO EXISTING PAVEMENT. NEW CONSTRUCTION AND 2" OR GREATER OVERLAYS MAY OMIT THE 2' X 2" KEYS IF ASPHALT CUSHIONS ARE PLACED WITHIN 24 HOURS OF FINAL LIFT.
2. DETAIL FOR USE ON ASPHALT ROADWAYS ONLY.
3. IF CURB-TO-CURB WIDTH IS GREATER THAN 42', INCREASE WIDTH BETWEEN SPEED CUSHIONS FROM 2' TO 3'.
4. IF ON-STREET PARKING, A BIKE LANE, OR SHOULDER AREAS CREATE A CLEAR WIDTH GREATER THAN 5', DESIGNER SHALL CONSIDER CURB EXTENSIONS OR OTHER METHODS OF REDUCING CLEAR WIDTH.
5. SPEED CUSHIONS SHALL NOT BE PLACED OVER MANHOLES, WATER VALVES, OTHER UTILITY APPURTENANCES, OR SURVEY MONUMENTS.
6. SPEED CUSHIONS SHALL NOT BE INSTALLED WITHIN 20' OF A DRIVEWAY OR CURB CUT OR WITHIN 100' OF AN INTERSECTION WITHOUT ACCEPTANCE FROM THE TOWN OF ERIE.
7. W17-1 SIGNS SHALL BE INSTALLED 20' TO 50' ADVANCE OF SPEED CUSHIONS.
8. SPEED CUSHIONS ARE TO BE CONSTRUCTED BETWEEN 3" AND 3.5" IN HEIGHT. BECAUSE SOME SETTLEMENT IS NORMAL, IT IS PREFERABLE FOR THE INITIAL HEIGHT TO BE MID-RANGE.
9. SPEED CUSHIONS SHALL BE INSTALLED WITH SX(75) PG 64-22 UNLESS OTHERWISE ACCEPTED BY THE TOWN OF ERIE. TACK COAT SHALL BE APPLIED PRIOR TO APPLICATION OF ASPHALT.
10. SPEED CUSHIONS SHALL NOT BE PLACED ALONG A CURVE OR WHERE ROADWAY RUNNING GRADE EXCEEDS 8%.
11. ASPHALT APPLICATION SHALL INCLUDE SEALANT AT THE EDGES.

The Town of ERIE COLORADO

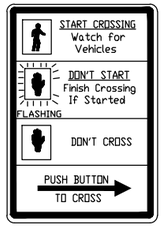
DRAWING TITLE: SPEED CUSHION

DRAWING NUMBER: SM5

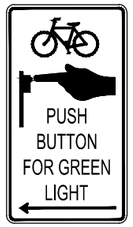
DRAWN BY: G. PRINCE APPROVED BY: D. PASIC DATE: 02/2025



R10-3e
9″x15″
COUNTDOWN



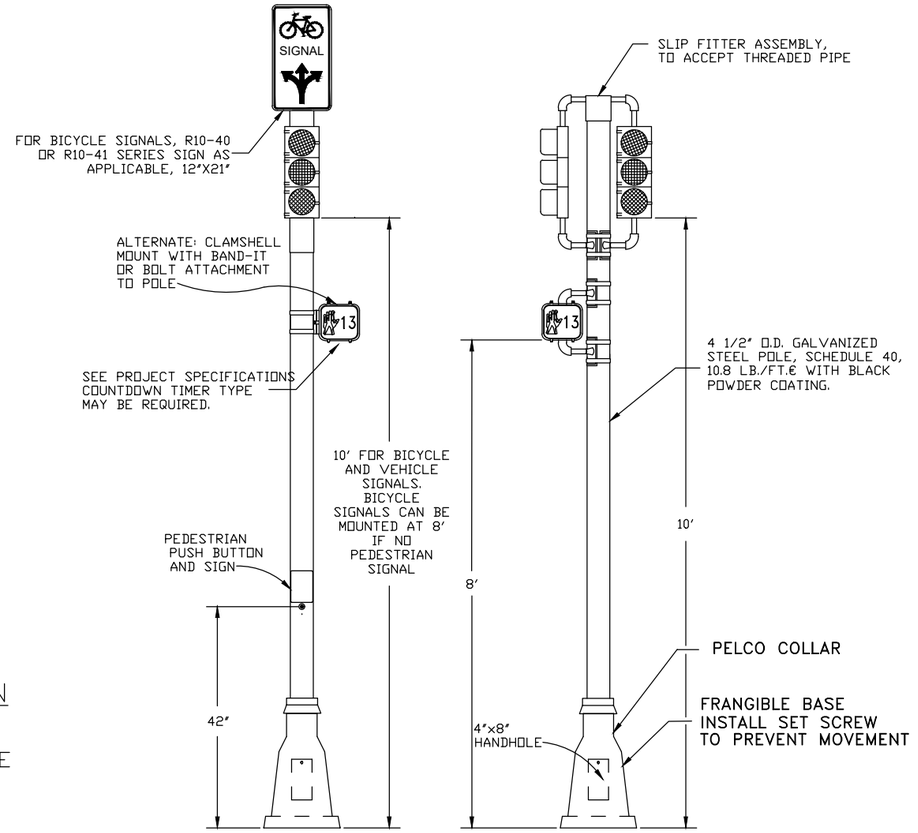
R10-3b
9″x12″
NON-COUNTDOWN



R10-26b
9″x15″

TYPICAL BICYCLE AND PEDESTRIAN PUSH-BUTTON SIGNS

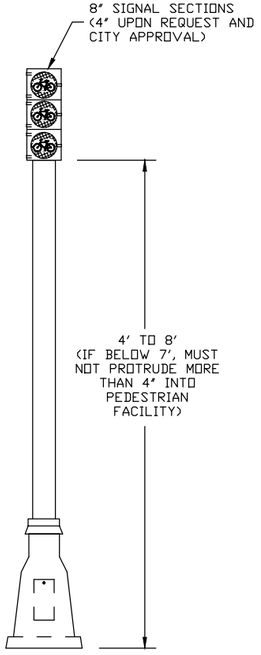
SIGN SHALL BE LABEL (STICK-ON) TYPE



PEDESTAL POLE SHALL BE HOT DIPPED GALVANIZED PER ASTM A123, EQUIVALENT TO 2 OZ. PER SQUARE FOOT, INSIDE AND OUT

TYPICAL PEDESTAL POLE DETAIL

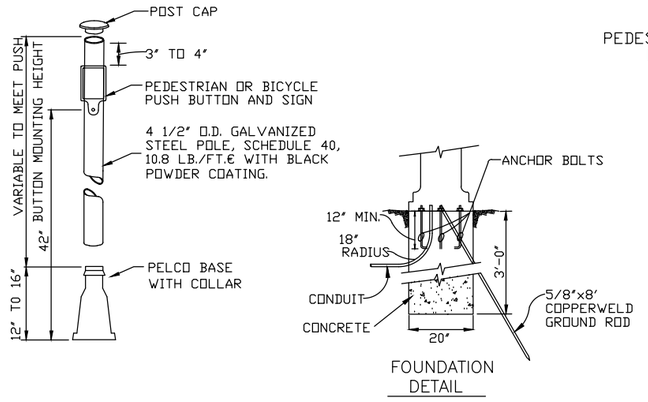
1/4" SPLIT PIN SHALL BE INSTALLED IN THE UPPER PORTION OF THE ALUMINUM BASE AND SHALL COMPLETELY PENETRATE BASE AND POLE TO SECURE POLE TO PREVENT MOVEMENT OR TWISTING. PELCO COLLAR TO BE INSTALLED.



NEAR-SIDE SUPPLEMENTAL BIKE SIGNAL DETAIL

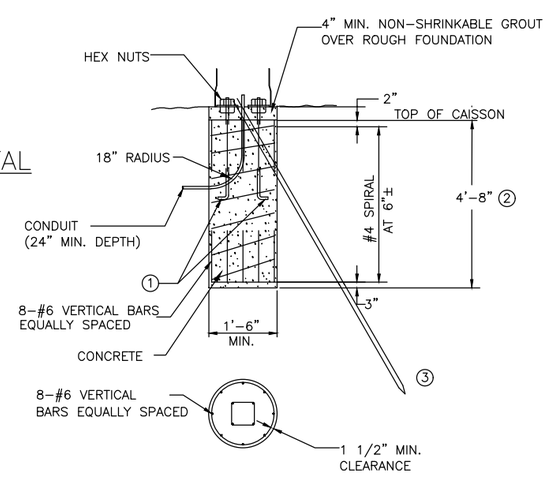
FOOTING NOTES

1. ANCHOR BOLTS (FURNISHED WITH POLE) PER MANUFACTURER'S TEMPLATE.
2. CAISSON DESIGNS REQUIRE THAT THE CAISSON BE FOUNDED IN COMPACT SAND, CLAY OR SANDY CLAY. IF, BY VISUAL INSPECTION OF THE HOLE' OTHER MATERIAL IS PRESENT, THE CAISSON DESIGN SHALL BE MODIFIED AND APPROVED BY THE CONSTRUCTION MANAGER.
3. 5/8"x8' COPPERWELD GROUND ROD THROUGH FOUNDATION, INTO GROUND.
4. HAND HOLE SHALL BE PROVIDED.



FOUNDATION DETAIL

PEDESTRIAN OR BICYCLE PUSH BUTTON POLE



TYPICAL PEDESTAL POLE FOUNDATION

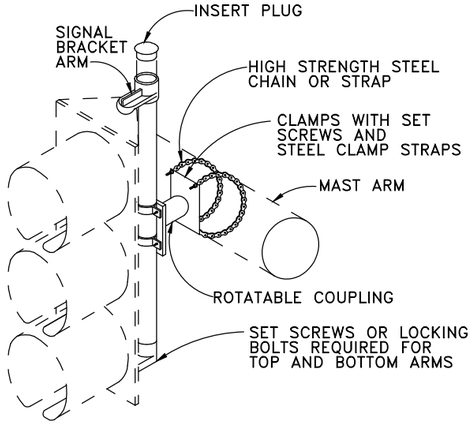
(CAST IN PLACE)

The Town of ERIE COLORADO

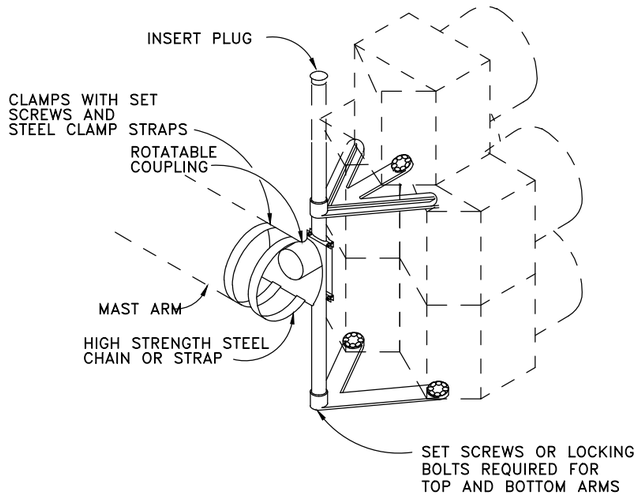
DRAWING TITLE: PEDESTRIAN POLE / BICYCLE POLE

DRAWING NUMBER: TS-7

DRAWN BY: E.G. APPROVED BY: D. PASIC DATE: 9/2024



DETAIL OF MAST ARM MOUNTING FOR IN-LINE SIGNAL HEAD (3-SECTION OR 4-SECTION)



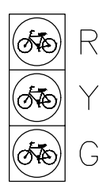
DETAIL OF MAST ARM MOUNTING FOR DOGHOUSE SIGNAL HEAD (5-SECTION)

NOTES

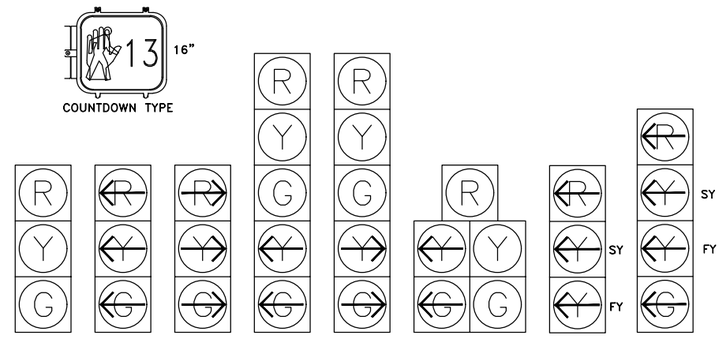
ALL VEHICLE SIGNAL HEADS AND FAR-SIDE PRIMARY BICYCLE SIGNAL HEADS SHALL BE POLYCARBONATE WITH 12" SECTIONS AND TUNNEL VISORS.

NEAR-SIDE SUPPLEMENTAL BICYCLE SIGNAL HEADS SHALL BE POLYCARBONATE WITH 8" SECTIONS AND TUNNEL VISORS. UPON CITY REVIEW AND APPROVAL, 4" SIGNAL SECTIONS WITHOUT VISORS MAY BE CONSIDERED.

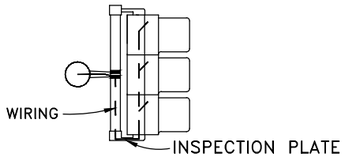
ALL VEHICLE, BICYCLE AND PEDESTRIAN SIGNAL HEADS SHALL BE LED TYPE, EXCEPT FOR ONE SIDE OF POLE RED WHICH SHALL BE INCANDESCENT. HEADS SHALL BE BLACK IN COLOR. PEDESTRIAN HEADS SHALL BE COUNTDOWN TYPE.



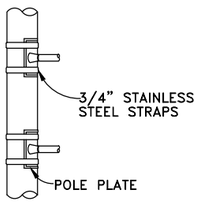
BIKE SIGNAL HEAD



PEDESTRIAN AND VEHICLE SIGNAL HEADS



WIRING DIAGRAM



TYPICAL SIDE OF POLE SIGNAL MOUNTING

MOUNTING NOTES

1. PIPE COUPLINGS FOR SIGNAL BRACKETS SHALL BE EITHER 1-1/2 OR 2 INCH DEPENDING UPON THE SIGNAL HEAD TO BE INSTALLED. SIGNAL BRACKETS SHALL BE FURNISHED BY THE MANUFACTURER OF THE SIGNAL HEADS.
2. UNLESS OTHERWISE SPECIFIED, ALL TRAFFIC SIGNALS MOUNTED ABOVE THE ROADWAY SHALL HAVE A HEIGHT OF 17' TO 19' ABOVE THE PAVEMENT GRADE AT THE ROADWAY CENTER, ALL SIDE-OF-POLE MOUNTED VEHICLE TRAFFIC SIGNALS SHALL HAVE A HEIGHT OF 10' ABOVE GROUND LINE. PEDESTRIAN AND BICYCLE SIGNALS SHALL HAVE A HEIGHT OF 8' ABOVE GROUND LINE AS MEASURED TO THE BOTTOM OF THE SIGNAL HEAD HOUSING OR BRACKET. NEAR-SIDE BICYCLE SIGNALS MAY HAVE A LOWER MOUNTING HEIGHT; SEE TS-7 FOR DETAILS.
3. MAST ARM MOUNTED SIGNAL HEADS SHALL USE ASTRO-BRAC's OR SKY-BRACK. ALL SIGNAL HEADS SHALL BE MOUNTED IN SUCH A MANNER AS TO BE EASILY REMOVED FROM THEIR SUPPORTING STRUCTURE.
4. GASKET SEALING COMPOUND SHALL BE USED IN ADDITION TO ANY LEAD WASHERS REQUIRED FOR CREATING A WATER-TIGHT CONNECTION BETWEEN THE SIGNAL HEAD AND MOUNTING BRACKET.
5. SIGNAL HEADS SHALL BE SECURELY AFFIXED BY USE OF A SERRATED COUPLING OR OTHER ACCESSORIES RECOMMENDED BY THE SIGNAL MANUFACTURER.
6. WIRING FROM INSIDE MAST ARM THROUGH A 1" FIELD DRILLED HOLE IN ARM SHALL BE BROUGHT THROUGH THE MOUNTING SUPPORT TUBE AND LOWER ARM (AS SHOWN). FIELD DRILLED HOLES SHALL HAVE RUBBER GROMMETS INSTALLED.

GENERAL WIRING NOTES

1. TRAFFIC SIGNAL CONDUIT SHALL NOT CARRY WIRING OF OTHER UTILITIES.
2. EXCEPT FOR LOOP DETECTOR LEADS, ALL SPLICES SHALL BE IN HAND HOLES AT POLE BASES AND NOT IN PULL BOXES.
3. PEDESTRIAN AND VEHICLE SIGNAL HEADS SHALL BE INDIVIDUALLY WIRED FROM THE POLE BASE TO THE SIGNAL HEAD.
4. CONTRACTOR SHALL PROVIDE 2 WIRING DIAGRAMS OF THE SIGNAL INSTALLATION TO THE TOWN.
5. UNLESS ALLOWED BY THE PUBLIC WORKS DIRECTOR, WIRE SHALL NOT OCCUPY MORE THAN 40% OF THE INSIDE AREA OF CONDUIT.

The Town of ERIE COLORADO

DRAWING TITLE: SIGNAL HEAD MOUNTING

DRAWING NUMBER: TS-9

DRAWN BY: E.G. APPROVED BY: D. PASIC DATE: 9/2024