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LOT 1 - ERIE AIR PARK REPLAT D

FINAL CONSTRUCTION PLANS

LOCATED IN THE NORTHWEST QUARTER OF SECTION 31, TOWNSHIP 1 NORTH RANGE 68 WEST OF THE 6TH P.M.
TOWN OF ERIE, COUNTY OF WELD, STATE OF COLORADO

AREA = 10.3405 ACRES
SITE PLAN - SP-001595-2023

GENERAL NOTES

1.	THESE PLANS ARE BASED ON THE ALTA/NSPS LAND TITLE SURVEY PREPARED BY SCOTT, COX & ASSOCIATES, INC., DATED 04/20/18, PROJECT NO. 17510. REFERENCE THE ERIE AIRPARK TRACT E MINOR SUBDIVISION PLAT, RECORDED 03/22/19 AT RECEPTION NO. 4475348.
2	THE BASIS OF BEARINGS IS THE NORTH LINE OF THE SW 1/4 OF SECTION 31, T1N, R68W OF THE 6TH P.M. BETWEEN THE FOUND MONUMENTS SHOWN HEREON AND BEARS S89°35'27"E, PER ERIE AIR PARK SUBDIVISION PLAT.
3	LAND TITLE GUARANTY COMPANY/OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY ORDER NO. FCC25152287-10 EFFECTIVE DATE 03/29/2019 AT 5:00 P.M. WAS RELIED UPON FOR INFORMATION REGARDING RIGHTS-OF-WAY, EASEMENTS AND ENCUMBRANCES IN PREPARATION OF THIS PLAT.
4	PORTIONS OF THE PROPERTY LIE WITHIN SPECIAL FLOOD HAZARD AREAS (SFHAS) SUBJECT TO INUNDATION BY THE 1 % ANNUAL CHANCE FLOOD. THE 1% ANNUAL CHANCE FLOOD (100-YEAR FLOOD), ALSO KNOWN AS THE BASE FLOOD, IS THE FLOOD THAT HAS A 1% CHANCE OF BEING EQUALLED OR EXCEEDED IN ANY GIVEN YEAR. THE SPECIAL FLOOD HAZARD AREA IS THE AREA SUBJECT TO FLOODING BY THE 1% ANNUAL CHANCE FLOOD. ZONE AE =BASE FLOOD ELEVATIONS DETERMINED. THE BASE FLOOD IS THE WATER-SURFACE ELEVATION OF THE 1% CHANCE FLOOD. ZONE X (SHADED) =AREAS OF 0.2% ANNUAL CHANCE FLOOD. SFHAS ARE SHOWN PER FLOOD INSURANCE RATE MAP (FIRM) MAP NO. 08013C0443K DATED 08/15/19.
5	DUE TO THE PROXIMITY OF THE PROPERTY TO THE ERIE MUNICIPAL AIRPORT, THERE WILL BE AIRCRAFT PASSING OVER THE PROPERTY. AIRCRAFT PASSAGE MAY RESULT IN NOISE AND OTHER IMPACTS ON THE PROPERTY. AIRCRAFT MAY CROSS PROPERTY AT LOW ALTITUDE IN ACCORDANCE WITH FAA REGULATIONS. THE FREQUENCY OF AIRCRAFT PASSING OVER THE PROPERTY MAY INCREASE IN THE FUTURE. THE OWNERS, THEIR HEIRS, SUCCESSORS AND ASSIGNS SPECIFICALLY ACKNOWLEDGE THE RIGHT OF PASSAGE OVER THE PROPERTY FOR AIRCRAFT AND AGREE TO HOLD HARMLESS THE TOWN OF ERIE FOR AIRCRAFT OPERATIONS.
6	PORTIONS OF ERIE AIR PARK REPLAT D ARE UNDERMINED. TO REDUCE RISK OF SUBSIDENCE-RELATED DAMAGE, NO BUILDING OR BUILDING SEGMENT MAY EXCEED 140 FEET IN LENGTH OR WIDTH. THE MINE SUBSIDENCE HAZARD AND MITIGATION RECOMMENDATIONS ARE DESCRIBED IN MINE SUBSIDENCE ASSESSMENT, TRACT E-2, ERIE AIR PARK SUBDIVISION (WESTERN ENVIRONMENT AND ECOLOGY, INC., SEPTEMBER 24, 2019). THIS REPORT IS ON FILE WITH THE TOWN OF ERIE PLANNING AND DEVELOPMENT DEPARTMENT, FILES. MS-001121-2019 AND PUD-001122-2019. AT THE TIME OF BUILDING PERMIT SUBMITTAL, ENGINEERING PLANS WILL BE SUBMITTED TO THE BUILDING DEPARTMENT (FOR REVIEW AND APPROVAL) WHICH WILL SHOW DETAILS OF THE BUILDING SEPARATION AND ISOLATION SYSTEMS. THIS WILL ENSURE THAT THE MAXIMUM BUILDING LENGTH REQUIREMENTS WOULD BE MET WITH AN ENGINEERED SOLUTION FOR THE FOUNDATION AND SUPERSTRUCTURE DESIGN.

LEGAL DESCRIPTION

LOT 1, ERIE AIR PARK REPLAT D, A PLAT RECORDED ON 12/05/22 AT RECEPTION NO. 4870475, TOWN OF ERIE, COUNTY OF WELD, STATE OF COLORADO

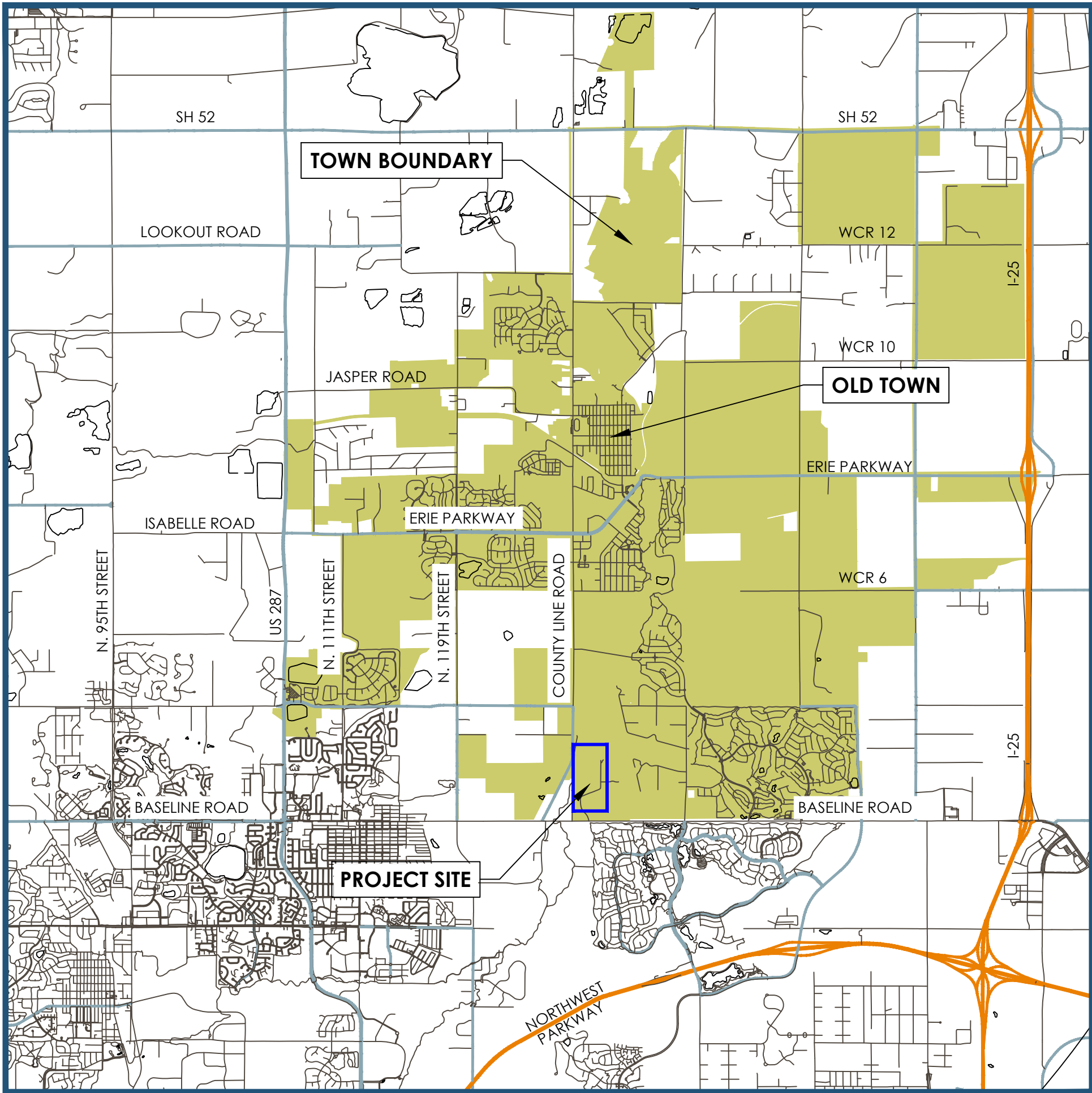
SITE DATA SUMMARY CHART

ITEM	SQUARE FOOTAGE	% OF GROSS SITE
GROSS SITE AREA	450,431	100.00%
• BUILDING FOOTPRINT	139,319	30.93%
• PARKING/ROADS (INCLUDING PLANTED INTERIOR PARKING ISLANDS)	216,412	48.05%
HARDSCAPE TOTAL	355,731	78.98%
• PLANTED AREA (EXCLUDING PLANTED INTERIOR PARKING ISLANDS)	94,700	21.02%
• EXISTING VEGETATION	-	0.00%
• TRAILS AND SIDEWALKS	-	0.00%
LANDSCAPE TOTAL	94,700	21.02%

PARKING TABLE

BLDG	USE	REQUIRED RATIO	BUILDING AREA	TOTAL PARKING REQUIRED	TOTAL PARKING PROVIDED	HANDICAP PARKING REQUIRED	HANDICAP PARKING PROVIDED
100	Light Industrial	1: 1500	37,500	25	29	1	2
200	Light Industrial	1: 1500	37,500	25	28	1	1
300	Light Industrial	1: 1500	21,875	15	15	1	1
400	Light Industrial	1: 1500	37,500	25	28	1	2
500	Light Industrial	1: 1500	37,500	25	29	1	1
	Shared Parking Area				56	1	
	TOTAL		171,875	115	185	6	7

NOTE: THE SITE HAS BEEN PARKED AT THE LIGHT INDUSTRIAL RATIO OF 1:1,500. THIS ASSUMES THAT THE FLOOR AREA (FIRST AND SECOND FLOORS) OF EACH UNIT IS PERMITTED AS LIGHT INDUSTRIAL, INCLUDING OFFICE USES THAT ARE ANCILLARY TO THE LIGHT INDUSTRIAL USE. IF OTHER INDIVIDUAL USES ARE PROPOSED FOR ANY OF THE UNITS, THEN THOSE USES WOULD BE PARKED AT THE RATIOS OUTLINED IN THE UDC. THERE ARE CURRENTLY SEVENTY (70) EXTRA SPACES AT THE SITE. ADDITIONAL USES WOULD BE DEDUCTED FROM THE EXTRA SPACES ALLOCATED TO THE PROJECT.



1 LOCATION MAP

SCALE: 1" = 5000'

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C-702	CIVIL DETAILS
C-703	CIVIL DETAILS
C-704	CIVIL DETAILS
C-705	CIVIL DETAILS
C-706	CIVIL DETAILS
C-707	CIVIL DETAILS
C-708	CIVIL DETAILS
C-709	CIVIL DETAILS
C-710	CIVIL DETAILS
C-711	CIVIL DETAILS
C-712	CIVIL DETAILS
C-713	CIVIL DETAILS
C-714	CIVIL DETAILS
C-715	CIVIL DETAILS
C-716	CIVIL DETAILS

LEGEND

SS	EX. SANITARY SEWER
W	EX. WATER
G	EX. UNDERGROUND GAS LINE
PO	EX. OVERHEAD ELECTRIC LINE
25	PROPOSED CONTOURS
55.25	EXISTING CONTOUR
25.8	POINT WHERE PROPOSED GRADE MEETS EXISTING GRADE
25.8	PROPOSED SPOT ELEVATION
25.8	EXISTING SPOT ELEVATION
	HISTORIC SHEET FLOW
	PROPOSED SHEET FLOW
	PROPOSED BASIN BOUNDARY
H 10.34	HISTORIC SUB-BASIN DESIGNATION AREA IN ACRES
A 2.36	PROPOSED SUB-BASIN DESIGNATION AREA IN ACRES
SS	PROPOSED SANITARY SEWER
W	PROPOSED WATER
ST	PROPOSED STORM SEWER
G	PROPOSED UNDERGROUND GAS LINE
PO	PROPOSED UNDERGROUND ELECTRIC LINE
W	PROPOSED WATER VALVE
	PROPOSED FIRE HYDRANT
	PROPOSED STORM MANHOLE
	PROPOSED STORM INLET
T	PROPOSED TRANSFORMER
SF	SILT FENCE
	SEDIMENT CONTROL LOG
	STRAW BALE BARRIER
	INLET PROTECTION
	STABILIZED STAGING AREA
	PERMANENT SEEDING
	VEHICLE TRACKING CONTROL
	CONCRETE WASHOUT AREA

TOWN OF ERIE ACCEPTANCE

ALL WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH CURRENT TOWN OF ERIE STANDARDS AND SPECIFICATIONS, AS AMENDED. THIS DRAWING HAS BEEN REVIEWED AND FOUND TO BE IN GENERAL COMPLIANCE WITH THESE STANDARDS AND SPECIFICATIONS AND OTHER TOWN REQUIREMENTS. THIS ACCEPTANCE SHALL NOT BE CONSTRUED TO RELIEVE ANY REQUIREMENT TO THE STANDARDS AND SPECIFICATIONS NOT SPECIFICALLY ADDRESSED IN THESE PLANS. IN ADDITION, THE ENGINEERING DESIGN AND CONCEPT REMAINS THE RESPONSIBILITY OF THE PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE APPEAR HEREON.

ACCEPTED BY:

TOWN ENGINEER DATE

siteworks
creativity for
the built environment

For
Submittal

Lot 1
Erie Air Park
Replat D
3020-3100 Airport Drive
Erie, Colorado 80516

#	Date	Description
1	05/22/23	Initial Submittal
2	07/28/23	SIP Submittal
3	12/08/23	Town Comments
4	06/14/24	Town Comments

Project No: 23126A
By: JAS/DPA
File: 23126A-1

Cover Sheet

Sheet

C-001

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GENERAL NOTES - CONSTRUCTION

- ALL CONSTRUCTION SHALL CONFORM TO THE LATEST "STANDARDS AND SPECIFICATIONS FOR DESIGN AND CONSTRUCTION OF PUBLIC IMPROVEMENTS" BY THE TOWN OF ERIE. COPIES OF THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS MAY BE OBTAINED FROM THE TOWN OF ERIE WEB SITE. CONTRACTOR SHALL HAVE A SET ON SITE AT ALL TIMES.
- A PRE-CONSTRUCTION MEETING WITH THE TOWN OF ERIE ENGINEERING STAFF PRIOR TO THE START OF CONSTRUCTION, THOSE IN ATTENDANCE SHALL INCLUDE THE OWNER, HIS ENGINEER, THE TOWN OF ERIE ENGINEERING STAFF, REPRESENTATIVES OF THE CONTRACTORS AND OTHER AFFECTED AGENCIES, PLANS SIGNED AND ACCEPTED BY THE TOWN OF ERIE WILL BE DISTRIBUTED AT THE PRE- CONSTRUCTION MEETING. CONTRACTOR SHALL HAVE (1) COPY OF THE SIGNED PLANS ON SITE AT ALL TIMES.
- THE TOWN OF ERIE, THROUGH ACCEPTANCE OF THIS DOCUMENT, ASSUMES NO RESPONSIBILITY FOR THE COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT, THE OWNER AND DESIGN ENGINEER UNDERSTAND THAT THE RESPONSIBILITY FOR THE ENGINEERING ADEQUACY OF THE FACILITIES DEPICTED IN THIS DOCUMENT LIES SOLELY WITH THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT. REPORT ALL DISCREPANCIES TO THE DESIGN ENGINEER IMMEDIATELY.
- PRIOR TO BEGINNING THE WORK, THE CONTRACTOR SHALL OBTAIN ANY/ALL WRITTEN AGREEMENTS FOR INGRESS AND EGRESS TO THE WORK SITE FROM ADJACENT PRIVATE PROPERTY OWNERS. A COPY OF ALL AGREEMENTS SHALL BE PROVIDED TO THE TOWN. ACCESS TO ANY ADJACENT PRIVATE PROPERTY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF ERIE ENGINEERING STAFF. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO TOWN STANDARDS AND SPECIFICATIONS. INSPECTIONS AND ONSITE VISITS ARE NOT TO BE CONSTRUED AS A GUARANTEE BY THE TOWN ENGINEERING STAFF OF THE CONTRACTORS' CONTRACTUAL COMMITMENT. REQUESTS FOR INSPECTION BY THE TOWN OF ERIE SHALL BE MADE BY THE CONTRACTOR A MINIMUM OF TWENTY-FOUR (24) HOURS IN ADVANCE.
- CONSTRUCTION WATER IS AVAILABLE TO THE CONTRACTOR AS ESTABLISHED IN THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT THE TOWN OF ERIE REGARDING CURRENT REGULATIONS, FEES AND REQUIRED AGREEMENTS RELATED TO THE PROVISION OF CONSTRUCTION WATER.
- THE CONTRACTOR SHALL COORDINATE HIS ACTIVITIES WITH THE AFFECTED UTILITY COMPANIES AND SHALL NOTIFY THE UTILITY NOTIFICATION CENTER, PHONE NUMBER 811, THREE (3) BUSINESS DAYS PRIOR TO THE START OF CONSTRUCTION.
- UTILITIES IN THE AREA OF CONSTRUCTION ARE APPROXIMATE ONLY. THEY HAVE BEEN LOCATED FROM FIELD INVESTIGATION AND THE BEST AVAILABLE UTILITY RECORDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION, PROTECTION AND REPAIR OF ALL UTILITIES ENCOUNTERED DURING CONSTRUCTION WHETHER SHOWN ON THESE PLANS OR NOT. THE CONTRACTOR SHALL CONTACT ALL RESPECTIVE UTILITIES AND HAVE ALL UTILITIES FIELD-LOCATED PRIOR TO CONSTRUCTION. IF ANY UNKNOWN SUBSURFACE STRUCTURES ARE ENCOUNTERED DURING CONSTRUCTION, IT SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE TOWN OF ERIE ENGINEERING STAFF AND DESIGN ENGINEER PRIOR TO PROCEEDING.
- THE CONTRACTOR SHALL NOTIFY TOWN OF ERIE ENGINEERING STAFF OF ANY PROBLEM IMPACTING WATER AND WASTE WATER FACILITIES THAT WOULD POTENTIALLY REQUIRE A VARIANCE FROM THE APPROVED PLANS AND SPECIFICATIONS, ANY VARIANCE FROM THE APPROVED DOCUMENTS SHALL BE AT THE SOLE DISCRETION OF THE TOWN OF ERIE ENGINEERING STAFF.
- CONTRACTOR SHALL OBTAIN, AT HIS OWN EXPENSE, ALL APPLICABLE SPECIFICATIONS AND PERMITS NECESSARY TO PERFORM THE PROPOSED WORK.
- AS-BUILT DRAWINGS AS REQUIRED IN THE SPECIFICATIONS, ARE TO BE SUBMITTED BY THE OWNER/DEVELOPER PRIOR TO INITIAL ACCEPTANCE OF THE CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING AND REPLACING ANY EXISTING SIGNS, STRUCTURES, FENCES, ETC., ENCOUNTERED ON THE JOB AND RESTORING THEM TO THEIR ORIGINAL CONDITION.
- THE CONTRACTOR IS RESPONSIBLE FOR:
 - NOTIFYING THE TOWN OF ERIE UTILITY CUSTOMERS OF POTENTIAL SERVICE OUTAGES, AND COORDINATE WITH THE TOWN OF ERIE FOR DETERMINATION OF MINIMUM TIME REQUIREMENT.
 - NOTIFYING THE TOWN OF ERIE ENGINEERING STAFF IF WORK IS SUSPENDED FOR ANY PERIOD OF TIME AFTER INITIAL START-UP. THE CONTRACTOR SHALL NOTIFY THE TOWN OF ERIE FORTY-EIGHT (48) HOURS PRIOR TO RESTART.
 - IN THE EVENT OF AN AFTER HOURS EMERGENCY, CALL 303-441-4444.
 - NOTIFYING THE MOUNTAIN VIEW FIRE PROTECTION DISTRICT OF ALL STREET CLOSURES AND EXISTING FIRE HYDRANTS TAKEN OUT OF SERVICE A MINIMUM OF FORTY-EIGHT (48) HOURS PRIOR TO THE START OF CONSTRUCTION.
- PRIOR TO INSTALLATION OF UTILITY MAINS, ROAD CONSTRUCTION MUST HAVE COMPLETED THE OVER LOT GRADING STAGE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ANY GROUNDWATER ENCOUNTERED DURING THE CONSTRUCTION OF ANY PORTION OF THIS PROJECT. A CONSTRUCTION DEWATERING PERMIT MUST BE OBTAINED FROM THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE). GROUNDWATER SHALL BE PUMPED, PIPED, REMOVED AND DISPOSED OF IN A MANNER WHICH DOES NOT CAUSE FLOODING OF EXISTING STREETS OR EROSION OF ABUTTING PROPERTIES IN ORDER TO CONSTRUCT THE IMPROVEMENTS SHOWN ON THESE PLANS. THE USE OF ANY SANITARY SEWER TO DISPOSE OF TRENCH WATER WILL NOT BE PERMITTED. NO CONCRETE SHALL BE PLACED WHERE GROUNDWATER IS VISIBLE OR UNTIL THE GROUNDWATER TABLE HAS BEEN LOWERED BELOW THE PROPOSED IMPROVEMENTS. ANY UNSTABLE AREAS, AS A RESULT OF GROUNDWATER, ENCOUNTERED DURING THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS SHALL BE STABILIZED AS AGREED UPON BY THE CONTRACTOR, THE TOWN OF ERIE, AND THE DESIGN ENGINEER AT THE TIME OF THE OCCURRENCE
- IT SHALL BE THE RESPONSIBILITY OF THE DESIGN ENGINEER TO RESOLVE CONSTRUCTION PROBLEMS WITH THE TOWN OF ERIE DUE TO CHANGED CONDITIONS ENCOUNTERED BY THE CONTRACTOR DURING THE PROGRESS OF ANY PORTION OF THE PROPOSED WORK. IF, IN THE OPINION OF THE TOWN OF ERIE, PROPOSED ALTERATIONS TO THE SIGNED CONSTRUCTION PLANS INVOLVES SIGNIFICANT CHANGES TO THE CHARACTER OF THE WORK, OR TO THE FUTURE CONTIGUOUS PUBLIC OR PRIVATE IMPROVEMENTS, THE DESIGN ENGINEER SHALL BE RESPONSIBLE FOR SUBMITTING REVISED PLANS TO THE TOWN OF ERIE FOR REVIEW, PRIOR TO ANY FURTHER CONSTRUCTION RELATED TO THAT PORTION OF THE WORK.
- DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, THE CONTRACTOR SHALL BE SOLELY

- AND COMPLETELY RESPONSIBLE FOR CONDITIONS AT AND ADJACENT TO THE JOB INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THE CONTRACTOR SHALL PROVIDE ALL LIGHTS, SIGNS, BARRICADES, FLAGMEN, OR OTHER DEVICES NECESSARY TO PROVIDE FOR PUBLIC SAFETY. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND IS NOT LIMITED TO NORMAL WORKING HOURS. THE TOWN OF ERIE OR THE DESIGN ENGINEER EXERCISE NO CONTROLS OVER THE SAFETY OR ADEQUACY OF ANY EQUIPMENT, BUILDING COMPONENTS, SCAFFOLDING, FORMS OR OTHER WORK AIDS USED IN OR ABOUT THE PROJECT, OR IN THE SUPERINTENDING OF THE SAME. THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD HARMLESS FROM ANY AND ALL LIABILITY, REAL AND ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER, THE DESIGN ENGINEER OR THE TOWN. THE TOWN OF ERIE ENGINEERING STAFF, OR ANY CONTRACTED ENGINEER, ARE NOT RESPONSIBLE FOR SAFETY IN, ON OR ABOUT THE PROJECT SITE, NOR FOR COMPLIANCE BY THE APPROPRIATE PARTY OF ANY REGULATIONS RELATING THERETO.
- WORK IN PUBLIC STREETS, ONCE BEGUN, SHALL BE PROSECUTED TO COMPLETION WITHOUT DELAY SO AS TO PROVIDE MINIMUM INCONVENIENCE TO ADJACENT PROPERTY OWNERS AND TO THE TRAVELING PUBLIC.
 - REGULAR WORK HOURS ARE SEVEN (7) A.M. UNTIL SEVEN (7) P.M. OR DUSK (WHICHEVER OCCURS FIRST) OF THE SAME DAY, MONDAY THROUGH FRIDAY. THE CONTRACTOR WILL NOT PERMIT OVERTIME WORK OUTSIDE OF REGULAR WORKING HOURS OR THE PERFORMANCE OF WORK ON SATURDAY, SUNDAY OR ANY LEGAL HOLIDAY WITHOUT RECEIVING WRITTEN CONSENT FROM THE PUBLIC WORKS DIRECTOR. REQUESTS FOR WEEKEND WORK APPROVAL MUST BE SUBMITTED, IN WRITING TO THE TOWN OF ERIE NO LATER THAN WEDNESDAYS AT 3:30PM FOR SUBSEQUENT WEEKEND AND REQUESTS FOR HOLIDAY WORK APPROVAL MUST BE SUBMITTED, IN WRITING TO THE TOWN OF ERIE NO LATER THAN 7:00AM-2 BUSINESS DAYS PRIOR TO THE HOLIDAY. ALL EXPENSES INCURRED BY THE TOWN SHALL BE REIMBURSED AT A RATE TO BE DETERMINED BY DIRECTOR OF FINANCE.
 - THE CONTRACTOR SHALL TAKE ALL NECESSARY AND PROPER PRECAUTIONS TO PROTECT ADJACENT PROPERTIES FROM ANY AND ALL DAMAGE THAT MAY OCCUR FROM STORM WATER RUNOFF AND/OR DEPOSITION OF DEBRIS RESULTING FROM ANY AND ALL WORK. THE OWNER/CONTRACTOR IS RESPONSIBLE FOR OBTAINING A STORMWATER DISCHARGE PERMIT FOR CONSTRUCTION ACTIVITIES FOR ANY PROJECT DISTURBING OVER ONE ACRE FROM BOTH THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND THE TOWN OF ERIE.
 - EACH TYPE OF CONSTRUCTION SHALL BE COMPLETED BY A CONTRACTOR THAT HAS DEMONSTRATED ACCEPTABLE QUALIFICATIONS TO THE TOWN AND IS A LICENSED CONTRACTOR IN THE TOWN OF ERIE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL DURING CONSTRUCTION. ALL TRAFFIC CONTROLS SHALL CONFORM TO THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (MUTCD) LATEST EDITIONS. A PLAN SHALL BE SUBMITTED TO THE TOWN FOR REVIEW AND ACCEPTANCE PRIOR TO CONSTRUCTION.
 - ALL BACKFILL SHALL CONFORM TO THE TRENCH DETAIL LOCATED IN THE TOWN OF ERIE STANDARDS & SPECIFICATIONS.
 - THE CONTRACTOR SHALL IMMEDIATELY REMOVE ANY CONSTRUCTION DEBRIS OR MUD TRACKED ONTO EXISTING ROADWAYS.
 - THE CONTRACTOR SHALL REPAIR ANY EXCAVATION OR PAVEMENT FAILURES CAUSED BY HIS CONSTRUCTION.
 - THE CONTRACTOR SHALL RENEW OR REPLACE ANY EXISTING TRAFFIC STRIPING AND/OR PAVEMENT MARKINGS, WHICH HAVE BEEN EITHER REMOVED OR THE EFFECTIVENESS OF WHICH HAS BEEN REDUCED DURING HIS OPERATION. RENEWAL OF PAVEMENT STRIPING AND MARKING SHALL BE DONE IN CONFORMANCE WITH THE TOWN OF ERIE STANDARD SPECIFICATIONS.
 - IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO TAKE EVERY MEASURE NECESSARY TO COMPLY WITH ANY STATE, COUNTY OR TOWN DUST CONTROL ORDINANCE.
 - CONSTRUCTION VEHICLES SHALL USE TRUCK ROUTES DESIGNATED BY THE TOWN.
 - THE OWNER/DEVELOPER WILL BE HELD RESPONSIBLE FOR THE PROPER FUNCTIONING OF THE IMPROVEMENTS FOR A MINIMUM OF TWO (2) YEARS FROM THE DATE OF INITIAL ACCEPTANCE OF THE IMPROVEMENTS BY THE TOWN OF ERIE. ANY FAILURE DURING THIS PERIOD OF GUARANTEE SHALL BE REMEDIED BY THE OWNER/CONTRACTOR TO THE SATISFACTION OF THE TOWN OF ERIE AT NO EXPENSE TO THE TOWN.
 - THE SOILS ENGINEER SHALL PERFORM SUFFICIENT INSPECTIONS DURING GRADING AND CONSTRUCTION SO THAT AN OPINION CAN BE RENDERED AND VERIFIED IN WRITING AS TO COMPLIANCE WITH THE PLANS AND CODES WITHIN THE SOILS ENGINEER'S PURVIEW.

GENERAL NOTES - GRADING

- ALL CONSTRUCTION ACTIVITIES THAT DISTURBS ONE OR MORE ACRES OF LAND, AS WELL AS ACTIVITIES THAT DISTURB LESS THAN ONE ACRE OF LAND, BUT IS PART OF A LARGER COMMON PLAN OF DEVELOPMENT, MUST COMPLY WITH BOTH LOCAL AND STATE REGULATIONS REGARDING STORMWATER DRAINAGE ON CONSTRUCTION SITES. OWNERS OR CONTRACTORS MUST OBTAIN A COLORADO STORMWATER DISCHARGE PERMIT FOR CONSTRUCTION ACTIVITIES FROM THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE) AND EITHER A PUBLIC IMPROVEMENT PERMIT OR A GRADING AND STORMWATER QUALITY PERMIT FROM THE TOWN OF ERIE. CONTRACTOR SHALL:
 - MAINTAIN A COPY OF THE STORM WATER MANAGEMENT PLAN (SWMP) ONSITE AT ALL TIMES. THE SWMP MUST BE MAINTAINED AND MADE AVAILABLE TO TOWN OF ERIE INSPECTORS UPON REQUEST.
 - INSTALL AND MAINTAIN EROSION, SEDIMENT, AND MATERIALS MANAGEMENT CONTROL BMPS AS SPECIFIED IN THE SWMP.
 - INSPECT ALL BEST MANAGEMENT PRACTICES (BMPS) AT LEAST EVERY FOURTEEN (14) DAYS AND WITHIN TWENTY FOUR (24) HOURS AFTER ANY PRECIPITATION OR SNOWMELT EVENT THAT CAUSES SURFACE RUNOFF.
 - MAINTAIN INSPECTION AND MAINTENANCE RECORDS OF BMPS ONSITE WITH THE SWMP. COPIES OF THESE REPORTS SHALL BE PROVIDED TO THE TOWN OF ERIE ENGINEERING STAFF.
 - BASED ON INSPECTIONS PERFORMED BY THE PERMIT HOLDER OR BY TOWN PERSONNEL, MODIFICATIONS TO THE SWMP WILL BE NECESSARY IF AT ANY TIME THE SPECIFIED BMPS DO NOT MEET THE OBJECTIVES OF THE PERMIT. ALL MODIFICATIONS SHALL BE COMPLETED AS SOON AS PRACTICABLE AFTER THE REFERENCED INSPECTION, AND SHALL BE RECORDED ON THE OWNER'S COPY OF THE SWMP.
 - THE OPERATOR SHALL AMEND THE SWMP WHENEVER THERE IS A SIGNIFICANT CHANGE IN DESIGN, CONSTRUCTION, OPERATION, OR MAINTENANCE, WHICH HAS A SIGNIFICANT EFFECT ON THE POTENTIAL FOR DISCHARGE OF POLLUTANTS TO THE RECEIVING WATERS, OR IF THE SWMP PROVES TO BE INEFFECTIVE IN ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
 - INSTALLATION AND MAINTENANCE OF BMPS SHALL BE SUPERVISED BY PERSONNEL CERTIFIED IN EROSION AND SEDIMENT CONTROL.
- ALL SITE GRADING (EXCAVATION, EMBANKMENT, AND COMPACTION) SHALL CONFORM TO THE RECOMMENDATIONS OF THE LATEST SOILS INVESTIGATION FOR THIS PROPERTY AND SHALL FURTHER BE IN CONFORMANCE WITH THE TOWN OF ERIE "STANDARDS AND SPECIFICATIONS FOR THE DESIGN AND CONSTRUCTION OF PUBLIC IMPROVEMENTS", LATEST EDITION.
- ALL GRADING AND FILLING OPERATIONS SHALL BE OBSERVED, INSPECTED AND TESTED BY A LICENSED SOILS ENGINEER. ALL TEST RESULTS SHALL BE SUBMITTED TO THE TOWN OF ERIE ENGINEERING STAFF.
- NATURAL VEGETATION SHALL BE RETAINED AND PROTECTED WHEREVER POSSIBLE. EXPOSURE OF SOIL TO EROSION BY REMOVAL OR DISTURBANCE OF VEGETATION SHALL BE LIMITED TO THE AREA REQUIRED FOR IMMEDIATE CONSTRUCTION OPERATION AND FOR THE SHORTEST PRACTICAL PERIOD OF TIME. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO AVOID ANY DAMAGE TO EXISTING FOLIAGE THAT LIES IN THE PROJECT AREA UNLESS DESIGNATED FOR REMOVAL AND SHALL BE LIABLE FOR SUCH DAMAGE AT HIS/HER EXPENSE.
- TOPSOIL SHALL BE STOCKPILED TO THE EXTENT PRACTICABLE ON THE SITE FOR USE ON AREAS TO BE REVEGETATED. ANY AND ALL STOCKPILES SHALL BE LOCATED AND PROTECTED FROM EROSION ELEMENTS.
- TEMPORARY VEGETATION SHALL BE INSTALLED ON ALL DISTURBED AREAS WHERE PERMANENT SURFACE IMPROVEMENTS ARE NOT SCHEDULED FOR IMMEDIATE INSTALLATION. SEEDING WILL BE DONE ACROSS THE SLOPE FOLLOWING THE CONTOURS. VEGETATION SHALL CONFORM TO THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS. PROJECT SCHEDULING SHOULD TAKE ADVANTAGE OF SPRING OR FALL PLANTING SEASONS FOR NATURAL GERMINATION. SEEDED AREAS SHALL BE IRRIGATED IN ACCORDANCE WITH THE TOWN OF ERIE'S STANDARDS AND SPECIFICATIONS.
- AT ALL TIMES, A WATER TRUCK SHALL BE ON-SITE AND THE PROPERTY SHALL BE MAINTAINED AND/OR WATERED TO PREVENT WIND-CAUSED EROSION. EARTHWORK OPERATIONS SHALL BE DISCONTINUED WHEN FUGITIVE DUST SIGNIFICANTLY IMPACTS ADJACENT PROPERTY. IF EARTHWORK IS COMPLETE OR
 - DISCONTINUED AND DUST FROM THE SITE CONTINUES TO CREATE PROBLEMS, THE OWNER/DEVELOPER SHALL IMMEDIATELY INSTITUTE MITIGATIVE MEASURES AND SHALL CORRECT DAMAGE TO ADJACENT PROPERTY.
- FILL SLOPES SHALL BE COMPACTED BY MEANS OF SHEEPSFOOT COMPACTOR OR OTHER SUITABLE EQUIPMENT. COMPACTING SHALL CONTINUE UNTIL SLOPES ARE STABLE AND THERE IS NOT AN APPRECIABLE AMOUNT OF LOOSE SOIL ON THE SLOPES.
- TEMPORARY CUT/FILL SLOPES SHALL ABIDE BY THE SOILS REPORT. PERMANENT SLOPES SHALL BE AS SHOWN ON PLANS.
- DEPTH OF MOISTURE-DENSITY CONTROL SHALL BE FULL DEPTH ON ALL EMBANKMENT AND SIX (6) INCHES ON THE BASE OF CUTS AND FILLS.
- OUTLET SIDES OF ALL STORM PIPES SHALL BE GRADED TO DRAIN AND SHALL HAVE SUFFICIENT EROSION PROTECTION.
- THE PERMITTEE OR HIS AGENT SHALL NOTIFY THE SITE GEOTECHNICAL ENGINEER WHEN THE GRADING OPERATION IS READY FOR EACH OF THE FOLLOWING INSPECTIONS:
 - INITIAL INSPECTION WHEN THE PERMITTEE IS READY TO BEGIN WORK, BUT NOT LESS THAN TWO (2) DAYS BEFORE ANY GRADING OR GRUBBING IS STARTED.
 - AFTER THE NATURAL GROUND OR BEDROCK IS EXPOSED AND PREPARED TO RECEIVE FILL, BUT BEFORE FILL IS PLACED.
 - EXCAVATION INSPECTION AFTER THE EXCAVATION IS STARTED BUT BEFORE THE VERTICAL DEPTH OF THE EXCAVATION EXCEEDS TEN (10) FEET.
 - FILL INSPECTION AFTER THE FILL PLACEMENT IS STARTED, BUT BEFORE THE FILL EXCEEDS TEN (10) FEET.

GENERAL NOTES - SEWER

- THE CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING SEWERS TO BE CONNECTED TO PRIOR TO CONSTRUCTION STAKING.
- CONNECTION TO EXISTING TOWN OF ERIE LINES WILL BE PERMITTED UPON INITIAL ACCEPTANCE OF THE NEW SANITARY SEWER SYSTEM. EXISTING PIPE AT THE POINT OF CONNECTION SHALL NOT BE "BROKEN OUT" UNTIL THE NEW SYSTEM IS ACCEPTED. IF CONNECTING TO AN EXISTING MANHOLE, THE NEW LINE SHALL BE PLUGGED UNTIL THE NEW SYSTEM IS ACCEPTED.
- MINIMUM VERTICAL SEPARATIONS BETWEEN ALL UTILITY PIPES SHALL BE EIGHTEEN (18) INCHES. IF VERTICAL SEPARATIONS ARE LESS THAN EIGHTEEN (18) INCHES, THE UTILITY PIPES SHALL BE REINFORCED AND PROTECTED AS REQUIRED BY CURRENT TOWN STANDARD SPECIFICATIONS.
- WATER AND SANITARY SEWER LINES SHALL HAVE A MINIMUM HORIZONTAL SEPARATION OF TEN (10) FEET. WHEN A TEN (10) FOOT SEPARATION IS NOT PROVIDED OR WHEN SEWER LINES CROSS WATER LINES WITH LESS THAN ONE AND ONE-HALF (1½) FEET OF VERTICAL SEPARATION, SEWER LINE JOINTS SHALL BE CONCRETE ENCASED. FOR PERPENDICULAR CROSSINGS, ENCASED JOINTS SHALL EXTEND TEN (10) FEET, PERPENDICULAR TO THE WATER LINE IN BOTH DIRECTIONS.
- ALL SANITARY SEWER SERVICES AND WATER SERVICES ARE TO BE TEN (10) FEET APART.
- SERVICE LATERALS SHALL EXTEND FIVE (5) FEET BEYOND RIGHTS OF WAY OR UTILITY EASEMENTS, WHICHEVER IS GREATER. THE ENDS SHALL BE MARKED BY A GREEN PAINTED WOOD POST UNTIL CURB AND GUTTER IS IN PLACE. WHEN CURB AND GUTTER IS IN PLACE THE LATERALS SHALL BE MARKED ON THE CONCRETE CURB FACE WITH AN "S" OR "X".
- THE LENGTH OF SANITARY SEWER LINE IS THE HORIZONTAL DISTANCE BETWEEN CENTER OF MANHOLE TO CENTER OF MANHOLE. THEREFORE, THE DISTANCES INDICATED ON THE PLANS ARE APPROXIMATE AND COULD VARY DUE TO VERTICAL ALIGNMENT AND MANHOLE DIMENSIONS.
- SERVICE LINE CONNECTIONS TO DEAD END MANHOLES THAT HAVE NO FURTHER POSSIBILITY OF EXTENSION SHALL BE ALLOWED AND SHALL
 - HAVE A MINIMUM DROP OF 0.75 X MAIN DIAMETER. SERVICE LINE CONNECTINGS TO IN-LINE MANHOLES ARE NOT PERMITTED. MINIMUM SERVICE LINE SLOPE: 4 INCHES=2%; 6 INCHES= 1%; 8 INCHES=0.4%.
- ALL FOUR (4) THROUGH FIFTEEN (15) INCH SANITARY SEWER PIPE SHALL BE POLYVINYL CHLORIDE (PVC) AND SHALL BE IN ACCORDANCE WITH ASTM D-3034-SDR35, "STANDARD SPECIFICATION FOR PVC SEWER PIPE AND FITTINGS". ANY SANITARY SEWER HAVING A DEPTH IN EXCESS OF FIFTEEN (15) FEET SHALL BE COORDINATED WITH THE PUBLIC WORKS DEPARTMENT.
- BEDDING MATERIAL SHALL CONFORM TO TOWN OF ERIE STANDARDS AND SPECIFICATIONS.
- WARNING TAPE SHALL BE INSTALLED 12" MINIMUM AND 18" MAXIMUM ABOVE SEWER PIPE.
- PRECAST CONCRETE MANHOLE SECTIONS SHALL BE IN ACCORDANCE WITH ASTM C-478. MANHOLE STEPS SHALL BE POLYPROPYLENE COVERED STEEL CONFORMING TO ASTM. D-4101 AND ASTMA-615. CAST IRON RING AND COVER SHALL CONFORM TO ASTM A-48.
- MANHOLES SHALL BE A MINIMUM FOUR (4) FOOT DIAMETER AND CONSTRUCTED PER THE STANDARDS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL TAKE CARE TO PROPERLY SHAPE ALL MANHOLE INVERTS AND BENCHES IN ACCORDANCE WITH THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS, TO PROMOTE SMOOTH FLOW THROUGH THE MANHOLE. INVERTS OF LINES INTERSECTING AT
 - 90 DEGREES AND AT HIGHLY DIVERGENT OR FLAT SLOPES ARE ESPECIALLY CRITICAL. MANHOLE INVERTS SHALL BE CONSTRUCTED WITH A SMOOTH TROWEL FINISH, AND BENCH FINISHED WITH A LIGHT BROOMED, NON-SKID, FINISH.
- SEWER TEES AND/OR WYES SHALL BE STAKED BY A SURVEY CREW. THE CONTRACTOR SHALL FURNISH TO THE ENGINEER "AS- CONSTRUCTED" LOCATION OF TEES AND WYES. ALL SERVICE LINES ARE FOUR (4) INCH UNLESS OTHERWISE NOTED.
- THE CONTRACTOR, AT THE OWNER'S EXPENSE, WILL MAKE ALL SEWER SERVICE TAPS.
- PRIOR TO BACKFILL THE TOWN OF ERIE ENGINEERING STAFF SHALL INSPECT ALL SANITARY SEWER MAINS AND SERVICE EXTENSIONS.
- MANHOLE RIMS SHALL BE SET AT AN ELEVATION RELATIVE TO THE PAVEMENT, IN ACCORDANCE WITH THE TOWN OF ERIE STANDARDS.
- WHETHER THE MANHOLE IS AT PAVED OR UNPAVED GRADE, A MINIMUM OF ONE (1) AND A MAXIMUM OF FOUR (4) CONCRETE RINGS SHALL BE USED TO ADJUST THE RIM ELEVATION TO FINAL GRADE. THE MAXIMUM ACCEPTABLE VERTICAL ADJUSTMENT UTILIZING CONCRETE RINGS IS EIGHTEEN (18) INCHES.
- INITIAL ACCEPTANCE OF THE NEW SANITARY SEWER MAINS IS CONTINGENT UPON COMPLETION OF ITEMS LISTED IN THE TOWNS STANDARDS AND SPECIFICATIONS.

creativity for
the built environment

For
Submittal

Lot 1 Erie Air Park Replat D

3020-3100 Airport Drive
Erie, Colorado 80516

#	Date	Description
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Project No:	23126A
By:	JAS/DPA
File:	23126A-1

General Notes

Sheet

C-002

GENERAL NOTES - WATER

- AT ALL POINTS OF CONNECTION OF NEW WATER MAINS TO EXISTING MAINS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR EXCAVATING AND VERIFYING LOCATION OF THE EXISTING LINES PRIOR TO ANY CONSTRUCTION.
- EXCEPT IN CASE OF AN EMERGENCY, VALVES ON THE TOWN OF ERIE WATER SYSTEM SHALL BE OPERATED BY OR UNDER THE DIRECTION OF THE APPROPRIATE TOWN OF ERIE PERSONNEL. THE CONTRACTOR SHALL GIVE THE TOWN OF ERIE ENGINEERING STAFF 48 HOURS NOTICE TO ARRANGE FOR OPERATING VALVES. BOTH THE CONTRACTOR AND THE APPROPRIATE TOWN OF ERIE PERSONNEL SHALL BE PRESENT WHEN THE VALVES ARE OPERATED.
- WATER AND SANITARY SEWER LINES SHALL HAVE A MINIMUM HORIZONTAL SEPARATION OF TEN (10) FEET. WHEN A TEN (10) FOOT SEPARATION IS NOT PROVIDED OR WHEN SEWER LINES CROSS WATER LINES WITH LESS THAN ONE AND ONE-HALF (1½) FEET OF VERTICAL SEPARATION, SEWER LINE JOINTS SHALL BE CONCRETE ENCASED. FOR PERPENDICULAR CROSSINGS, ENCASED JOINTS SHALL EXTEND TEN (10) FEET, PERPENDICULAR TO THE WATER LINE IN BOTH DIRECTIONS.
- ALL WATER LINES SHALL HAVE A MINIMUM OF FOUR AND ONE-HALF (4½) FEET OF COVER AND BE LOCATED A MINIMUM OF TEN (10) FEET FROM THE SANITARY SEWER AND THREE (3) FEET FROM THE EDGE OF CONCRETE CURB AND GUTTER PAN.
- CHANGES IN DIRECTION OF WATERLINE PIPE SHALL REQUIRE BENDS IN ALL INSTANCES. AXIAL DEFLECTION AT THE JOINTS SHALL NOT BE ALLOWED.
- WHEN IT IS NECESSARY TO DEPRESS WATER LINES AT UTILITY CROSSINGS, A MINIMUM CLEARANCE OF ONE AND ONE-HALF (1-1/2) FEET SHALL BE MAINTAINED BETWEEN OUTSIDES OF PIPE.
- DISTANCES FOR WATER LINES ARE THE HORIZONTAL DISTANCE BETWEEN THE CENTERS OF THE FITTINGS. THEREFORE, DISTANCES SHOWN ON THE PLANS ARE APPROXIMATE AND COULD VARY DUE TO VERTICAL ALIGNMENT AND FITTING DIMENSIONS.
- ALL WATER LINE VALVES SHALL BE SET ADJACENT TO THE TEE, EXCEPT FOR POINTS THAT FALL IN THE FLOW LINE OF A CONCRETE CROSS PAN. IN WHICH CASE, THE VALVE SHALL BE LOCATED SO THAT SURFACE DRAINAGE DOES NOT INFILTRATE THE VALVE BOX. VALVE BOXES SHALL BE SET AT AN ELEVATION IN ACCORDANCE WITH TOWN PAVING REQUIREMENTS.
- ALL WATER MAINS SHALL BE POLYVINYL CHLORIDE (PVC) PRESSURE PIPE UNLESS SPECIFIED OTHERWISE. NOMINAL PVC PIPE SIZES 6-INCH THROUGH 12-INCH SHALL CONFORM TO ALL REQUIREMENTS OF AWWA STANDARD C-900, PRESSURE CLASS 150 (DR18). NOMINAL PVC PIPE SIZES 16-INCH THROUGH 24-INCH SHALL CONFORM TO ALL REQUIREMENTS OF AWWA STANDARD C-905, PRESSURE CLASS 165 (DR25). ALL PVC PIPES SHALL HAVE OUTSIDE DIAMETERS EQUIVALENT TO CAST IRON PIPE.
- FIRE HYDRANT ASSEMBLY INCLUDES THE FIRE HYDRANT, SIX (6) INCH VALVE, AND SIX (6) INCH PIPE. INSTALLATION SHALL BE IN ACCORDANCE WITH THE TOWN OF ERIE STANDARDS AND SPECIFICATIONS.
- ALL FITTINGS SHALL BE MADE FROM DUCTILE IRON, FURNISHED WITH MECHANICAL JOINT ENDS OR INTEGRAL RESTRAINED JOINTS, AND SHALL HAVE A PRESSURE RATING OF 350 PSI.
- POLYETHYLENE WRAPPING SHALL BE INSTALLED AROUND ALL DUCTILE IRON PIPES, FITTINGS, VALVES, FIRE HYDRANT BARRELS AND ROD AND CLAMPS. THE POLYETHYLENE SHALL HAVE A MINIMUM THICKNESS OF EIGHT (8) MILS, IN ACCORDANCE WITH AWWA STANDARD C-105.
- ALL WATER LINE PIPE SHALL BE PROVIDED WITH A MINIMUM GAGE SIZE OF 12 SINGLE STRAND INSULATED COPPER WIRE. SPLICES IN TRACER WIRE SHALL BE CAPPED IN WATER PROOF GEL CAP TYPE CONNECTORS SUITED FOR DIRECT BURY APPLICATION (3M TYPE DBY-6 LOW VOLTAGE OR EQUAL). WIRE SHALL BE ATTACHED TO TOP OF WATER LINE WITH 2-INCH WIDE PVC TAPE @ 5-FT INTERVALS ALONG PIPE. TRACER WIRE SHALL EXTEND TO THE SURFACE AND BE COILED IN A LOCATE BOX AT THE BACKSIDE OF EITHER EACH FIRE HYDRANT OR VALVE. UNDER THE SUPERVISION OF TOWN OF ERIE ENGINEERING STAFF, TEST SHALL BE MADE BY THE CONTRACTOR @ THE COMPLETION OF CONSTRUCTION TO INSURE THAT THE TRACER WIRES CARRY A CONTINUOUS CURRENT BETWEEN ALL ACCESS POINTS.
- WARNING TAPE SHALL BE INSTALLED 12" MINIMUM AND 18" MAXIMUM ABOVE WATER PIPE.
- BEDDING MATERIAL SHALL CONFORM TO TOWN OF ERIE STANDARDS AND SPECIFICATIONS.
- VALVES SHALL OPEN COUNTER CLOCKWISE. VALVES 12-INCH AND SMALLER SHALL BE RESILIENT SEAT GATE VALVES. LARGER VALVES SHALL BE BUTTERFLY VALVES.
- VALVE BOXES SHALL BE RAISED TO ONE-FOURTH (1/4) INCH BELOW GRADE AFTER COMPLETION OF SURFACE PAVING OR FINAL GRADING. VALVE BOXES IN NON-PAVED AREAS SHALL HAVE A CONCRETE COLLAR AROUND THE VALVE LID IN ACCORDANCE WITH THE DETAIL.
- ALL SERVICE LINE TAPS SHALL HAVE DOUBLE STRAP BRASS TAPPING SADDLES. (ROMAC 202B OR APPROVED EQUAL).
- ALL RESIDENTIAL WATER TAPS SHALL BE THREE-QUARTER (3/4) INCH OR AS REQUIRED BY THE CURRENT BUILDING CODE.
- ALL WATER SERVICE LATERALS SHALL EXTEND FIVE (5) FEET BEYOND RIGHT OF WAY OR UTILITY EASEMENTS, WHICHEVER IS GREATER. THE ENDS SHALL BE MARKED BY A BLUE PAINTED WOOD POST UNTIL CURB AND GUTTER IS IN PLACE. WHEN CURB AND GUTTER IS IN PLACE THE LATERALS SHALL BE MARKED ON THE CONCRETE CURB FACE WITH A "V" or "W".
- CONCRETE THRUST BLOCKS AND/OR "MEGA-LUG" MECHANICAL RESTRAINTS ARE REQUIRED AT ALL MECHANICAL FITTINGS. THRUST BLOCKS MAY NOT BE REQUIRED IF PIPE RESTRAINT IS PROVIDED IN ACCORDANCE WITH RESTRAINED PIPE DETAIL.
- NO WORK SHALL BE BACKFILLED (INCLUDING BEDDING MATERIAL ABOVE THE SPRING LINE OF THE PIPE) UNTIL THE CONSTRUCTION HAS BEEN INSPECTED AND APPROVED FOR BACKFILLING BY THE TOWN OF ERIE ENGINEERING STAFF.
- ONLY ONE CONNECTION TO THE EXISTING WATER DISTRIBUTION SYSTEM SHALL BE MADE UNTIL ALL HYDROSTATIC TESTING, CHLORINATION AND FLUSHING HAS BEEN COMPLETED.
- DISINFECTION AND HYDROSTATIC TESTING SHALL BE DONE IN THE PRESENCE OF A TOWN OF ERIE ENGINEERING STAFF. CONTACT THE TOWN OF ERIE DEPARTMENT OF PUBLIC WORKS, FORTY-EIGHT (48) HOURS PRIOR TO DISINFECTING AND/OR TESTING.
- DISINFECTION AND FLUSHING SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF HEALTH AND THE PROCEDURE SET FORTH IN AWWA C651, "STANDARD FOR DISINFECTING WATER MAINS". THE CHLORINATION OF THE WATER LINE SHALL BE PERFORMED PRIOR TO THE HYDROSTATIC TESTING. ALL VALVES, FIRE HYDRANTS AND OTHER APPURTANCES SHALL BE OPERATED WHILE PIPELINE IS FILLED WITH THE CHLORINATING AGENT TO INSURE THAT HIGH CHLORINE CONTACT IS MADE WITH ALL INTERNAL SURFACES.

- ALL WATER LINES SHALL BE HYDROSTATIC TESTED. PRESSURE AND LEAKAGE TESTS SHALL BE CONDUCTED ACCORDING TO THE APPLICABLE SECTIONS OF AWWA C600/605 TO A MINIMUM PRESSURE OF ONE HUNDRED AND FIFTY (150) POUNDS PER SQUARE (PSI) INCH AT THE LOW POINT OF THE SECTION BEING TESTED FOR THE DURATION OF TWO (2) HOURS. THE MAXIMUM LENGTH OF LINE TO BE TESTED SHALL BE ONE THOUSAND (1,000) FEET. ALL JOINTS IN CONNECTIONS ARE TO BE WATERTIGHT WITHIN TOLERANCES ALLOWED BY THE SPECIFICATIONS IN AWWA C600/605. ANY LEAKAGE THAT IS DISCOVERED BY OBSERVATION OR TESTS SHALL BE LOCATED AND MADE WATERTIGHT BY THE CONTRACTOR. PRESSURE AND LEAKAGE TESTS SHALL NOT BE CONDUCTED UNTIL THE LINE HAS PASSED ALL REQUIRED DISINFECTION TESTS.
- INITIAL ACCEPTANCE OF THE NEW WATER LINES ARE CONTINGENT UPON RECEIVING COPIES OF:
 - WATER TRENCH COMPACTION TEST RESULTS
 - HYDRO STATIC TESTING OF 100% OF THE SYSTEM
 - HEALTH DEPARTMENT TESTS. (CHLORINE AND/OR CLEAR WATER AS REQUIRED)
- ALL METER PITS AND CURB STOPS SHALL BE PROTECTED AT THE TIME OF INSTALLATION WITH A MINIMUM OF THREE (3) T-POSTS AND ORANGE SAFETY FENCE. THE T-POST AND SAFETY FENCE SHALL REMAIN IN PLACE AND IN GOOD CONDITION UNTIL THE LANDSCAPING IS INSTALLED.
- ALL WATER VAULTS SHALL BE WATER TIGHT. CONTRACTOR SHALL SEAL VAULTS TO ENSURE SURFACE WATER DOES NOT INFILTRATE INTO THE VAULTS. VAULT LIDS SHALL BE PLACED TO ENSURE THAT SURFACE WATER DOES NOT FLOW INTO THE VAULTS.

GENERAL NOTES - STORM DRAIN

- EXCEPT WHERE NOTED, ALL STORM SEWER PIPE SHALL BE REINFORCED CONCRETE, CLASS III AND SHALL CONFORM TO REQUIREMENTS OF ASTM C76. ALL RCP SHALL HAVE RUBBER GASKETED JOINTS AND SHALL CONFORM TO REQUIREMENTS OF ASTM C443, AND SHALL PROVIDE WATERTIGHT PERFORMANCE CHARACTERISTICS.
- TONGUE AND GROOVE JOINTS SHALL NOT BE ALLOWED.
- THE MINIMUM COVERAGE FOR ALL STORM DRAINAGE PIPES SHALL BE
 - 1.5 FEET FOR CLASS III PIPE AND 1 FOOT FOR CLASS IV PIPE.
- BEDDING MATERIAL SHALL CONFORM TO TOWN OF ERIE STANDARDS AND SPECIFICATIONS.
- ALL MANHOLES SHALL BE CONCRETE AND CONFORM TO CDOT STANDARD M-604-20.
- THE MINIMUM MANHOLE DIAMETER SHALL BE AS SPECIFIED BELOW:

PIPE DIAMETER	MANHOLE SIZE
15" TO 18"	4' DIAMETER
21" TO 42"	5' DIAMETER
48" TO 54"	6' DIAMETER
60" AND LARGER	BOX BASE MANHOLE
- ALL STREET INLETS SHALL BE CURB OPENING TYPE R CONFORMING TO CDOT STANDARD M-604-12, EXCEPT WHERE OTHERWISE NOTED.
- ALL INLET ACCESS COVERS SHALL HAVE THE WORDS "NO DUMPING - DRAINS TO RIVERS" AND "STORM SEWER" CAST INTO THE COVER PER TOWN OF ERIE STANDARD DETAIL.
- ALL END SECTIONS SHALL CONFORM TO CDOT STANDARD M-603-10.
- WHERE RIPRAP OR GROUTED BOULDERS ARE CALLED FOR ON THE PLANS FOR EROSION CONTROL, IT SHALL CONFORM TO THE URBAN STORM DRAINAGE CRITERIA MANUAL SPECIFICATIONS (LATEST REVISION).

GENERAL NOTES - ROADWAY

- ALL STATIONING IS BASED ON CENTERLINE OF ROADWAYS UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL PREPARE THE SUBGRADE BY SCARIFYING THE UPPER ONE (1) FOOT OF THE SUBGRADE IN CUT AREAS OR AREAS WITH LITTLE OR NO FILL, UNLESS SPECIFIED IN THE SOILS REPORT. THE WORK SHALL CONFORM TO THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
- PAVEMENT SHALL NOT BE CONSTRUCTED UNTIL ALL UNDERGROUND UTILITIES HAVE BEEN INSTALLED, TESTED AND ACCEPTED BY THE TOWN OF ERIE ENGINEERING STAFF.
- IT SHALL BE THE RESPONSIBILITY OF THE OWNER/CONTRACTOR TO SUPERVISE AND CERTIFY THAT PROPER COMPACTION HAS BEEN OBTAINED BY SUBCONTRACTORS AND AGENCIES CONCERNING UTILITY LINE BACKFILL INCLUDING, BUT NOT LIMITED TO, SEWER, WATER, ELECTRICAL, GAS AND LANDSCAPE IRRIGATION LINES AND ACCEPTED BY THE TOWN OF ERIE ENGINEERING STAFF AND THE SOILS ENGINEER.
- STREET PAVING SHALL NOT START UNTIL:
 - A SOILS REPORT AND PAVEMENT DESIGN IS ACCEPTED BY THE TOWN OF ERIE ENGINEERING STAFF.
 - ALL STREETS ARE COMPACTED IN ACCORDANCE WITH THE SOILS REPORT AND THE TOWN OF ERIE SPECIFICATIONS.
 - ALL COMPACTION TEST REPORTS HAVE BEEN SUBMITTED TO THE TOWN ENGINEERING STAFF PRIOR TO PROOF ROLLS.
 - PROOF ROLLS ARE PERFORMED USING A LOADED SINGLE AXLE 2000 GALLON WATER TRUCK AND MONITORED BY THE TOWN OF ERIE ENGINEERING STAFF.
- THE OWNER/CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING ALL UTILITY MANHOLE COVERS AND ACCESS LIDS TO GRADE.
- ALL CONCRETE SHALL BE A MINIMUM OF CLASS B, IN CONFORMANCE WITH CDOT STANDARDS.
- ALL CONCRETE EDGES MUST BE ROUNDED TO A FOURTH (1/4) INCH RADIUS, EXCEPT WHERE SHOWN OTHERWISE ON DRAWINGS.
- ONE HALF (1/2) INCH EXPANSION JOINTS SHALL BE INSTALLED AT ALL CURB RETURNS, CURB CUTS AND EXISTING STRUCTURES. CONTROL JOINTS SHALL BE INSTALLED PER THE TOWNS STANDARDS AND SPECIFICATIONS.
- THE GRADATION OF THE MINERAL AGGREGATE WILL BE GRADING SX (1/2" NOMINAL) FOR ALL TOP LIFTS AND OVERLAYS.
- TACK COAT SHALL BE USED PRIOR TO OVERLAY, (CSS-1H), 50:50 DILUTION, 0.10 GAL/SY. ALL EDGES ABUTTING NEW PAVEMENT SHALL BE TACKED.
- WHEN IT IS REQUIRED TO MATCH EXISTING PAVEMENT, EXISTING PAVEMENT SHALL BE SAW CUT IN A MANNER TO AFFECT A SMOOTH, VERTICAL STRAIGHT CUT EDGE. T PATCH MILLING MUST BE DONE PER STANDARD DETAILS.
- ALL SAWCUT EDGES OF EXISTING PAVEMENT SHALL BE CLEAN AND COATED WITH TACK COAT PRIOR TO PLACING NEW PAVEMENT ADJACENT TO THE EXISTING PAVEMENT.
- ALL ASPHALT SHALL BE ONE FOURTH (1/4) INCH ABOVE CONCRETE EDGES, MANHOLE COVERS AND ACCESS LIDS.
- SIGNAGE AND STRIPING SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COLORADO DEPARTMENT OF TRANSPORTATION M&S STANDARDS, AND THE TOWN OF ERIE STANDARD DESIGN CRITERIA AND STANDARD CONSTRUCTION REQUIREMENTS.
- THE PURCHASE AND INSTALLATION OF STREET NAME SIGNS SHALL BE THE RESPONSIBILITY OF THE OWNER/CONTRACTOR. THE OWNER/CONTRACTOR SHALL SECURE THE APPROVAL OF THE TOWN OF ERIE ENGINEERING STAFF FOR TYPE AND LOCATION OF THE STREET NAME SIGNS PRIOR TO INSTALLATION.
- ALL NEW ROADWAY SECTIONS SHALL HAVE SUBGRADE PREPARATION AND INITIAL ASPHALT PAVEMENT PLACED WITH A 1% CROWN. FINAL OVERLAY IS TO BE PLACED WITH A 2% CROWN. SEE DETAIL S17 IN THE "STANDARD DETAILS-STREET" FOR MORE INFORMATION.
- DETERMINATION OF CROWN FOR CUL DE SAC PAVING SHALL BE EVALUATED ON A CASE BY CASE BASIS.

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the built environment

For
Submittal

Lot 1 Erie Air Park Replat D

3020-3100 Airport Drive
Erie, Colorado 80516

#	Date	Description
1	05/22/23	Initial Submittal
2	07/28/23	SIP Submittal
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

General Notes

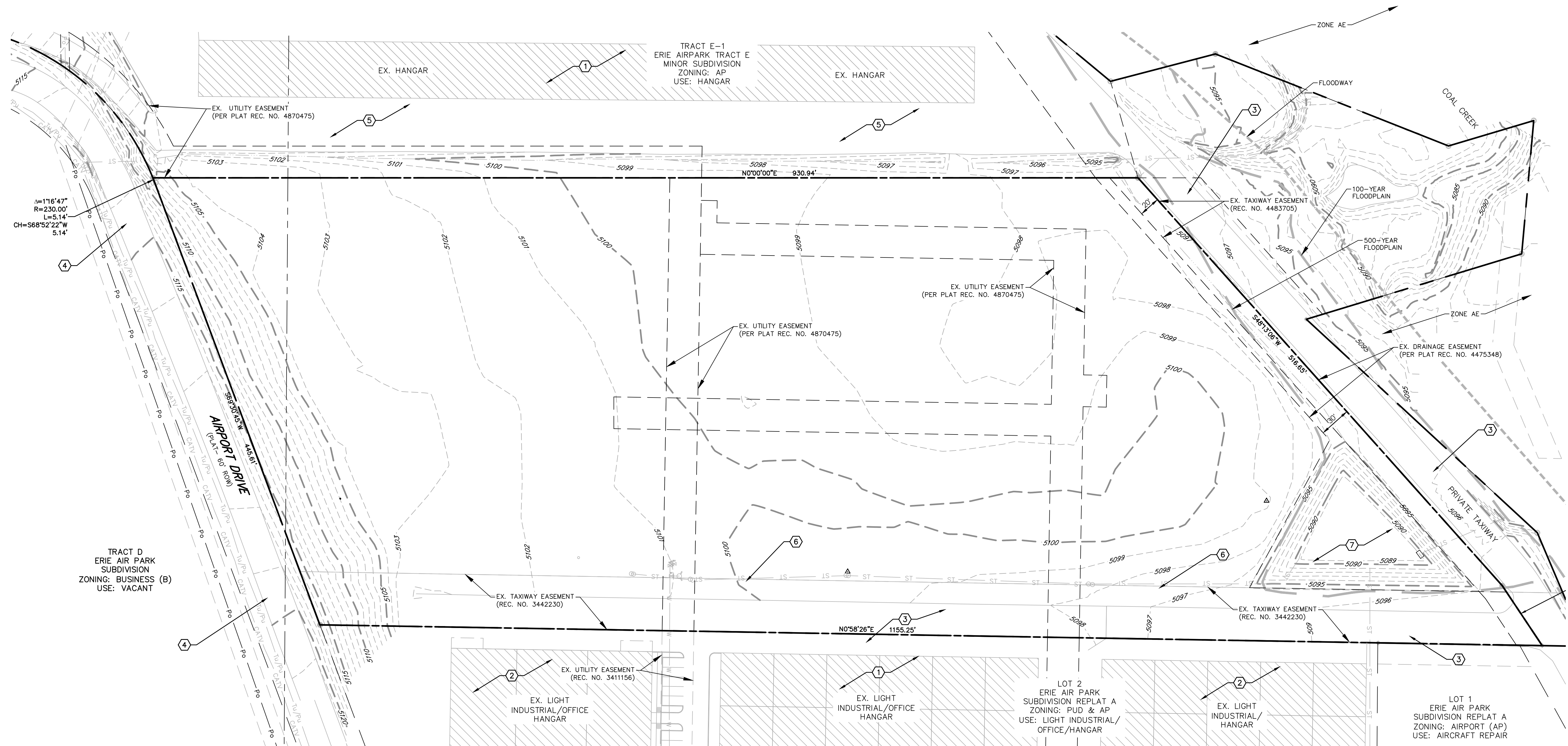
Sheet

C-003

3020-3100 Airport Drive
Erie, Colorado 80516

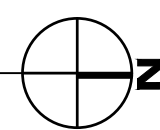
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Sheet




1

SCALE: 1" = 50'



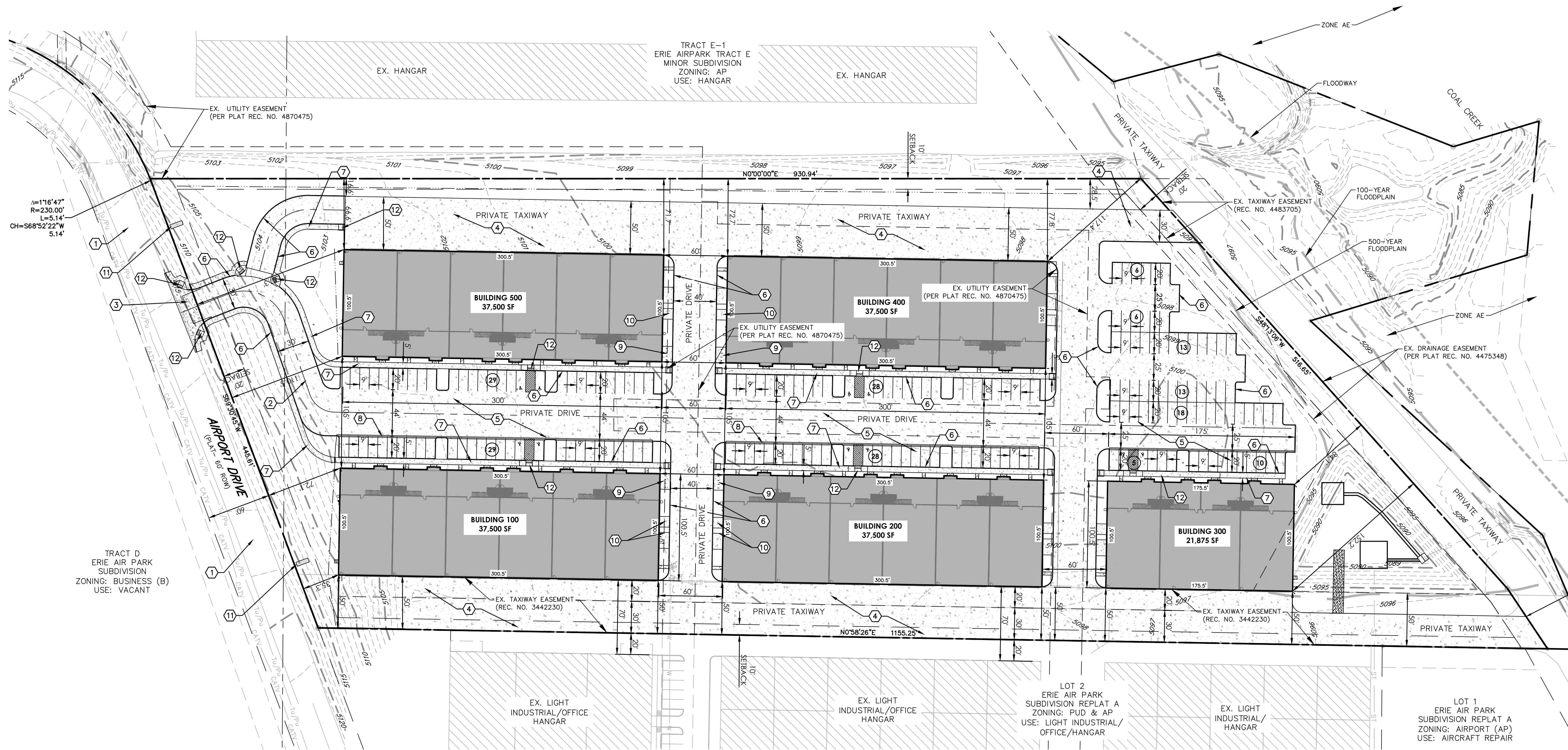
Scale: 1" = 50'



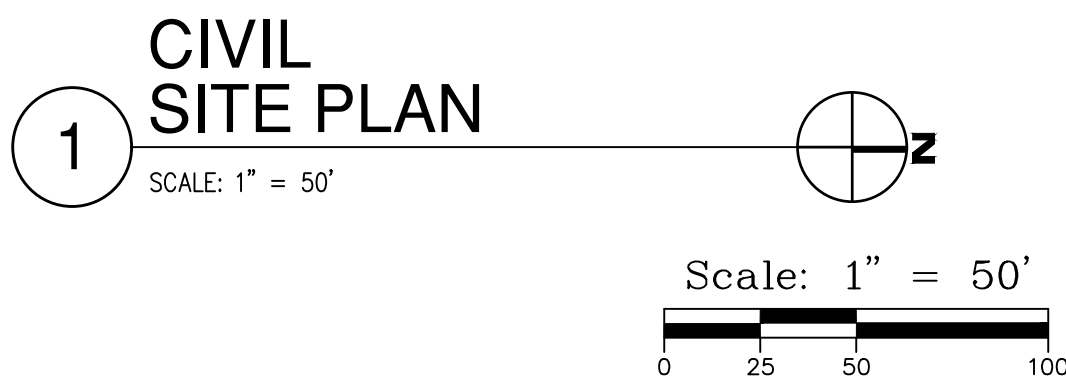
A horizontal scale bar with a black and white alternating pattern. It is marked with the numbers 0, 25, 50, and 100, representing feet.

1.	EXISTING AIRCRAFT HANGARS TO REMAIN.
2.	EXISTING LIGHT INDUSTRIAL BUILDINGS TO REMAIN.
3.	EXISTING TAXIWAY TO REMAIN.
4.	EXISTING ASPHALT ROADWAY TO REMAIN.
5.	EXISTING AIRCRAFT APRON TO REMAIN.
6.	EXISTING STORM DRAIN PIPE TO BE REMOVED.
7.	EXISTING STORMWATER QUALITY POND.

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- NOTES:
- EV PARKING SPACES WILL BE PROVIDED IN ACCORDANCE WITH IBC AND IECC 2021.
1. MINIMUM NUMBER OF SPACES WITH EVSE INSTALLED (2% OF 185 PROVIDED) = 4 SPACES REQUIRED/4 SPACES PROVIDED.
 2. MINIMUM NUMBER OF EV-READY SPACES (8% OF 185 PROVIDED) = 15 SPACES REQUIRED/16 SPACES PROVIDED.
 3. MINIMUM NUMBER OF EV-CAPABLE SPACES (10% OF 185 PROVIDED) = 19 SPACES REQUIRED/20 SPACES PROVIDED.
 4. MINIMUM NUMBER OF EV CAPABLE LIGHT SPACE (10% OF 185 PROVIDED) = 19 SPACES/20 SPACES PROVIDED.



KEYED NOTES	
1.	EXISTING ASPHALT ROADWAY TO REMAIN.
2.	PROPOSED CONCRETE DRIVEWAY.
3.	PROPOSED STANDARD DRIVE ENTRANCE.
4.	PROPOSED CONCRETE TAXIWAY.
5.	PROPOSED CONCRETE PARKING AREA.
6.	PROPOSED CURB AND GUTTER.
7.	PROPOSED CONCRETE SIDEWALK.
8.	PROPOSED 2' CONCRETE DRAIN PAN.
9.	PROPOSED BICYCLE RACKS ON CONCRETE SLAB.
10.	PROPOSED TRASH ENCLOSURE.
11.	PROPOSED MONUMENT SIGN.
12.	ADA CURB RAMP.

For
Submittal

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Replat D**
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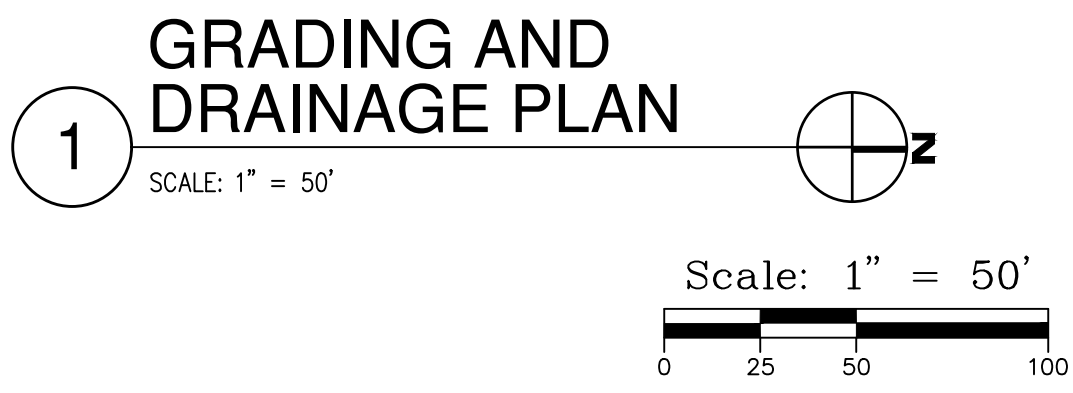
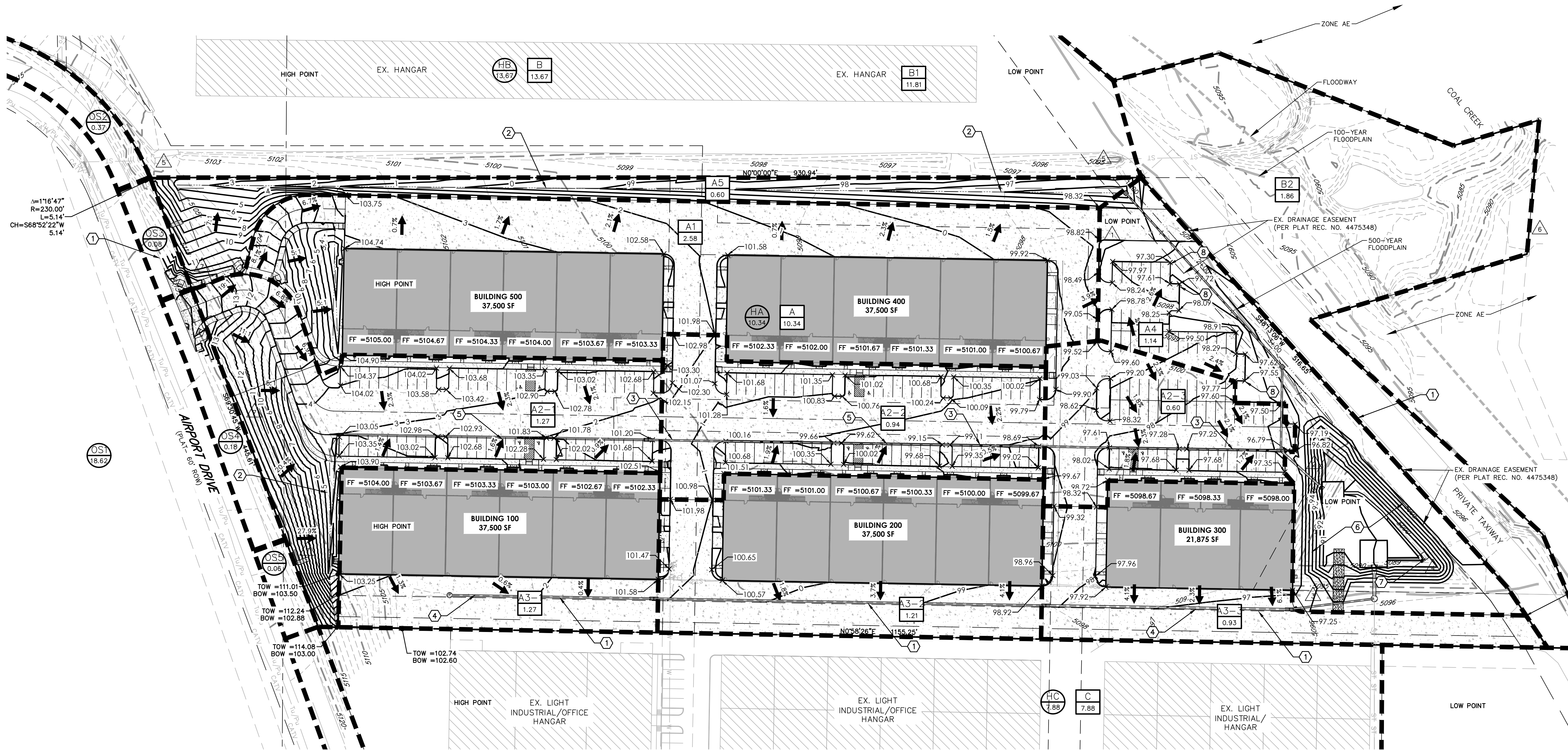
Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Site Plan

Sheet

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KEYED NOTES	
1.	SAW CUT AND MATCH GRADE AT EXISTING ASPHALT.
2.	PROPOSED DRAINAGE SWALE.
3.	PROPOSED STORM SEWER.
4.	PROPOSED STORM SEWER FOR ROOF DRAINS.
5.	PROPOSED 2' CONCRETE DRAIN PAN.
6.	PROPOSED STORMWATER QUALITY/DETENTION FACILITY.
7.	PROPOSED LIMITED RELEASE OUTLET STRUCTURE.
8.	PROPOSED 1' CURB CUT.

For
Submittal

**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

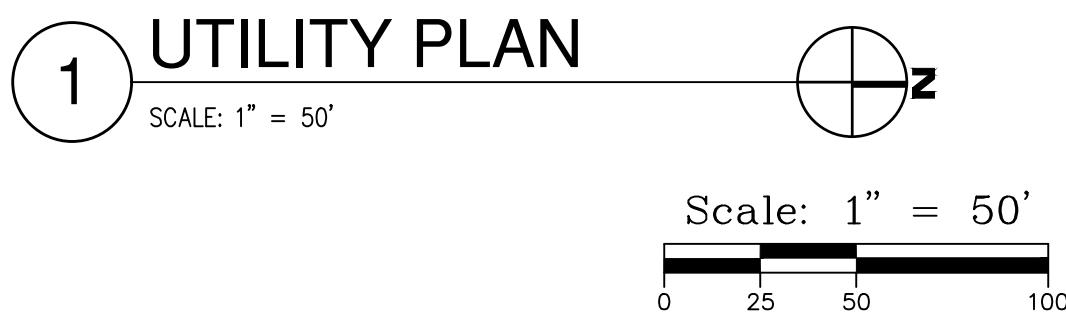
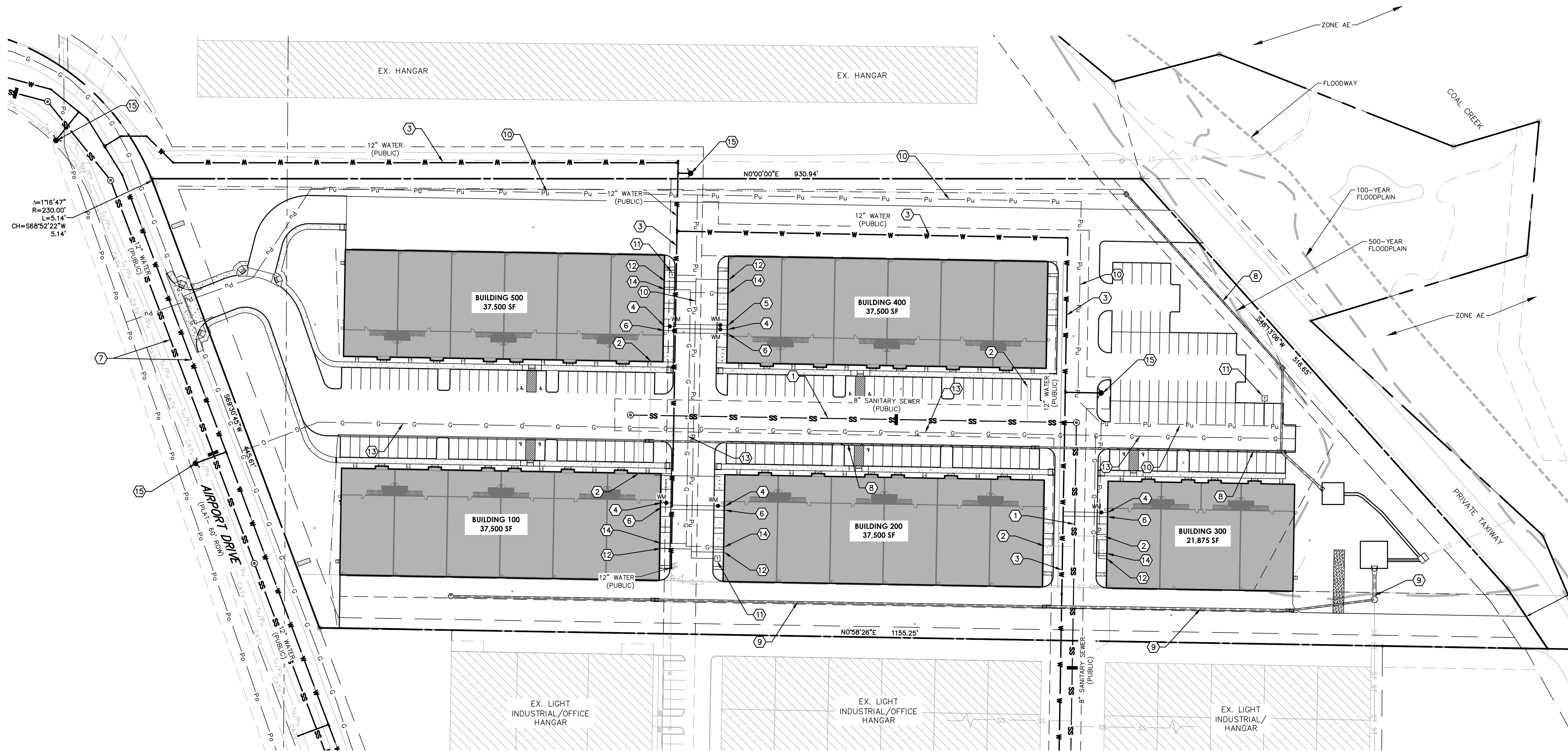
#	Date	Description
1	05/22/23	Initial Submittal
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Project No: 23126A
By: JAS/DPA
File: 23126A-1

**Grading and
Drainage Plan**

Sheet

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KEYED NOTES	
1.	PROPOSED 12" SANITARY SEWER MAIN. SEE CIVIL ENGINEERING CONSTRUCTION PLAN SET.
2.	PROPOSED 4" SANITARY SEWER SERVICE AT 2% SLOPE.
3.	PROPOSED 12" WATER MAIN. SEE CIVIL ENGINEERING CONSTRUCTION PLAN SET.
4.	PROPOSED DOMESTIC WATER SERVICE, METER AND METER PIT.
5.	PROPOSED IRRIGATION SERVICE, METER AND METER PIT.
6.	PROPOSED FIRE SERVICE. PROVIDE 6' X 10' FIRE ENTRY ROOM WITH EXTERIOR DOOR.
7.	SEWER AND WATER MAIN EXTENSIONS DOWN AIRPORT DRIVE TO TRACT E-1 AND E-3. SEE CIVIL ENGINEERING CONSTRUCTION PLAN SET.

8.	PROPOSED STORM SEWER SYSTEM.
9.	PROPOSED STORM SEWER FOR ROOF DRAINS.
10.	PROPOSED ELECTRICAL SERVICE WITH LANDSCAPE SCREENING PER CHAPTER 6 OF THE UDC. SEE LANDSCAPE PLANS.
11.	PROPOSED ELECTRICAL TRANSFORMER.
12.	PROPOSED ELECTRIC METERS AND DISCONNECT WITH LANDSCAPE SCREENING PER CHAPTER 6 OF THE UDC. SEE LANDSCAPE PLANS.
13.	PROPOSED GAS SERVICE.
14.	PROPOSED GAS METERS WITH LANDSCAPE SCREENING PER CHAPTER 6 OF THE UDC. SEE LANDSCAPE PLANS.
15.	PROPOSED FIRE HYDRANT LATERAL.

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Project No: 23126A
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File: 23126A-1

Utility Plan

Sheet

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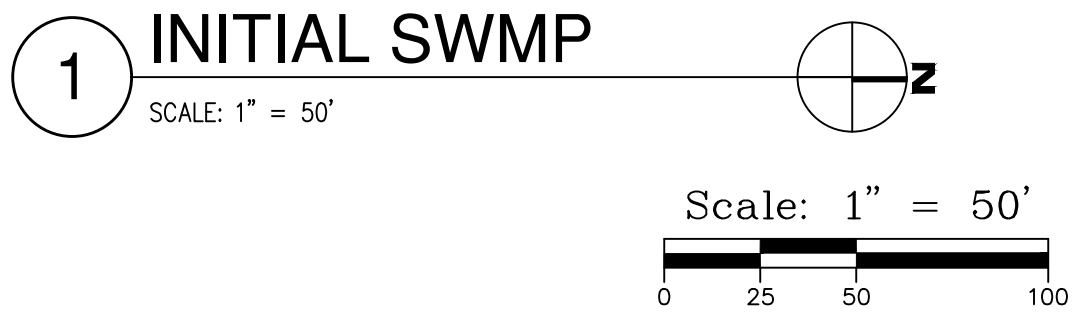
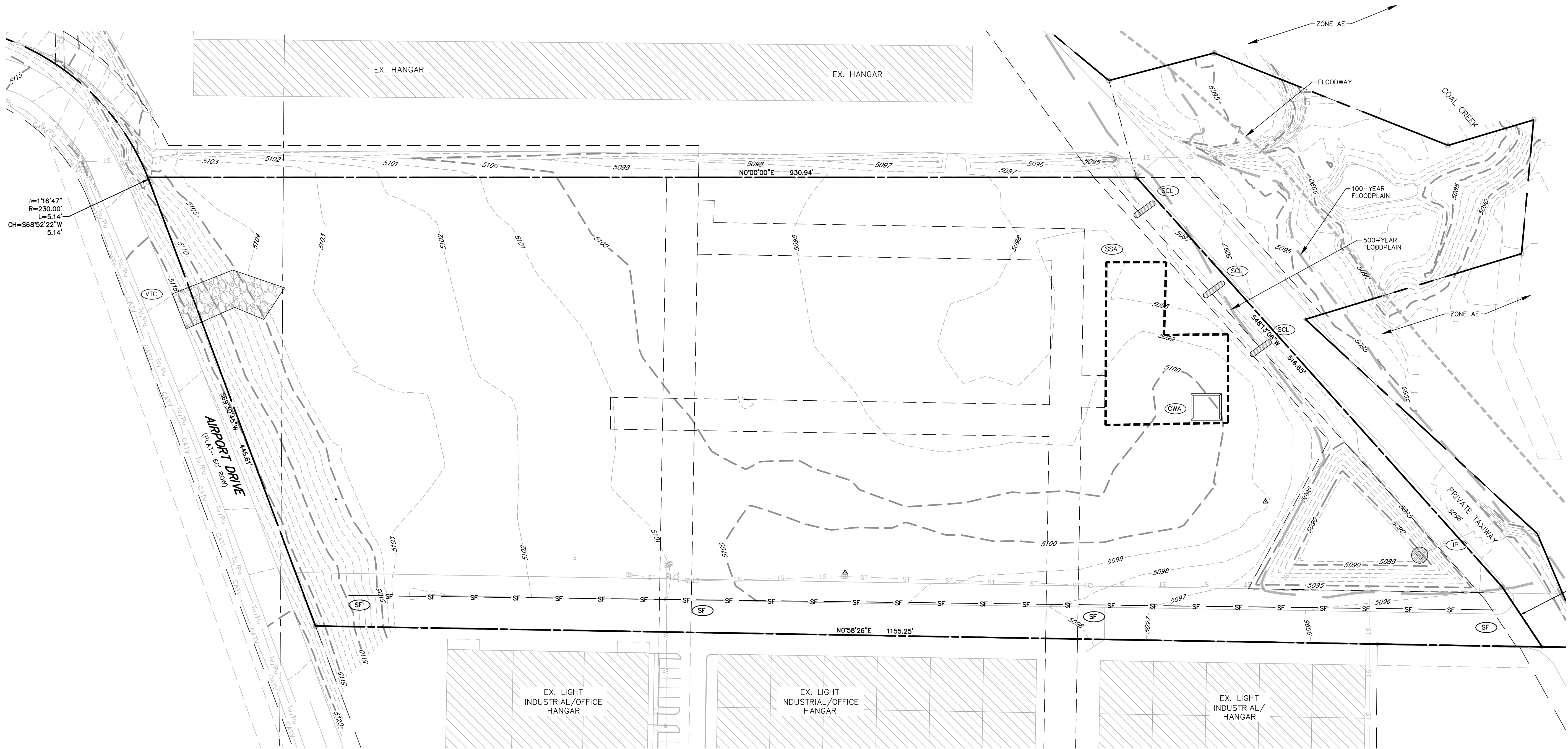
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Turning Analysis

Sheet

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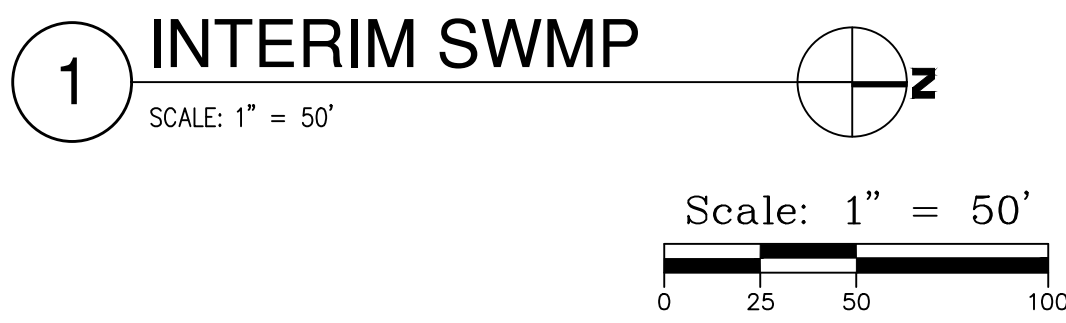
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By: JAS/DPA
File: 23126A-1

Initial Stormwater
Management Plan

Sheet

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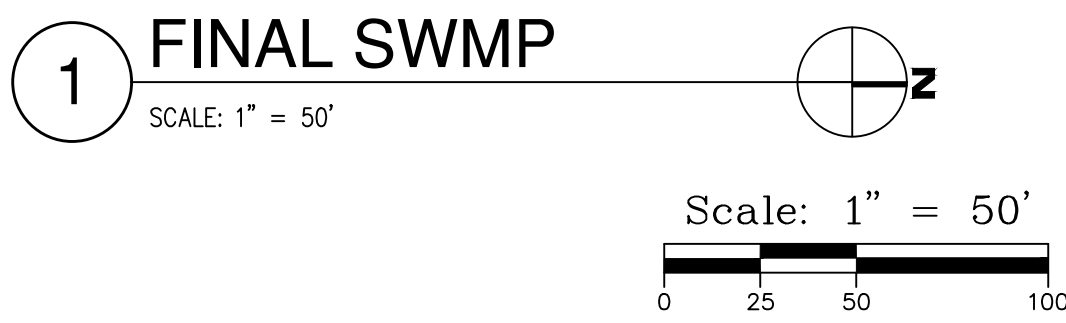
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By: JAS/DPA
File: 23126A-1

Interim
Stormwater
Management Plan

Sheet

C-202

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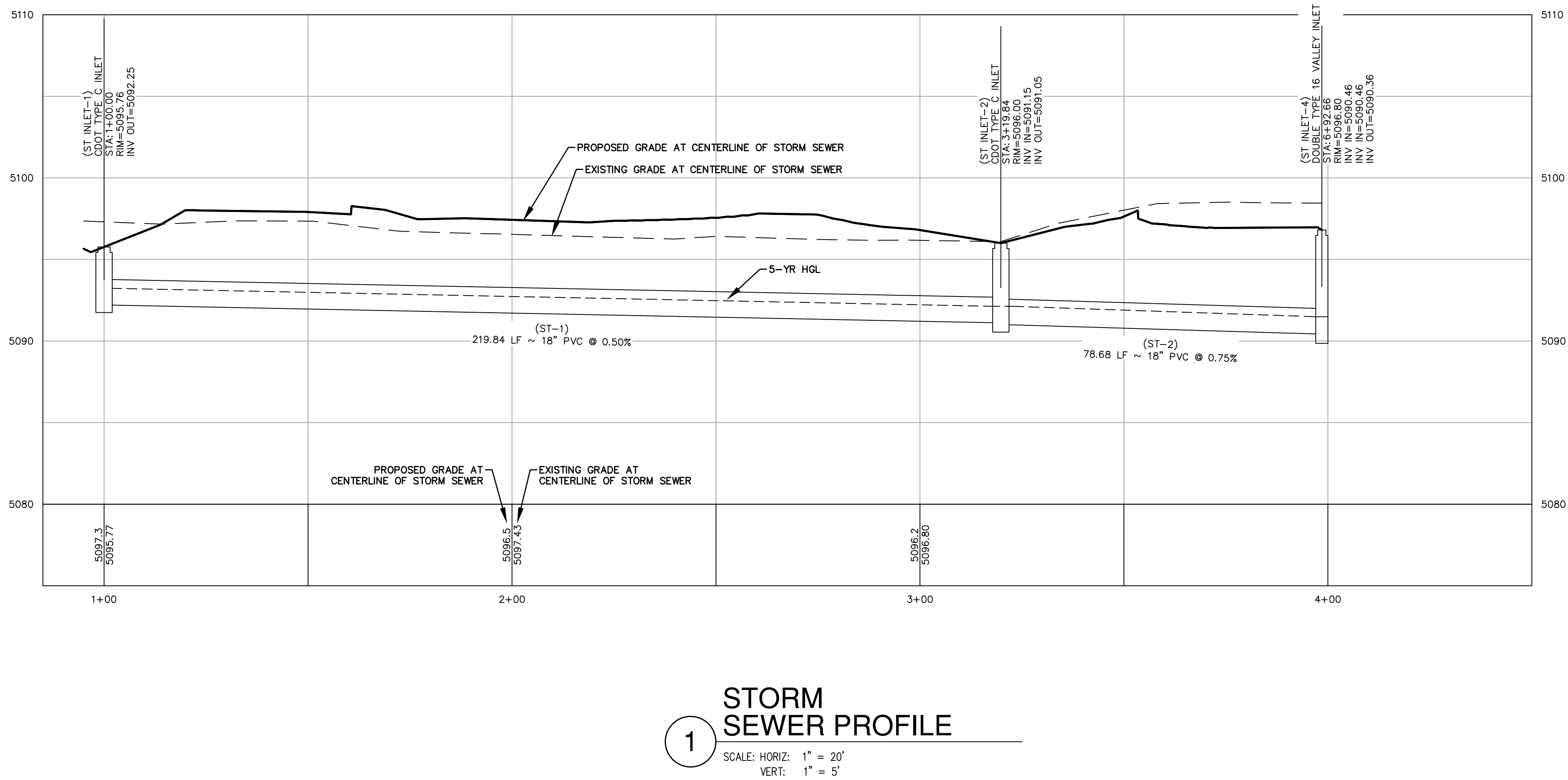
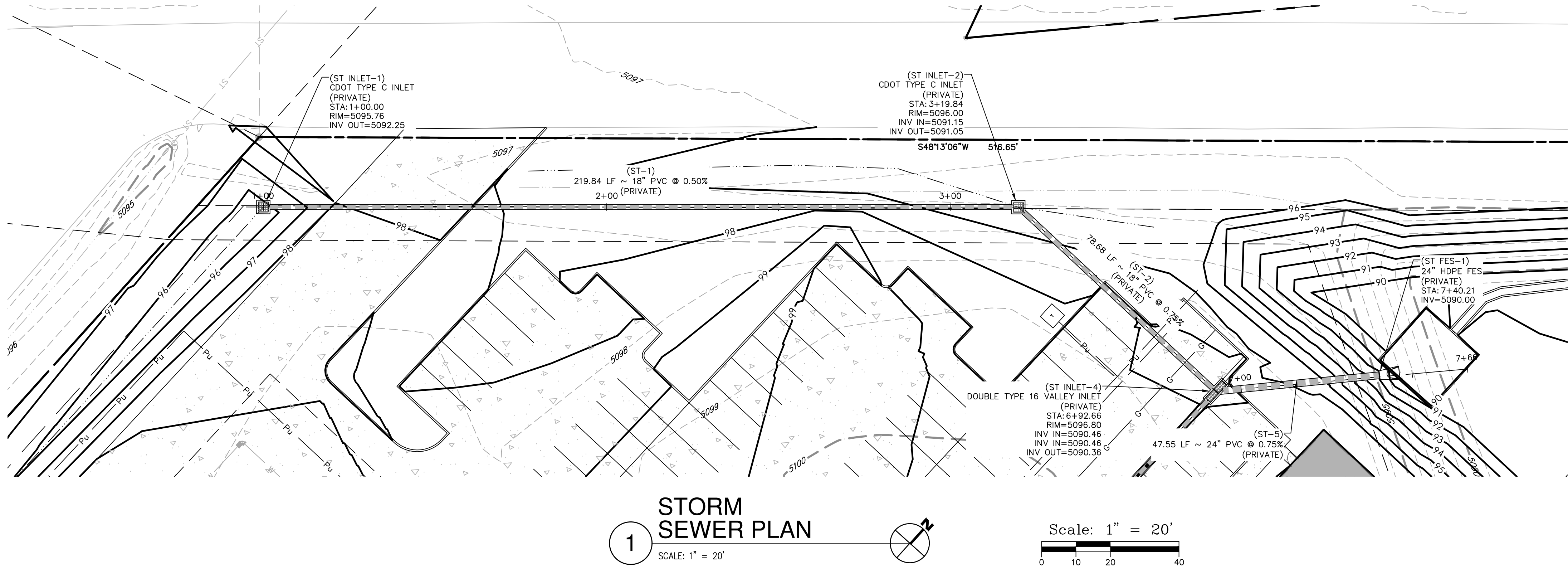
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By: JAS/DPA
File: 23126A-1

Final Stormwater
Management Plan

Sheet

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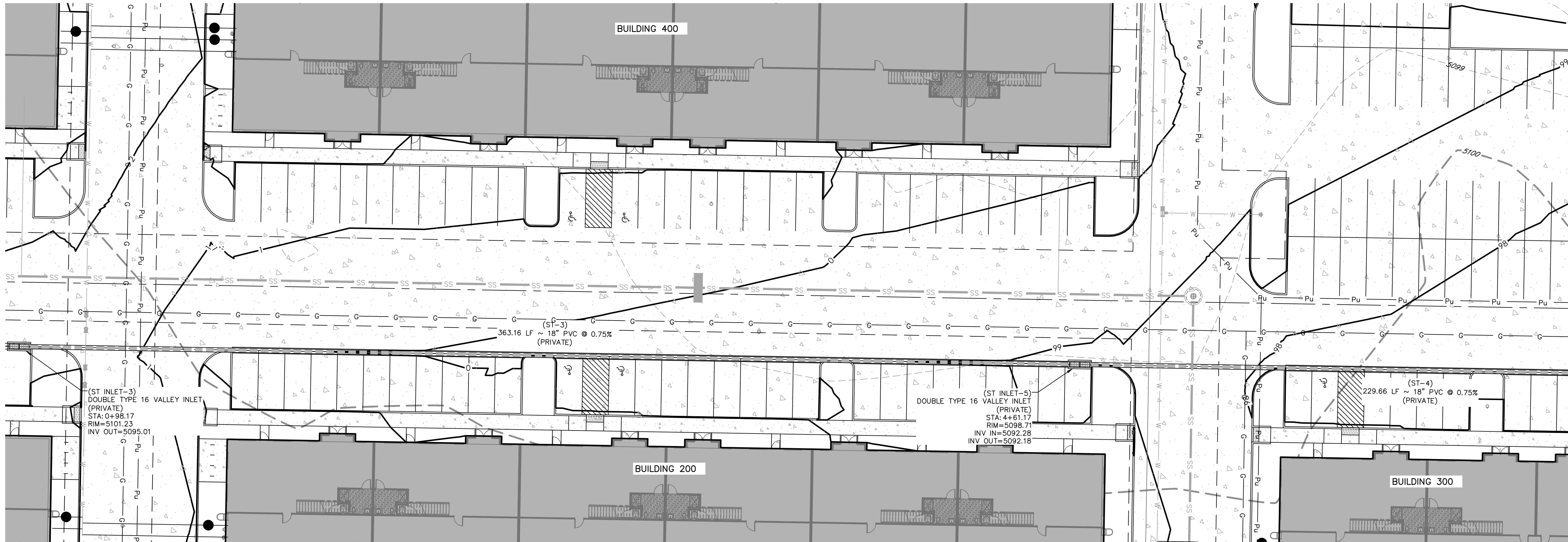
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By: JAS/DPA
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Storm Sewer Plan and Profile

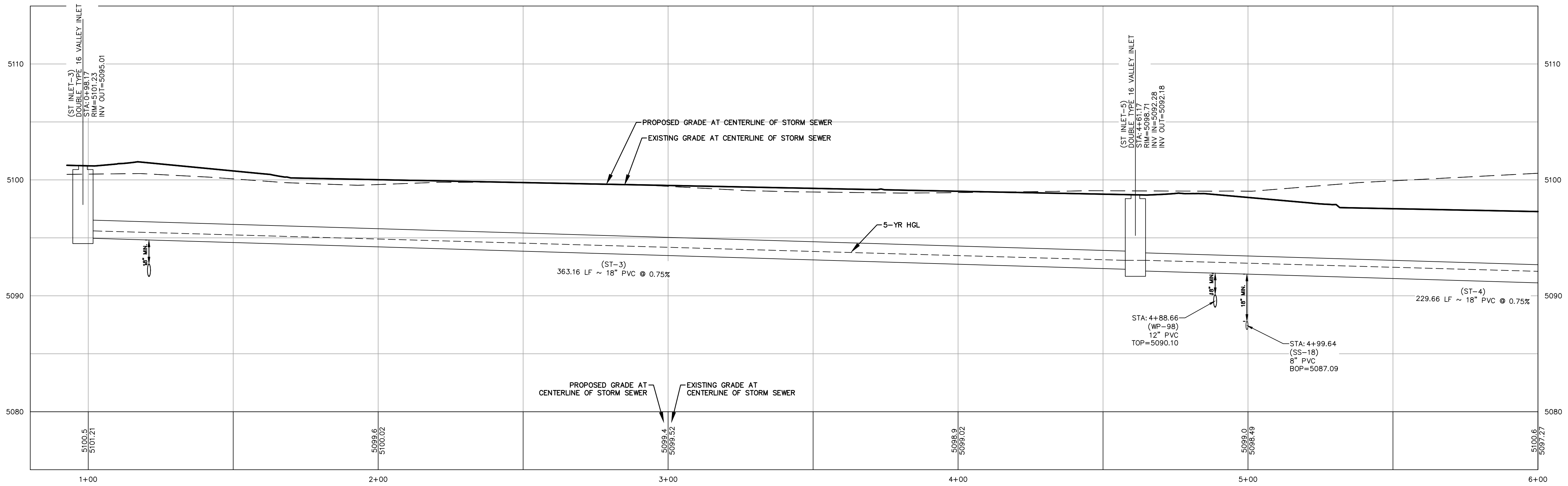
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1 STORM SEWER PLAN
SCALE: 1" = 20'



1 STORM SEWER PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

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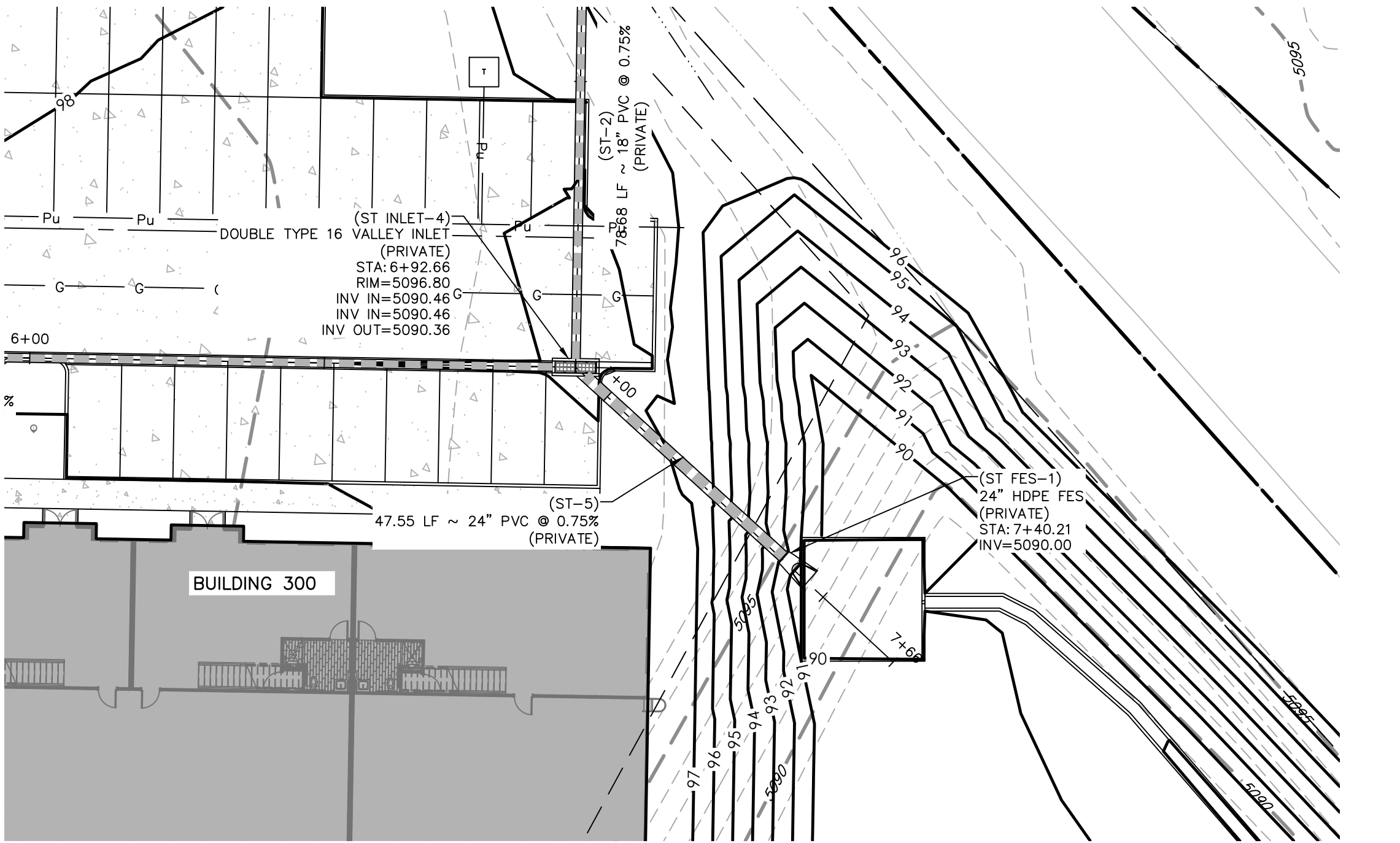
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By: JAS/DPA
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Storm Sewer Plan and Profile

Sheet

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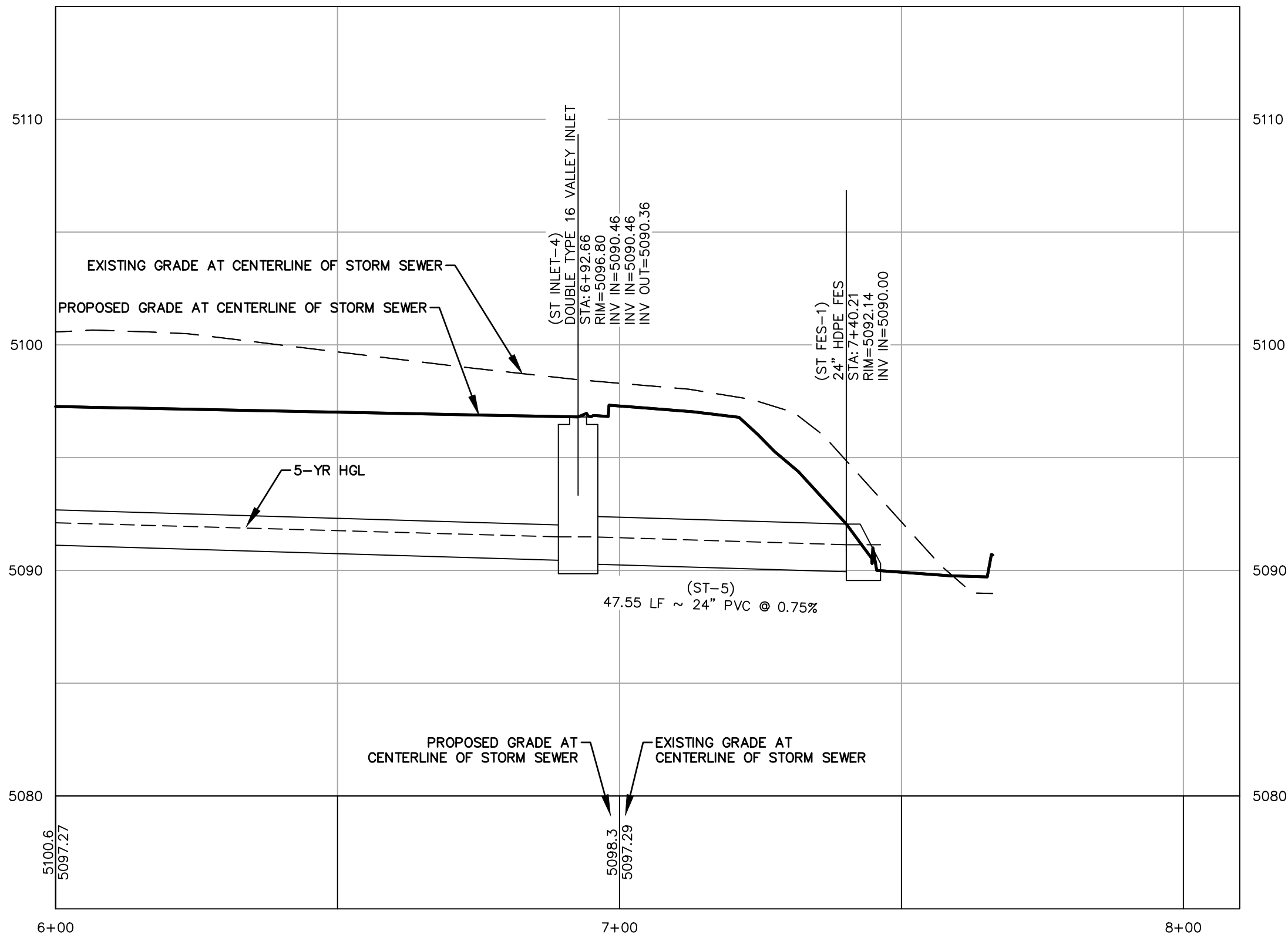


1 STORM SEWER PLAN

SCALE: 1" = 20'

Scale: 1" = 20'

0 10 20 40



1 STORM SEWER PROFILE

SCALE: HORIZ: 1" = 20'

VERT: 1" = 5'

For
Submittal

Lot 1 Erie Air Park Replat D

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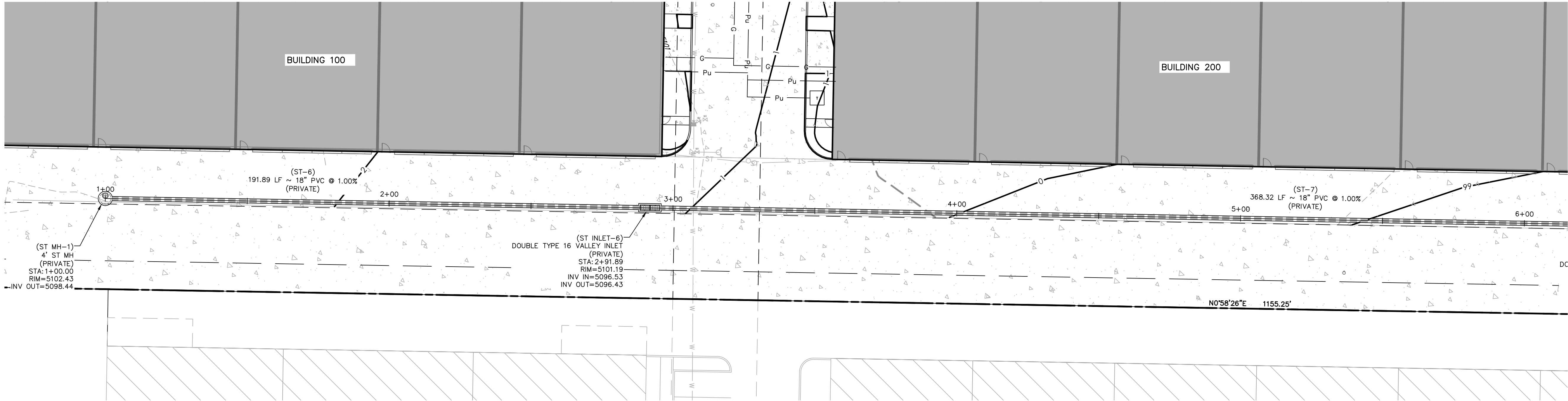
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By: JAS/DPA
File: 23126A-1

Storm Sewer Plan and Profile

Sheet

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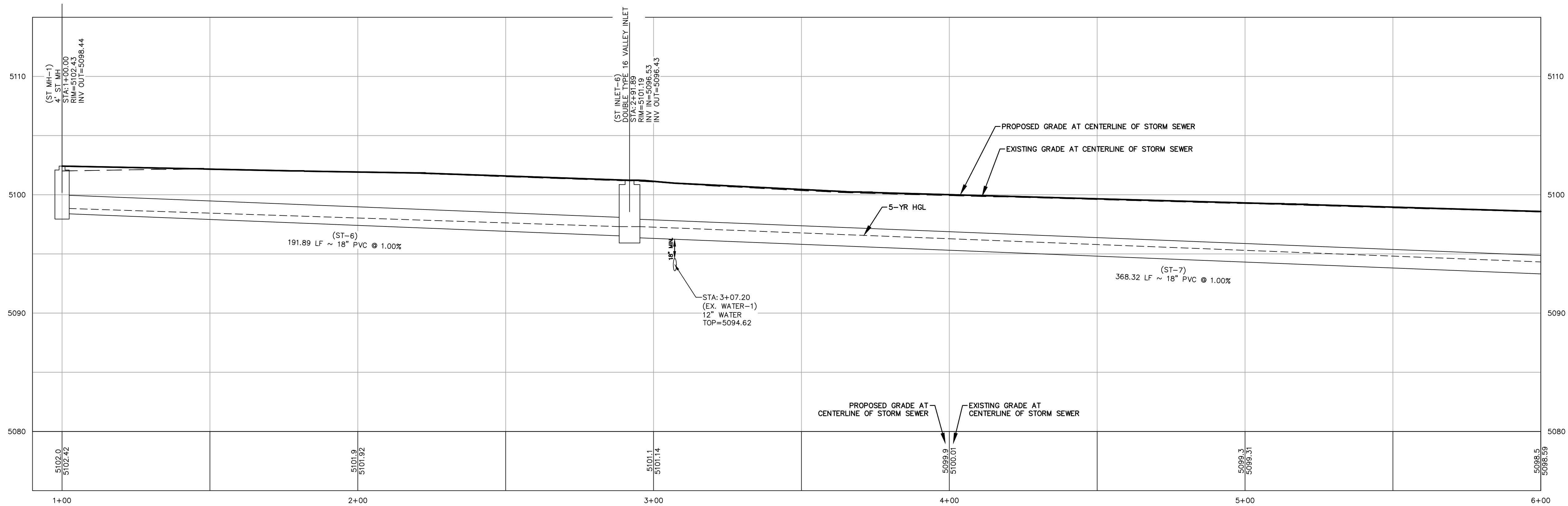


1 STORM SEWER PLAN

SCALE: 1" = 20'

Scale: 1" = 20'

0 10 20 40

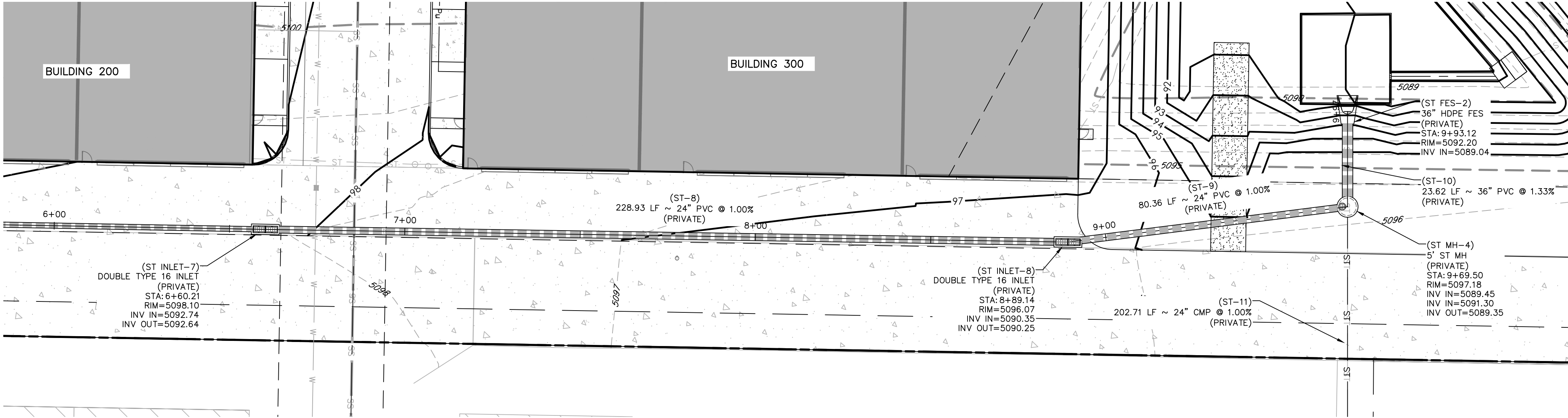


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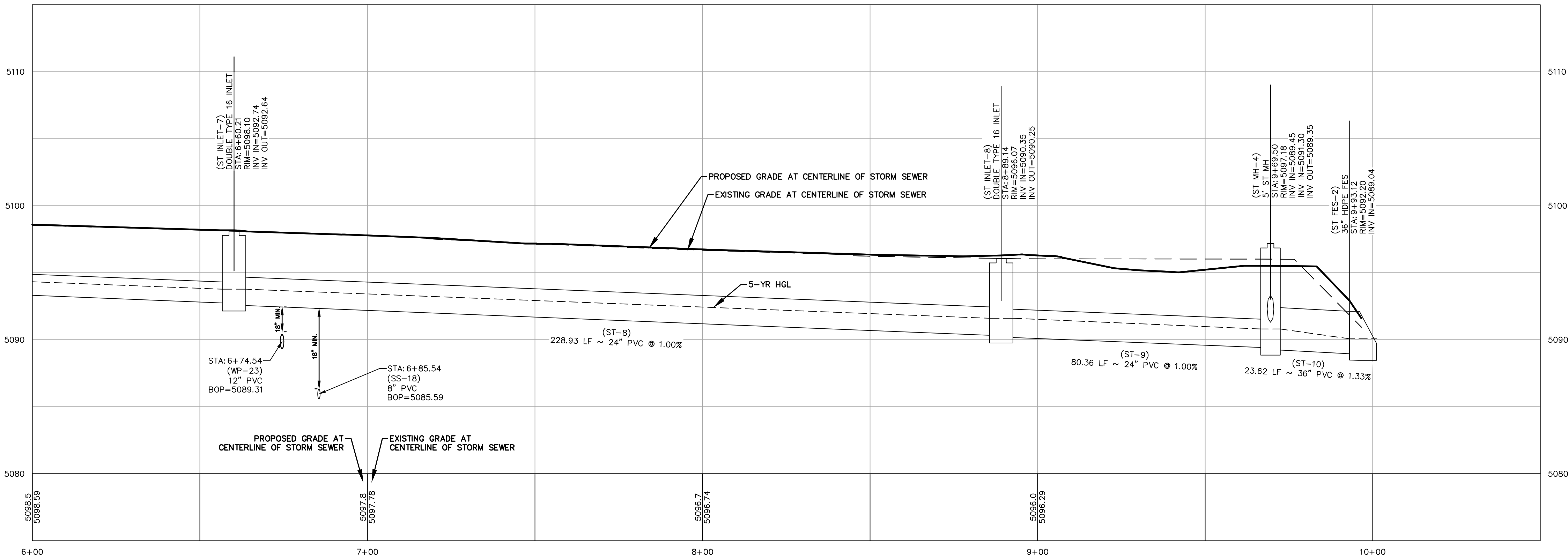
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1 STORM SEWER PLAN
SCALE: 1" = 20'



1 STORM SEWER PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

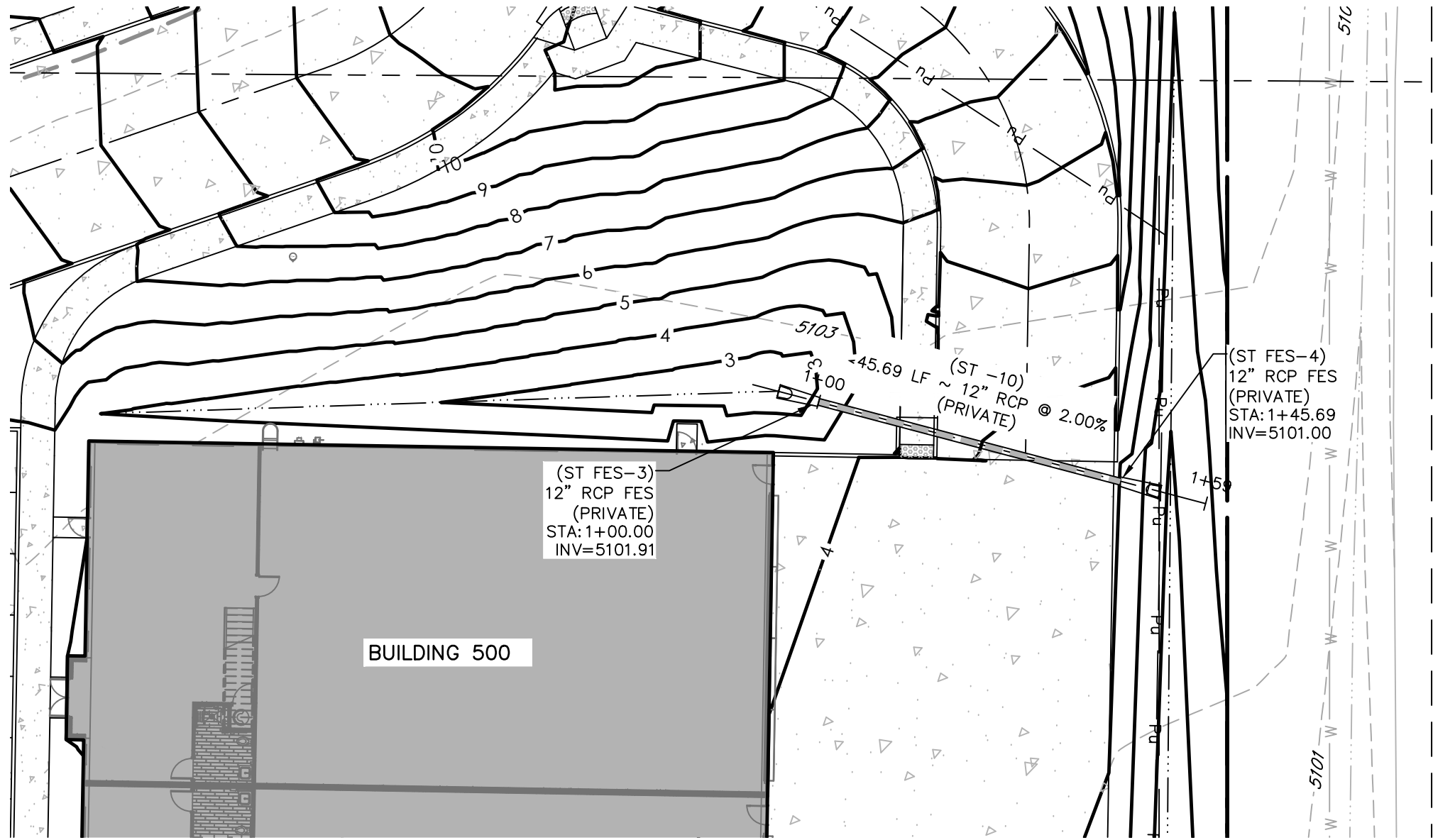
**Lot 1
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Project No: 23126A
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File: 23126A-1

**Storm Sewer Plan
and Profile**

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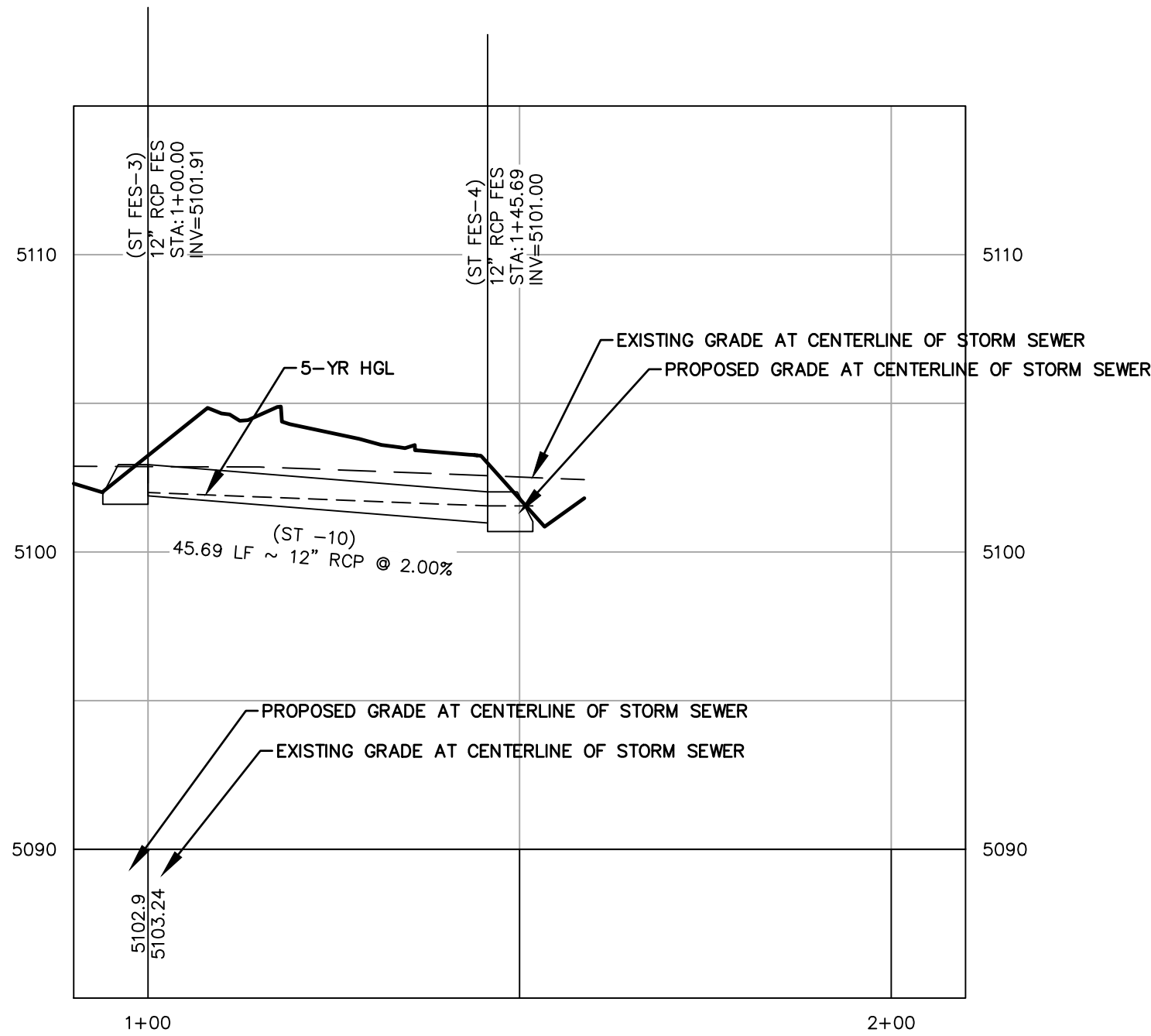


1 STORM SEWER PLAN

SCALE: 1" = 20'

Scale: 1" = 20'

0 10 20 40



1 STORM SEWER PROFILE

SCALE: HORIZ: 1" = 20'

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File: 23126A-1

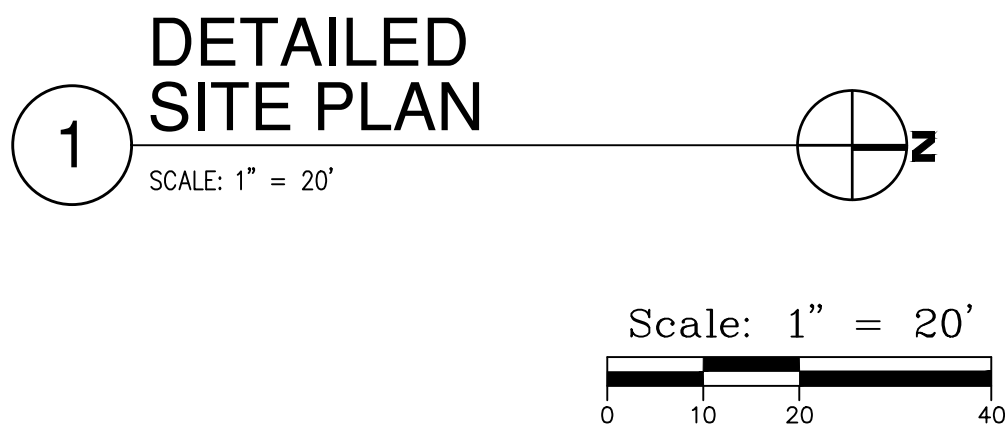
Storm Sewer Plan
and Profile

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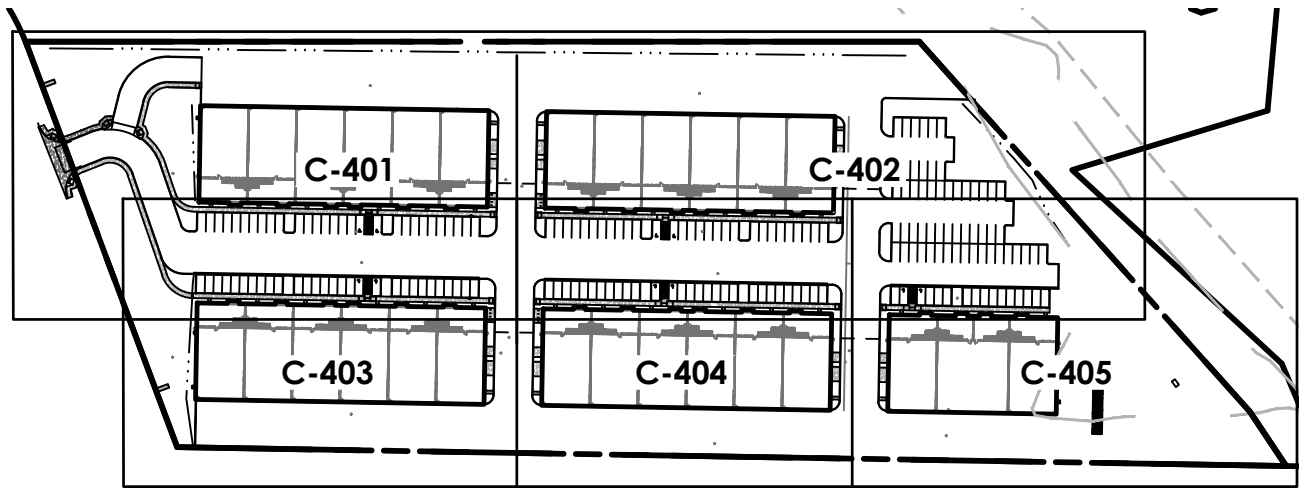


LEGEND	
	PROPOSED CATCH CURB
	PROPOSED SPILL CURB

- NOTES:
- EV PARKING SPACES WILL BE PROVIDED IN ACCORDANCE WITH IBC AND IECC 2021.
- MINIMUM NUMBER OF SPACES WITH EVSE INSTALLED (2% OF 185 PROVIDED) = 4 SPACES REQUIRED/4 SPACES PROVIDED.
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 - MINIMUM NUMBER OF EV CAPABLE LIGHT SPACE (10% OF 185 PROVIDED) = 19 SPACES/20 SPACES PROVIDED.



KEYED NOTES	
1.	EXISTING ASPHALT ROADWAY TO REMAIN.
2.	PROPOSED CONCRETE DRIVEWAY.
3.	PROPOSED STANDARD DRIVE ENTRANCE.
4.	PROPOSED CONCRETE TAXIWAY.
5.	PROPOSED CONCRETE PARKING AREA.
6.	PROPOSED CURB AND GUTTER.
7.	PROPOSED CONCRETE SIDEWALK.
8.	PROPOSED 2' CONCRETE DRAIN PAN.
9.	PROPOSED BICYCLE RACKS ON CONCRETE SLAB.
10.	PROPOSED TRASH ENCLOSURE WITH FLUSH CURB.
11.	PROPOSED 1' CURB CUT.
12.	ADA CURB RAMP.
13.	TWO (2) PROPOSED ADA PARKING SPACES WITH VAN ACCESSIBLE AND ADA SIGNAGE.
14.	CONCRETE ACCESS FOR OVERHEAD DOOR WITH FLUSH CURB.
15.	PARKING SPACE WITH THE REQUIRED NUMBER OF ELECTRIC VEHICLE SUPPLY EQUIPMENT (EVSE) INSTALLED.
16.	PARKING SPACE WITH THE REQUIRED NUMBER OF EV-READY AND EV-CAPABLE EQUIPMENT INSTALLED.



KEY MAP

SCALE: 1" = 200'

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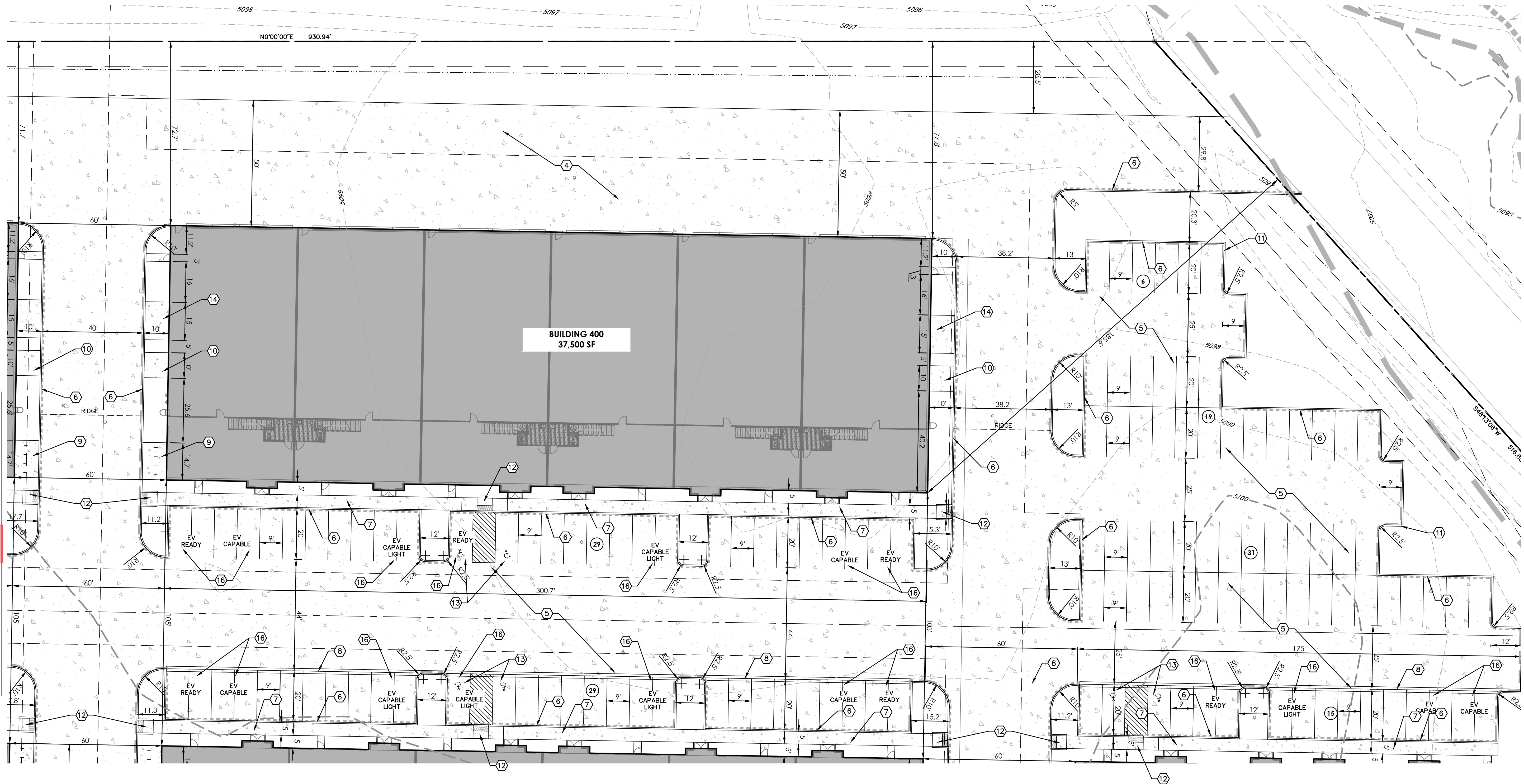
Project No: 23126A
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File: 23126A-1

Detailed Site Plan

Sheet

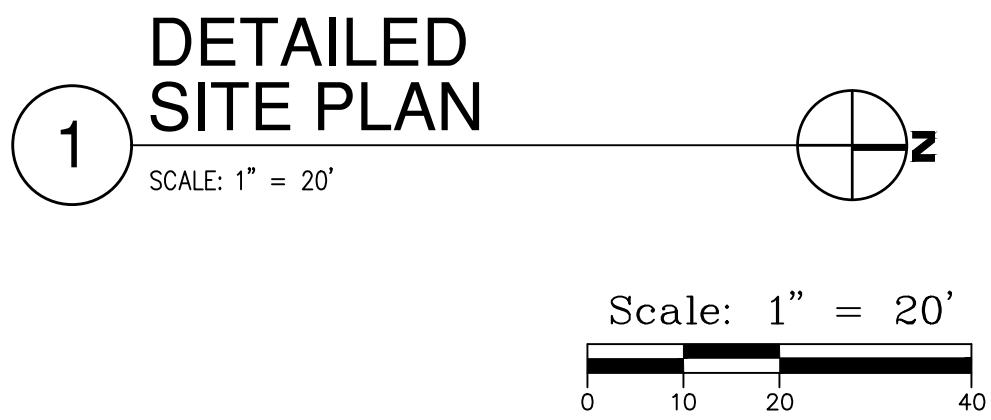
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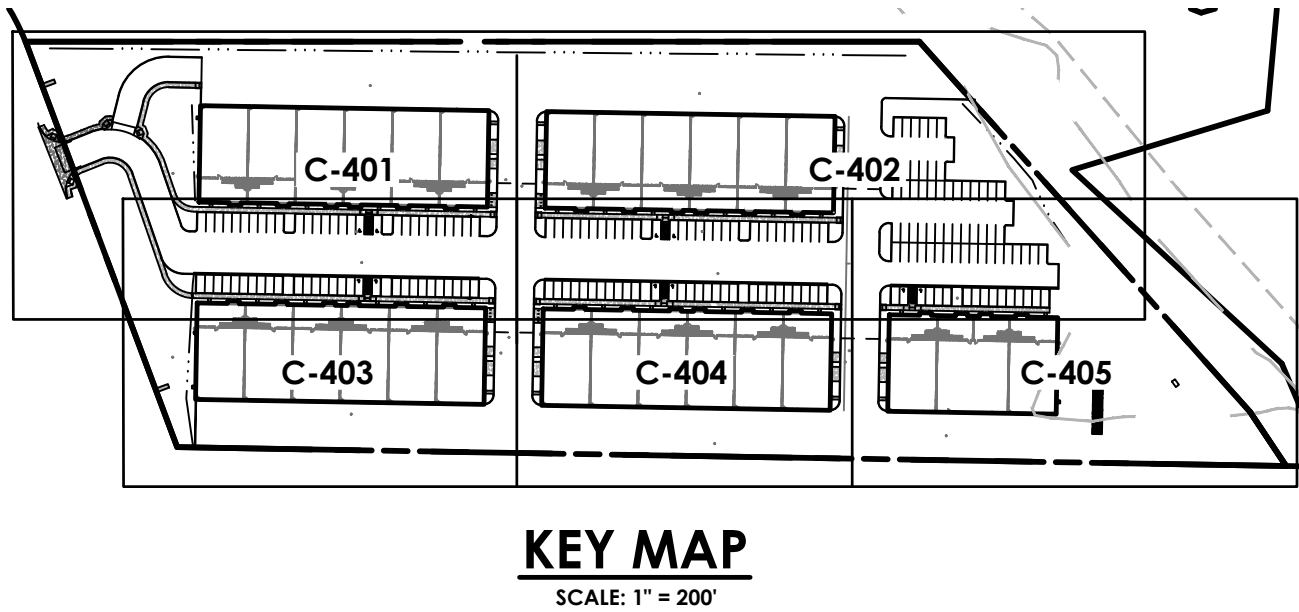


LEGEND	
	PROPOSED CATCH CURB
	PROPOSED SPILL CURB

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KEYED NOTES	
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KEY MAP

SCALE: 1" = 200'

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Project No: 23126A
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File: 23126A-1

Detailed Site Plan

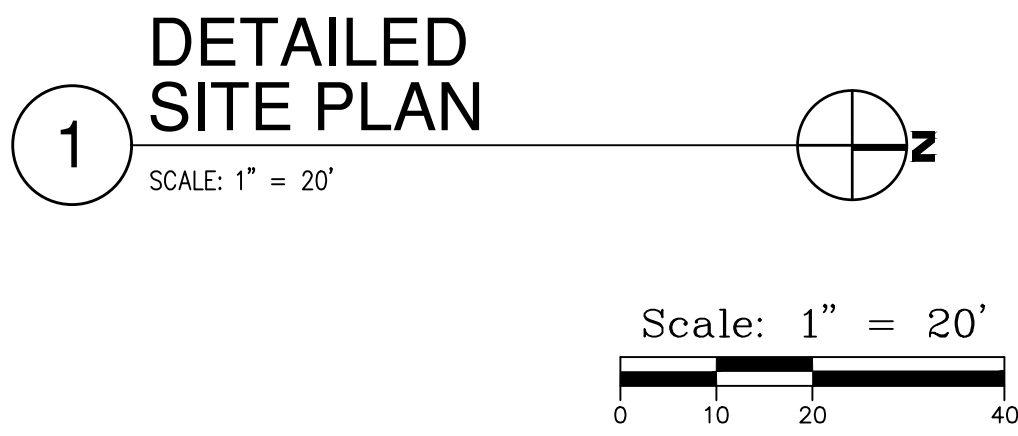
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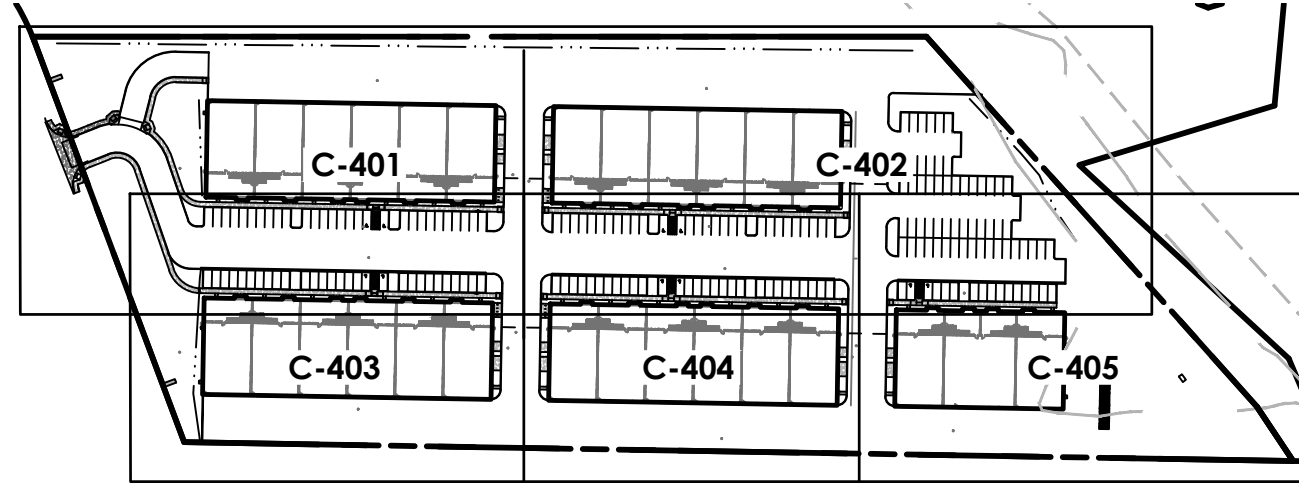


LEGEND	
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----	PROPOSED SPILL CURB

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KEY MAP

SCALE: 1" = 200'

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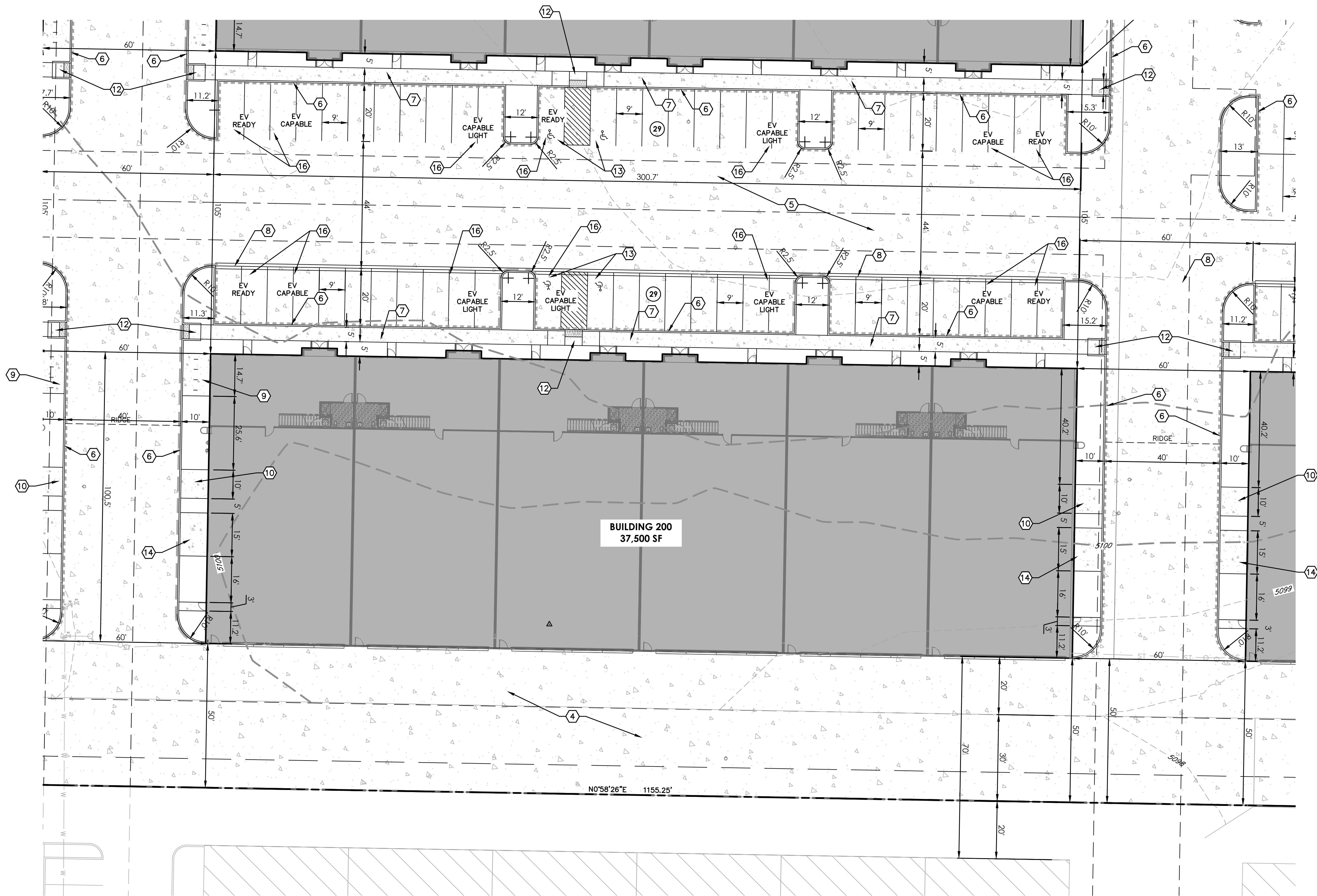
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By: JAS/DPA
File: 23126A-1

Detailed Site Plan

Sheet

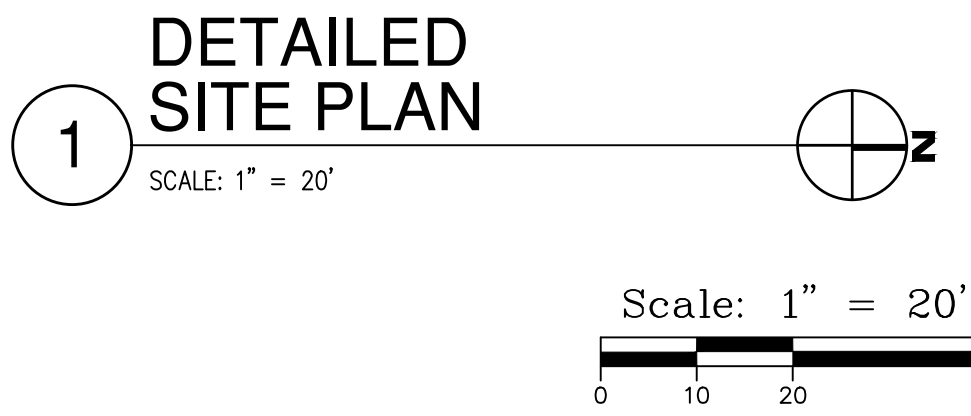
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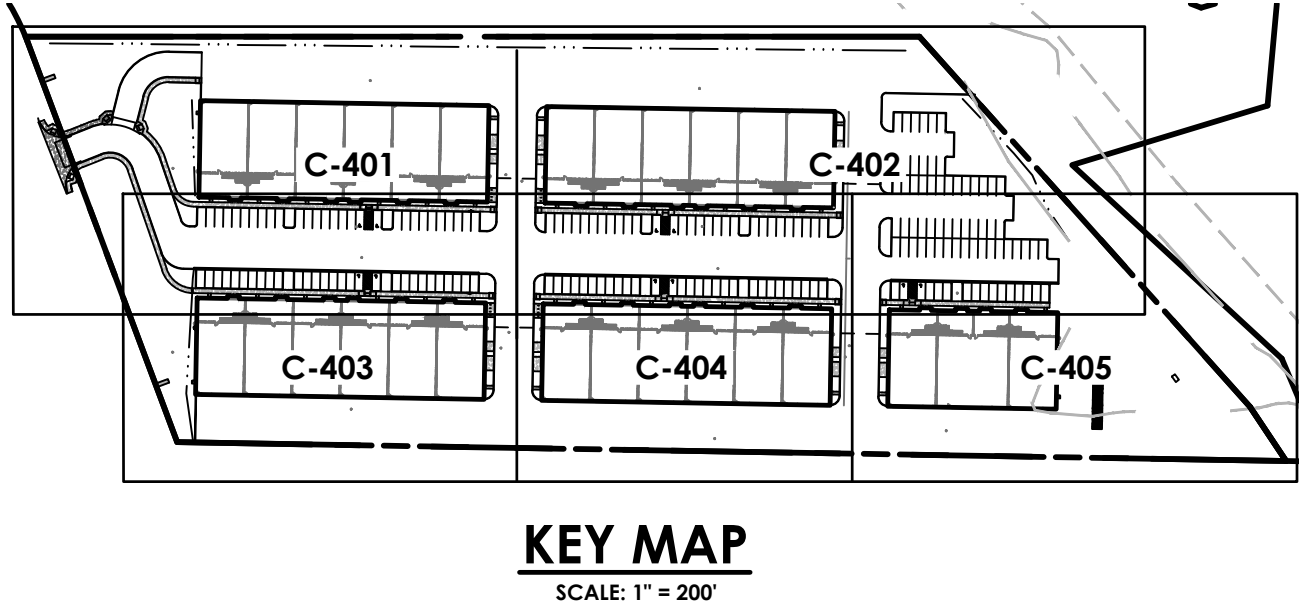


LEGEND	
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----	PROPOSED SPILL CURB

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- MINIMUM NUMBER OF SPACES WITH EVSE INSTALLED (2% OF 185 PROVIDED) = 4 SPACES REQUIRED/4 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV-READY SPACES (8% OF 185 PROVIDED) = 15 SPACES REQUIRED/16 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV-CAPABLE SPACES (10% OF 185 PROVIDED) = 19 SPACES REQUIRED/20 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV CAPABLE LIGHT SPACE (10% OF 185 PROVIDED) = 19 SPACES/20 SPACES PROVIDED.



KEYED NOTES	
1.	EXISTING ASPHALT ROADWAY TO REMAIN.
2.	PROPOSED CONCRETE DRIVEWAY.
3.	PROPOSED STANDARD DRIVE ENTRANCE.
4.	PROPOSED CONCRETE TAXIWAY.
5.	PROPOSED CONCRETE PARKING AREA.
6.	PROPOSED CURB AND GUTTER.
7.	PROPOSED CONCRETE SIDEWALK.
8.	PROPOSED 2' CONCRETE DRAIN PAN.
9.	PROPOSED BICYCLE RACKS ON CONCRETE SLAB.
10.	PROPOSED TRASH ENCLOSURE WITH FLUSH CURB.
11.	PROPOSED 1' CURB CUT.
12.	ADA CURB RAMP.
13.	TWO (2) PROPOSED ADA PARKING SPACES WITH VAN ACCESSIBLE AND ADA SIGNAGE.
14.	CONCRETE ACCESS FOR OVERHEAD DOOR WITH FLUSH CURB.
15.	PARKING SPACE WITH THE REQUIRED NUMBER OF ELECTRIC VEHICLE SUPPLY EQUIPMENT (EVSE) INSTALLED.
16.	PARKING SPACE WITH THE REQUIRED NUMBER OF EV-READY AND EV-CAPABLE EQUIPMENT INSTALLED.



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For
Submittal

**Lot 1
Erie Air Park
Replat D**
3020-3100 Airport Drive
Erie, Colorado 80516

#	Date	Description
1	05/22/23	Initial Submittal
2	07/28/23	SIP Submittal
3	12/08/23	Town Comments
4	06/14/24	Town Comments

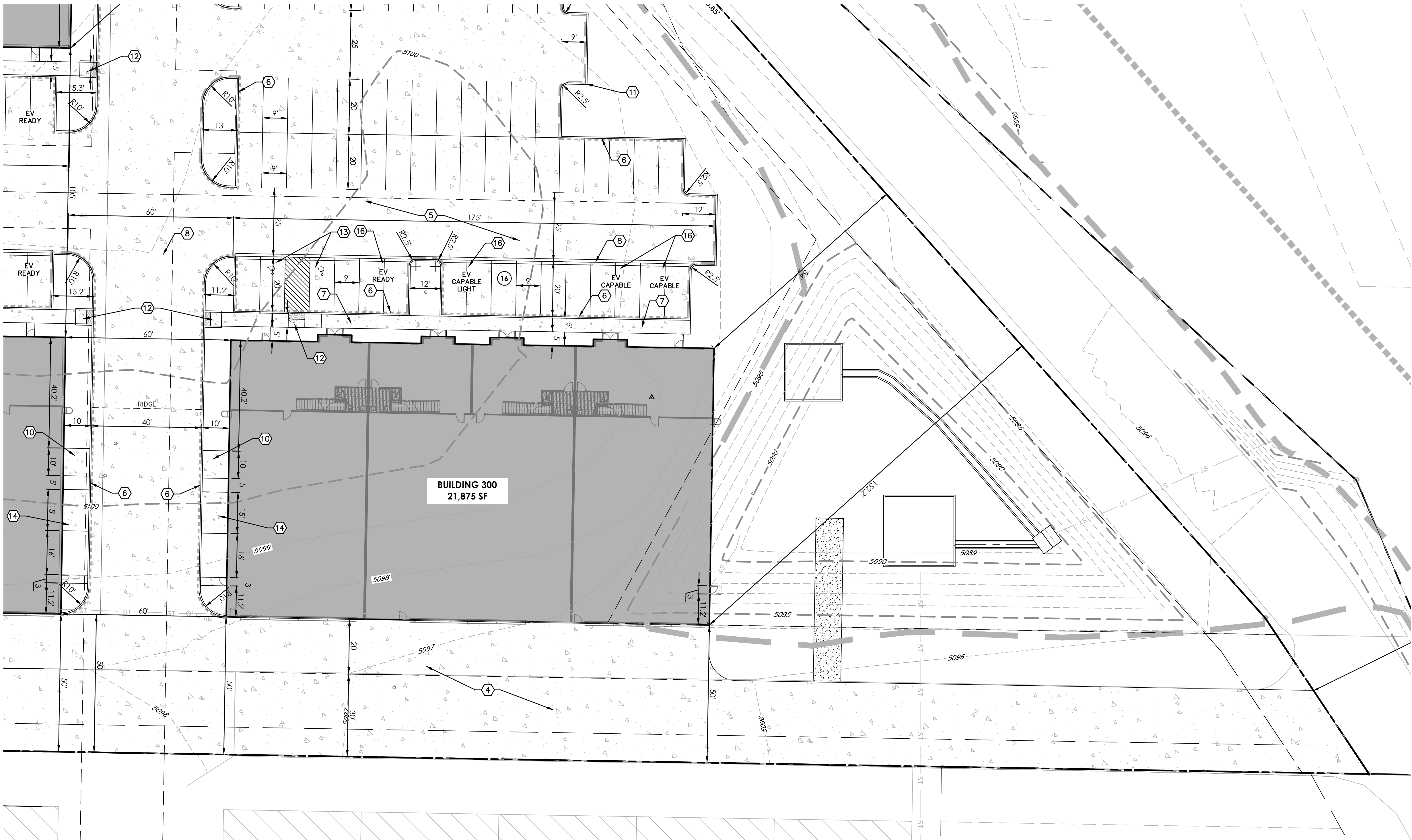
Project No: 23126A
By: JAS/DPA
File: 23126A-1

Detailed Site Plan

Sheet

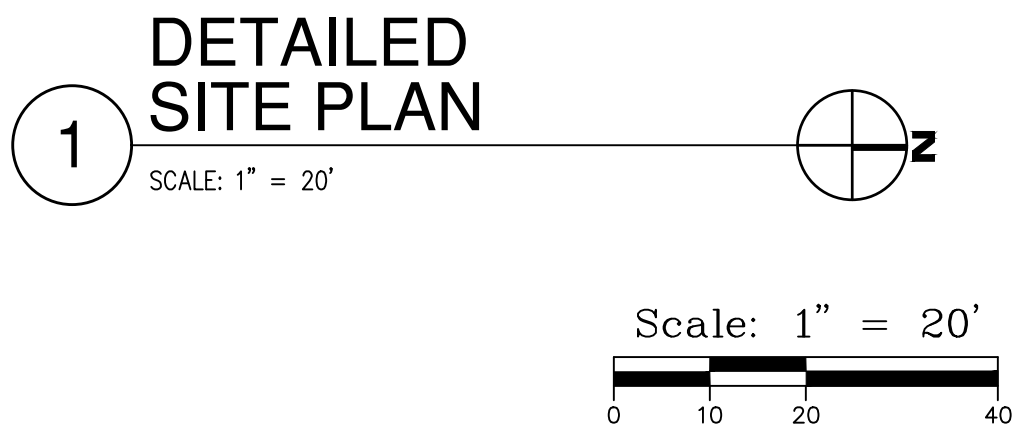
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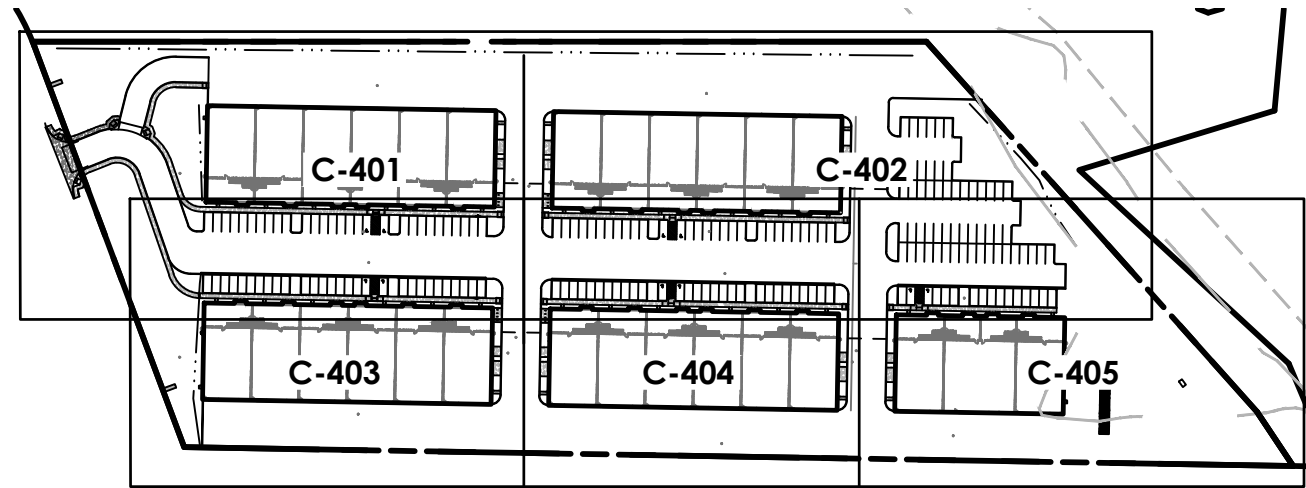


LEGEND	
	PROPOSED CATCH CURB
	PROPOSED SPILL CURB

- NOTES:
- EV PARKING SPACES WILL BE PROVIDED IN ACCORDANCE WITH IBC AND IECC 2021.
- MINIMUM NUMBER OF SPACES WITH EVSE INSTALLED (2% OF 185 PROVIDED) = 4 SPACES REQUIRED/4 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV-READY SPACES (8% OF 185 PROVIDED) = 15 SPACES REQUIRED/16 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV-CAPABLE SPACES (10% OF 185 PROVIDED) = 19 SPACES REQUIRED/20 SPACES PROVIDED.
 - MINIMUM NUMBER OF EV CAPABLE LIGHT SPACE (10% OF 185 PROVIDED) = 19 SPACES/20 SPACES PROVIDED.



KEYED NOTES	
1.	EXISTING ASPHALT ROADWAY TO REMAIN.
2.	PROPOSED CONCRETE DRIVEWAY.
3.	PROPOSED STANDARD DRIVE ENTRANCE.
4.	PROPOSED CONCRETE TAXIWAY.
5.	PROPOSED CONCRETE PARKING AREA.
6.	PROPOSED CURB AND GUTTER.
7.	PROPOSED CONCRETE SIDEWALK.
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9.	PROPOSED BICYCLE RACKS ON CONCRETE SLAB.
10.	PROPOSED TRASH ENCLOSURE WITH FLUSH CURB.
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16.	PARKING SPACE WITH THE REQUIRED NUMBER OF EV-READY AND EV-CAPABLE EQUIPMENT INSTALLED.



KEY MAP

SCALE: 1" = 200'

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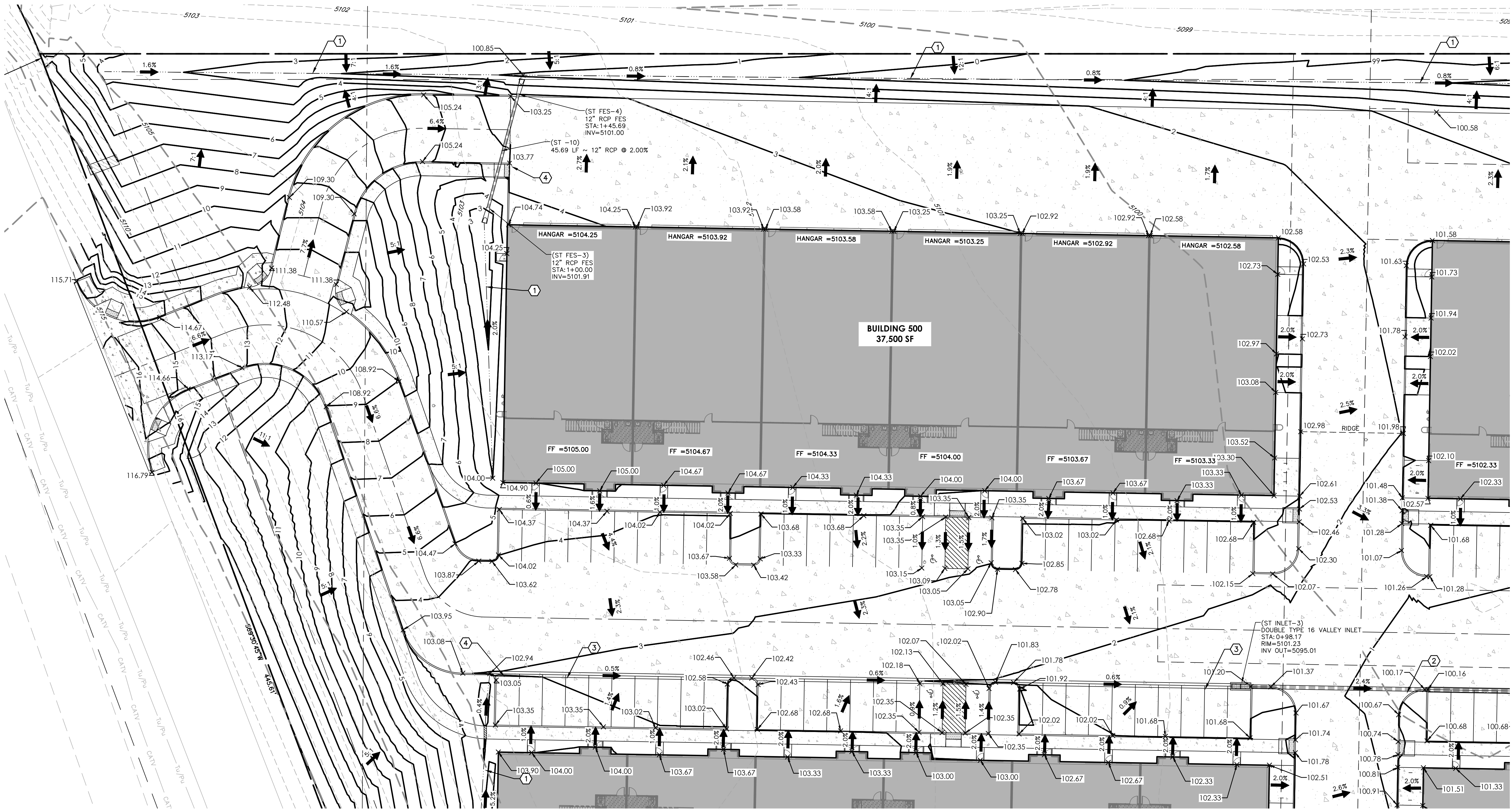
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File: 23126A-1

Detailed Site Plan

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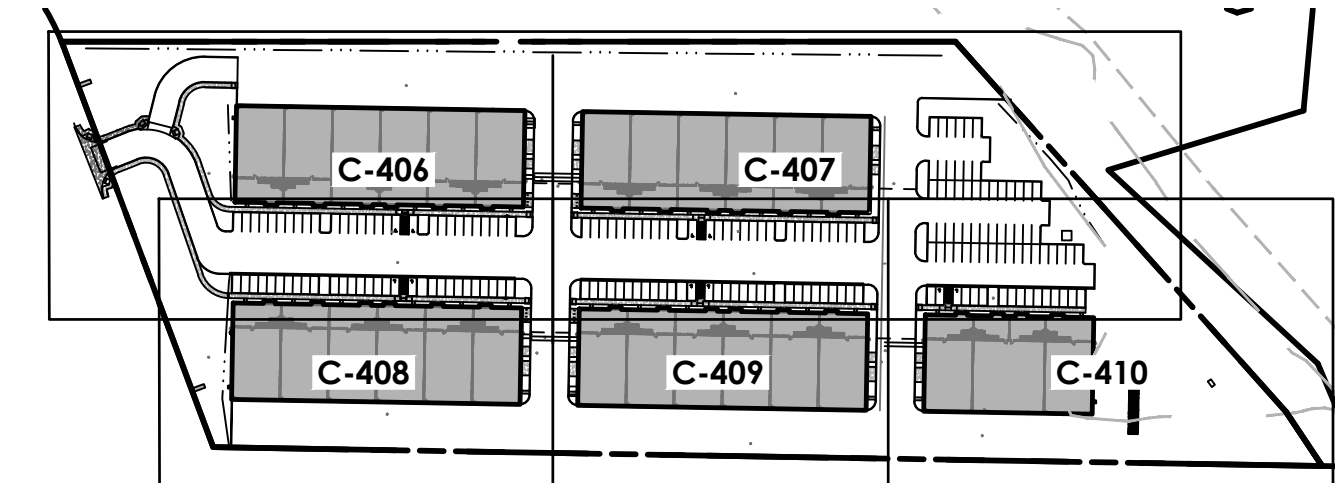
1 DETAILED GRADING PLAN

SCALE: 1" = 50'

Scale: 1" = 20'

KEYED NOTES

1. PROPOSED DRAINAGE SWALE.
2. PROPOSED STORM SEWER.
3. PROPOSED 2' CONCRETE DRAIN PAN.
4. PROPOSED 1' CURB CUT.



KEY MAP

SCALE: 1" = 200'

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Project No: 23126A
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File: 23126A-1

Detailed Grading Plan

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#	Date	Description
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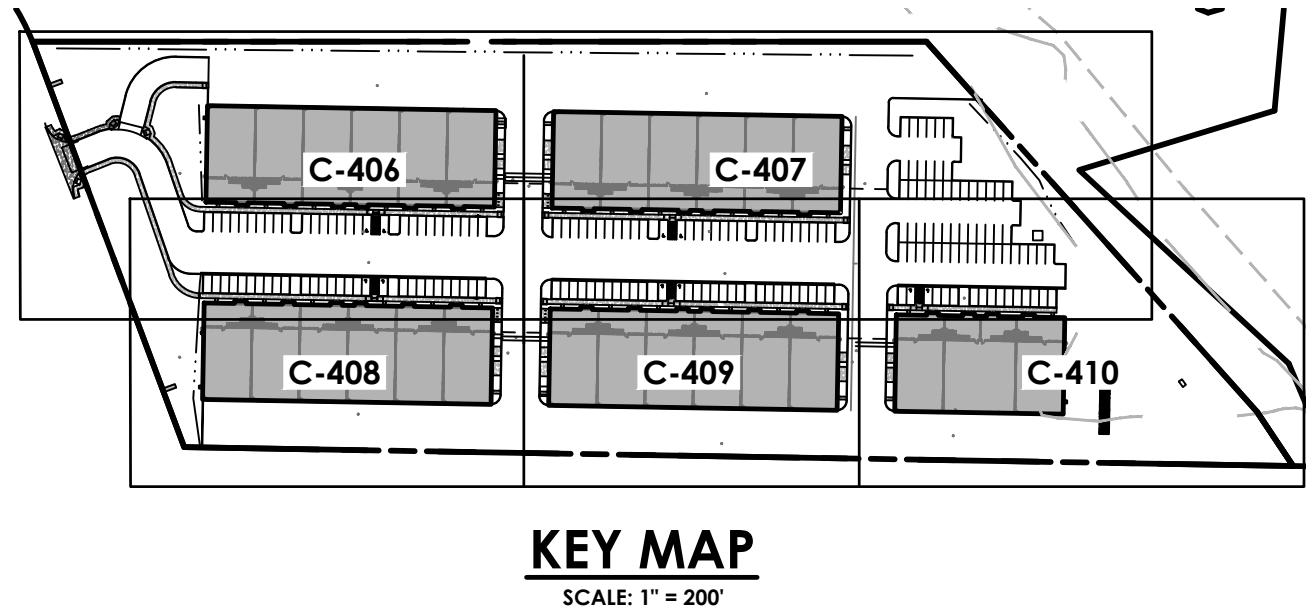
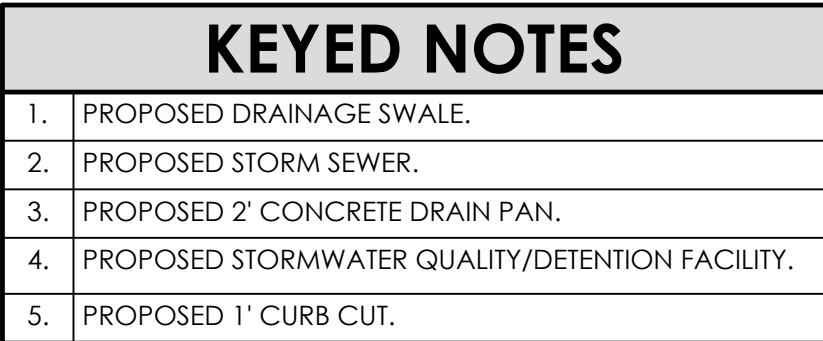
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File: 23126A-1

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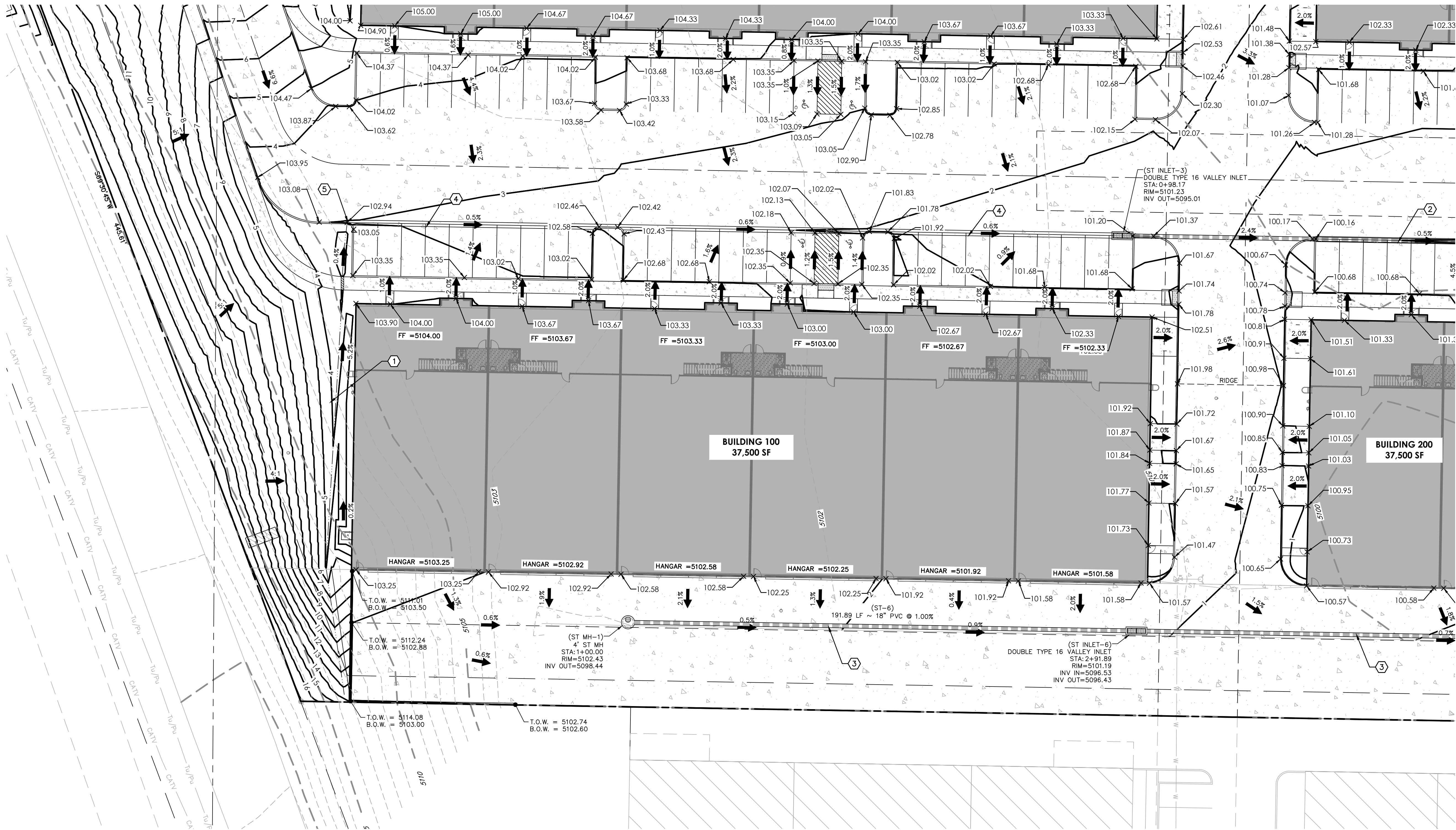
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**DETAILED
GRADING PLAN**

SCALE: 1" = 50'

Scale: 1" = 20'

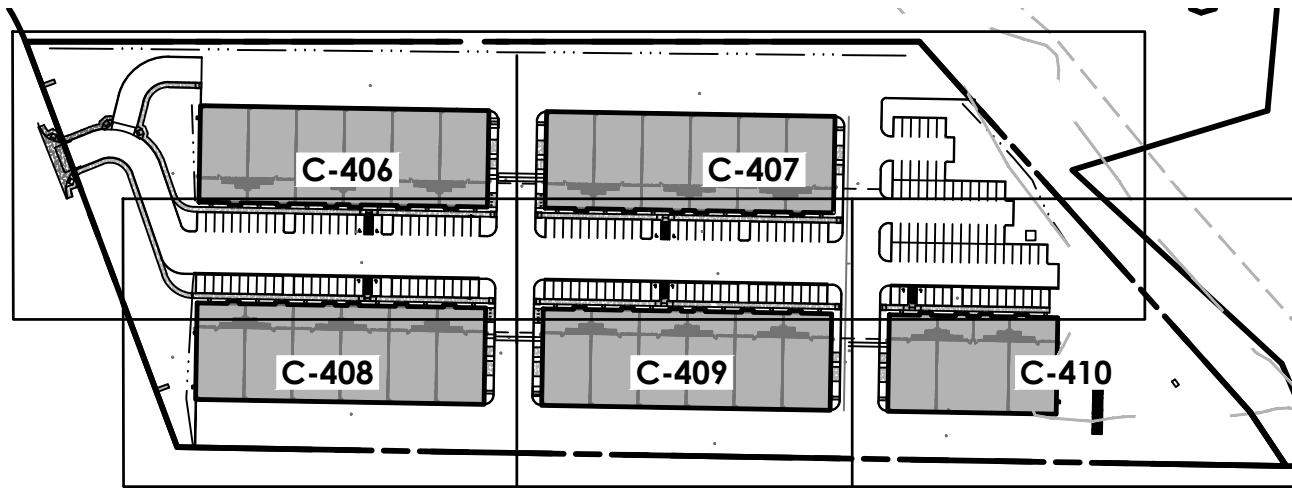
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1 DETAILED GRADING PLAN
SCALE: 1" = 50'

Scale: 1" = 20'
0 10 20 40

KEYED NOTES	
1.	PROPOSED DRAINAGE SWALE.
2.	PROPOSED STORM SEWER.
3.	PROPOSED STORM SEWER FOR ROOF DRAINS.
4.	PROPOSED 2' CONCRETE DRAIN PAN.
5.	PROPOSED 1' CURB CUT.



KEY MAP
SCALE: 1" = 200'

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By: JAS/DPA
File: 23126A-1

Detailed Grading Plan

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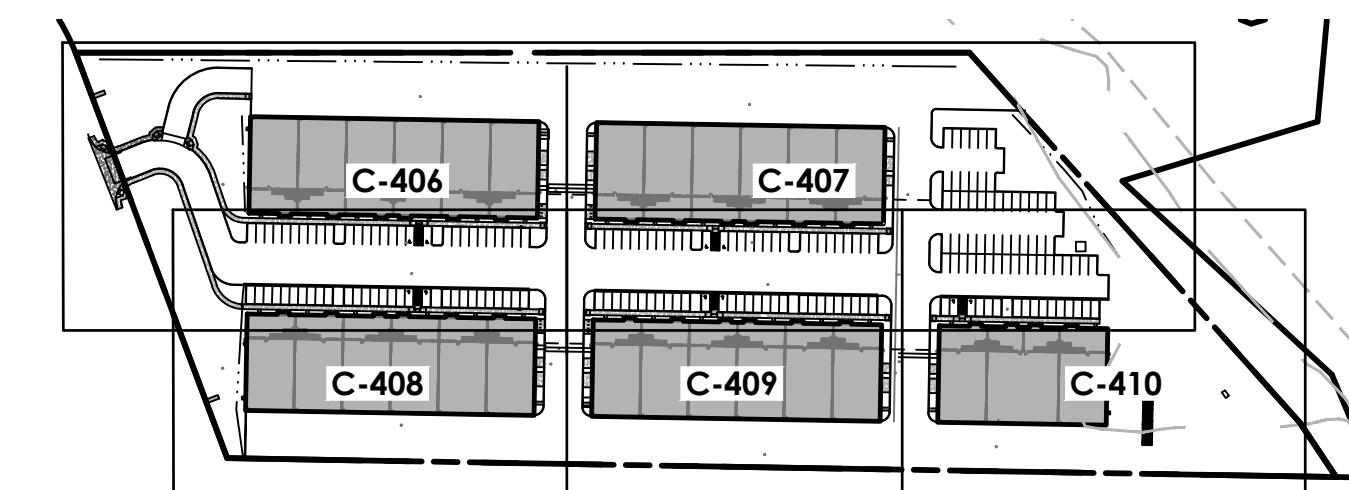
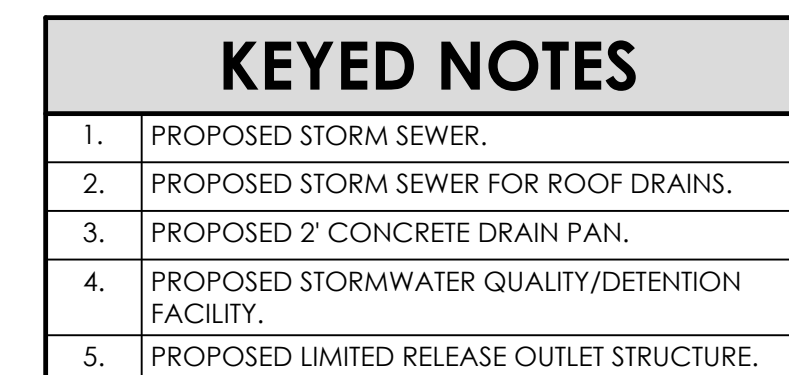
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Erie, Colorado 80516

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By: JAS/DPA
File: 23126A-1

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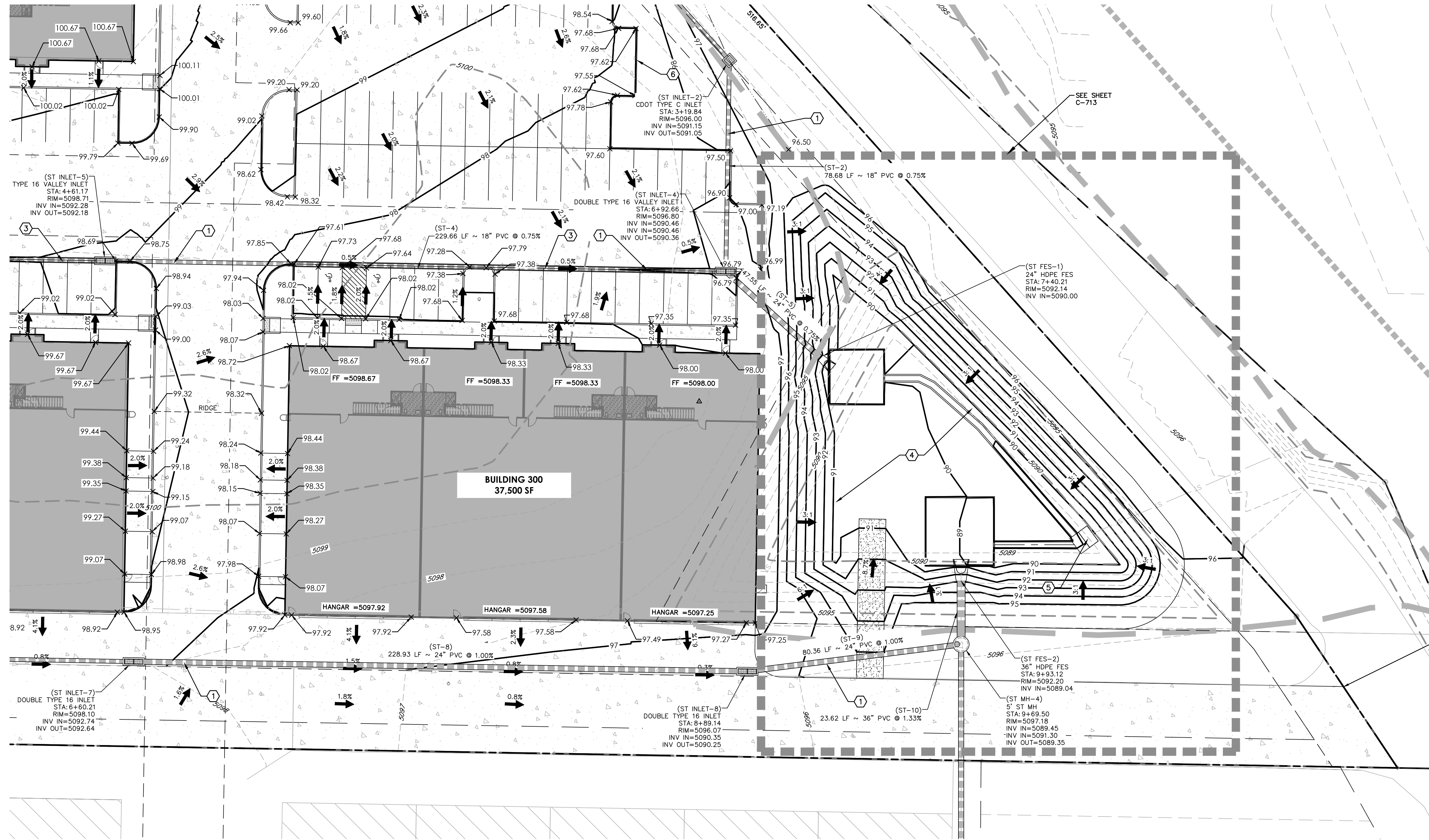
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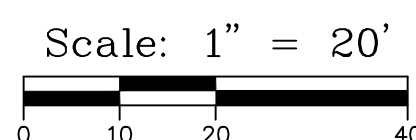
KEY MAP
SCALE: 1" = 200'

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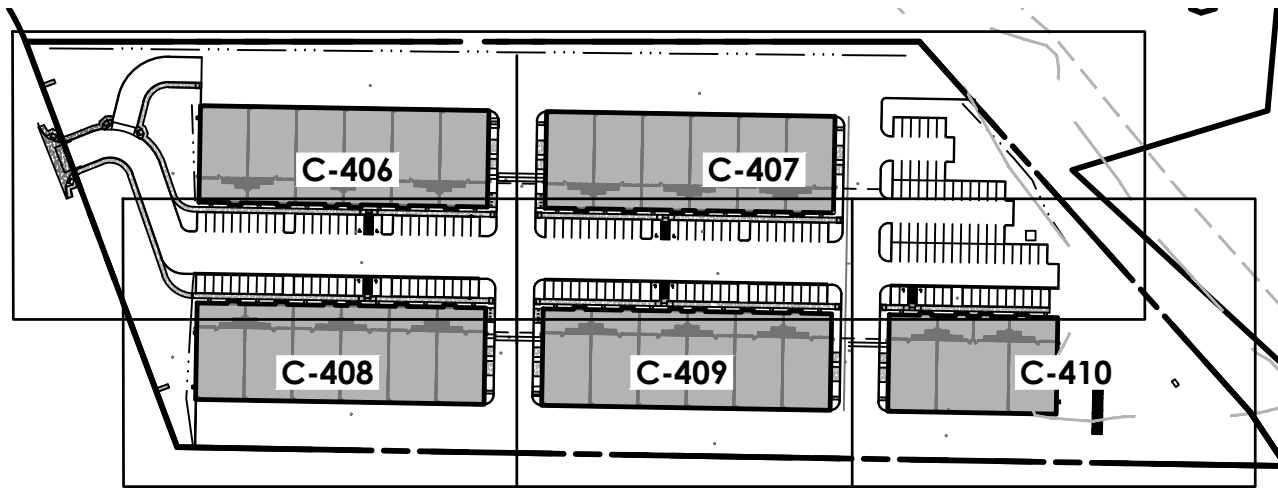
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1 GRADING AND DRAINAGE PLAN
SCALE: 1" = 50'



KEYED NOTES	
1.	PROPOSED STORM SEWER.
2.	PROPOSED STORM SEWER FOR ROOF DRAINS.
3.	PROPOSED 2' CONCRETE DRAIN PAN.
4.	PROPOSED STORMWATER QUALITY/DETENTION FACILITY.
5.	PROPOSED LIMITED RELEASE OUTLET STRUCTURE.
6.	PROPOSED 1' CURB CUT.



KEY MAP
SCALE: 1" = 200'

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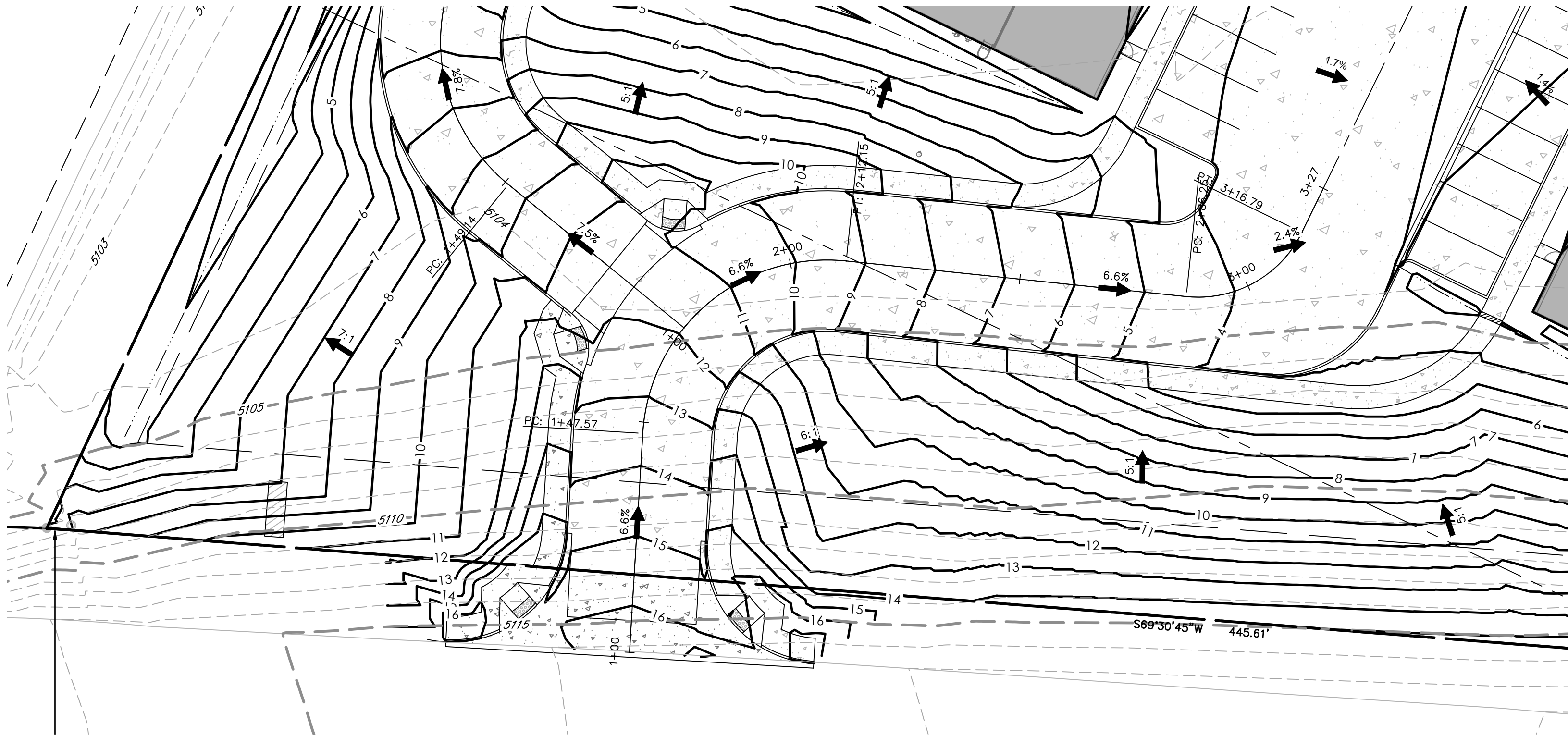
Project No: 23126A
By: JAS/DPA
File: 23126A-1

Detailed Grading Plan

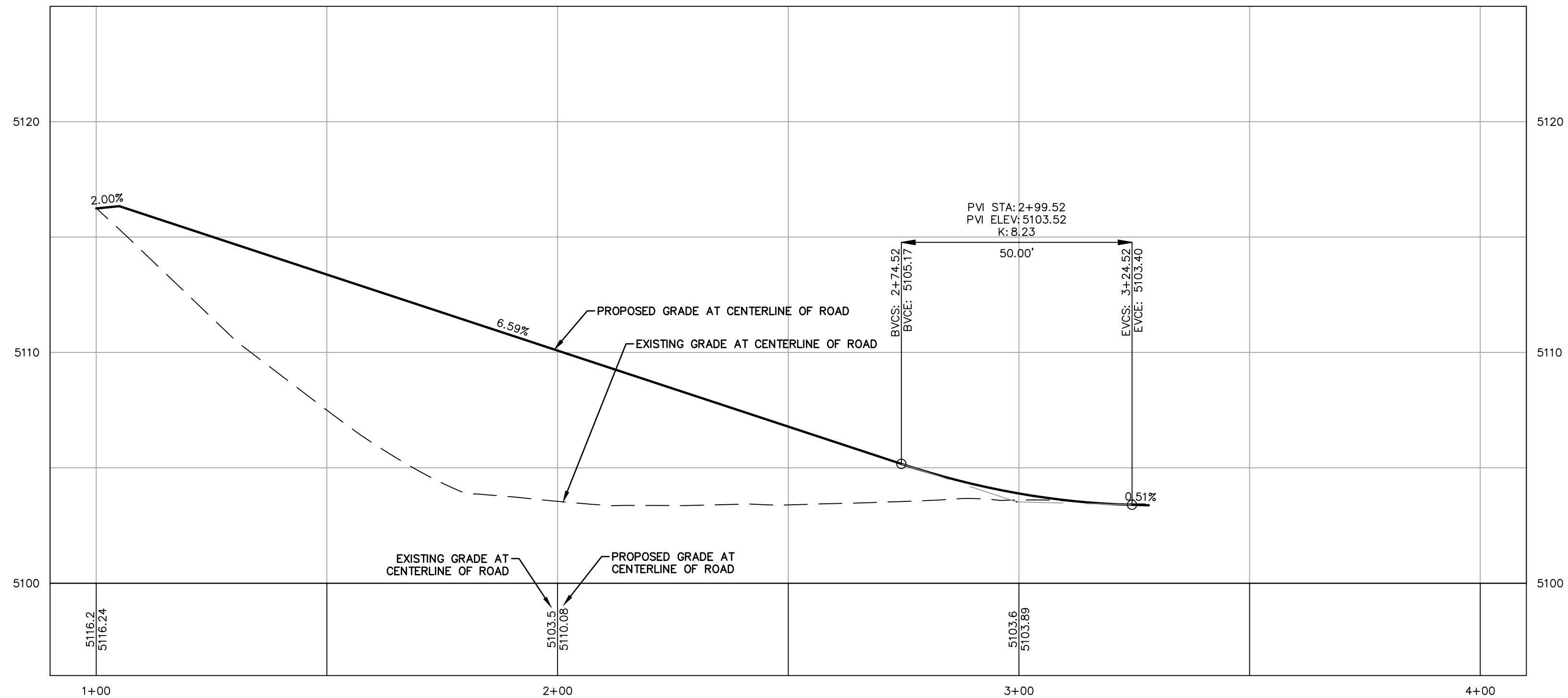
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1 ROADWAY PLAN
SCALE: 1" = 20'



1 ROADWAY PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

For
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Replat D
3020-3100 Airport Drive
Erie, Colorado 80516

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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Roadway Plan
and Profile

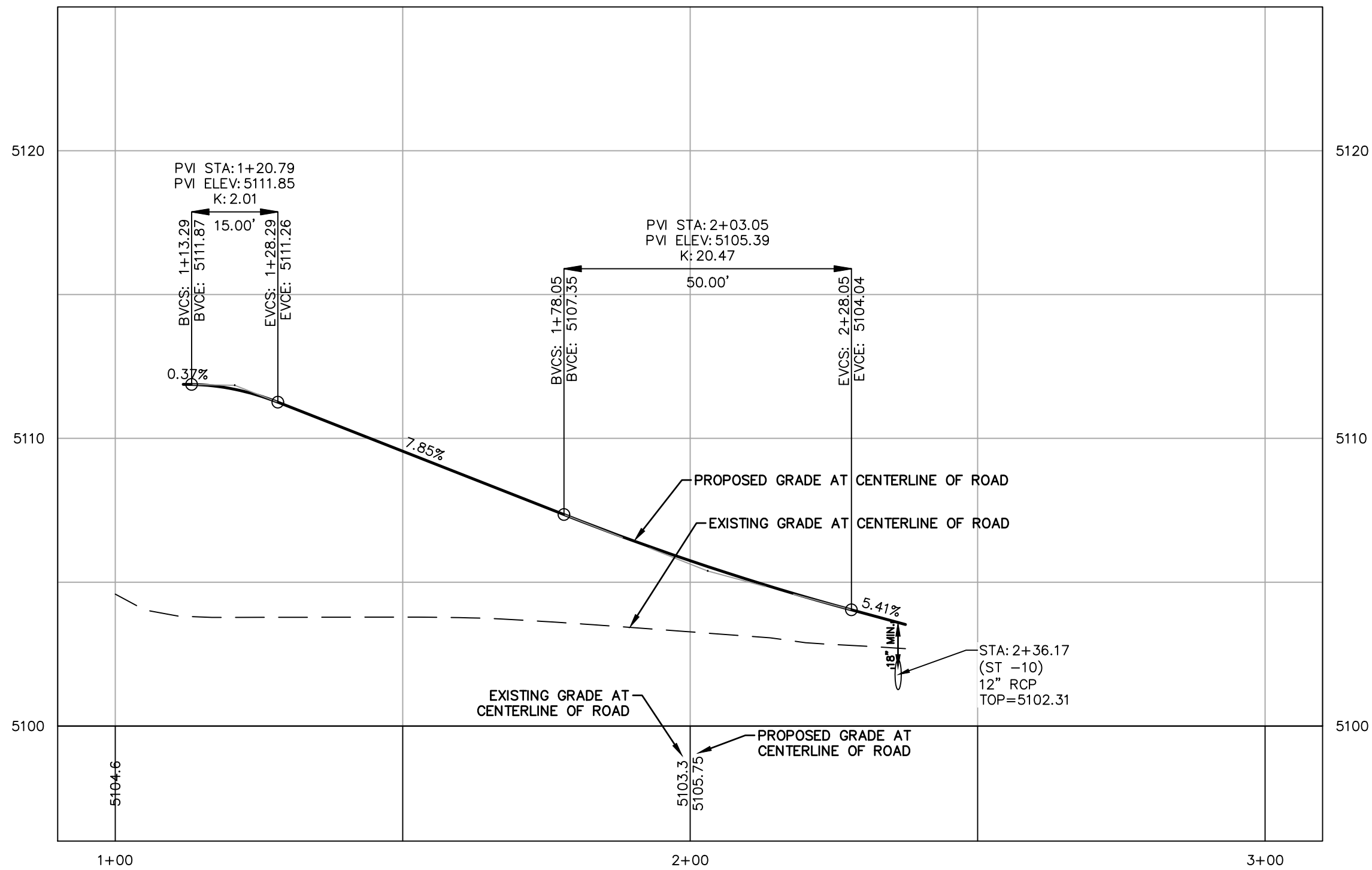
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1 ROADWAY PLAN
SCALE: 1" = 20'



1 ROADWAY PROFILE
SCALE: HORIZ: 1" = 20'
VERT: 1" = 5'

For
Submittal

Lot 1 Erie Air Park Replat D

3020-3100 Airport Drive
Erie, Colorado 80516

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Project No: 23126A
By: JAS/DPA
File: 23126A-1

Roadway Plan and Profile

Sheet

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Lot 1
Erie Air Park
Replat D
3020-3100 Airport Drive
Erie, Colorado 80516

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4	06/14/24	Town Comments

Project No: 23126A
By: JAS/DPA
File: 23126A-1

Photometric Plan

Sheet

C-601




1 PHOTOMETRIC PLAN
SCALE: 1" = 50'

NOTE: ALL EXTERIOR LIGHT FIXTURES
SHALL BE FULL CUT-OFF.



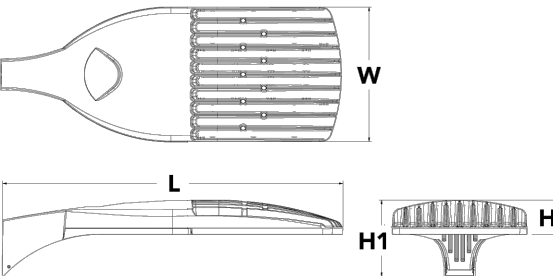
LUMINAIRE SCHEDULE								
CALLOUT	SYMBOL	MODEL	DESCRIPTION	LAMP	MOUNTING	MOUNTING HEIGHT	NOTES	QUANTITY
AL-1		LITHONIA LIGHTING, DSX1 LED P5 30K T3M MVOLT ***	DSX1 LED P5 30K T3M MVOLT	(1) LED	POLE	20'	LED DRIVER	4
AL-3		LITHONIA LIGHTING, DSX1 LED P5 30K T4M MVOLT ***	DSX1 LED P5 30K T4M MVOLT	(1) LED	POLE	20'	LED DRIVER	5
WL-1		LITHONIA LIGHTING, DSXW1 LED 20C 1000 30K T3M MVOLT ***	DSXW1 LED WITH (2) 10 LED LIGHT ENGINES, TYPE T3M OPTIC, 3000K, @ 1000mA.	(1) LED	WALL	20'	LED DRIVER	72

NOTE: ALL EXTERIOR LIGHT FIXTURES SHALL BE FULL CUT-OFF.



D-Series Size 1
LED Area Luminaire

Specifications
EPA: 1.01 ft² (0.09 m²)
Length: 33" (83.8 cm)
Width: 13" (33.0 cm)
Height H1: 7-1/2" (19.0 cm)
Height H2: 3-1/2" (9.0 cm)
Weight (max): 27 lbs (12.2 kg)



Ordering Information

EXAMPLE: DSX1 LED P7 40K T3M MVOLT SPA NLTAIR2 PIRHN DDBXD

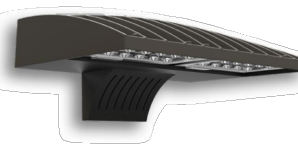
DSX1 LED	Series	LEDs	Color temperature	Distribution	Voltage	Mounting
DSX1LED	Forward optics P1 P4 P7 P2 P5 P8 P3 P6 P9 Rotated optics P10 ¹ P12 ¹ P11 ¹ P13 ¹	30K 3000 K 40K 4000 K 50K 5000 K	T15 Type I short T2M Type II medium T3S Type III short T3M Type III medium T4M Type IV medium T1TM Forward throw medium	T595 Type V very short T5S Type V short T5M Type V medium T5W Type V wide BLC Backlight control ² LCCO Left corner cutoff ² RCCO Right corner cutoff ²	MVOLT ¹ 120 ³ 208 ⁴ 240 ⁴ 277 ⁴ 347 ^{4,5} 480 ^{4,6}	Shipped included SPA Square pole mounting RPA Round pole mounting WBA Wall bracket SPUMBA Square pole universal mounting adaptor ⁴ RPU MBA Round pole universal mounting adaptor ⁴ Shipped separately KMAH DDBXD U Heat arm mounting bracket adaptor (specify finish) ⁷

Control options	Other options	Finish (specify)	
Shipped installed NLTAIR2 -1 Light AIR generation 2 enabled ⁸ PIRHN Network, high-flow motion/ambient sensor ⁹ PER NEMA twist-lock receptacle only (controls ordered separately) ¹⁰ PERS Five-pin receptacle only (controls ordered separately) ^{10,11} PERD Seven-pin receptacle only (controls ordered separately) ^{10,11} DMG 0-10v dimming wires pulled outside fixture (for use with an external control, ordered separately) ¹² DS Dual switching ^{10,13}	PIR High-flow, motion/ambient sensor, 8-15' mounting height, ambient sensor enabled at 5% ¹⁴ PIRH High-flow, motion/ambient sensor, 15-30' mounting height, ambient sensor enabled at 5% ¹⁴ PIR1FCV High-flow, motion/ambient sensor, 8-15' mounting height, ambient sensor enabled at 1% ¹⁴ PIR1HFCV High-flow, motion/ambient sensor, 15-30' mounting height, ambient sensor enabled at 1% ¹⁴ FAO Field adjustable output ¹⁴	Shipped installed HS House-side shield ¹⁷ SF Single fuse (120, 277, 347V) ¹⁸ DF Double fuse (208, 240 or 480V) ¹⁸ LSD Left rotated optics ¹ RSD Right rotated optics ¹ Shipped separately BS Blind spikes ⁹ EGS External glare shield ¹⁸	DOBXD Dark bronze DBXD Black DNAXD Natural aluminum DWBXD White DOBTD Textured dark bronze DBLXD Textured black DNATD Textured natural aluminum DWBGD Textured white

LITHONIA LIGHTING COMMERCIAL OUTDOOR

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DSX1-LED
Rev. 08/15/19
Page 1 of 8



D-Series Size 1
LED Wall Luminaire

Specifications
Width: 13-3/4" (34.9 cm)
Depth: 10" (25.4 cm)
Height: 6-3/8" (16.2 cm)
Weight: 12 lbs (5.4 kg)

Back Box (BBW, ELCW)
Width: 13-3/4" (34.9 cm)
Depth: 4" (10.2 cm)
Height: 6-3/8" (16.2 cm)
Weight: 5 lbs (2.3 kg)

Ordering Information

EXAMPLE: DSXW1 LED 20C 1000 40K T3M MVOLT DDBTXD

DSXW1 LED	Series	LEDs	Drive Current	Color temperature	Distribution	Voltage	Mounting	Control Options
DSXW1LED	10C 10 LEDs (one engine) 20C 20 LEDs (two engines) ¹	350 350 mA 530 530 mA 700 700 mA 1000 1000 mA (1 A) ²	30K 3000 K 40K 4000 K 50K 5000 K AMBC Amber phosphor converted	T2S Type II Short T2M Type II Medium T3S Type III Short T3M Type III Medium T4M Type IV Medium T1TM Forward Throw Medium	MVOLT ¹ 120 ³ 208 ⁴ 240 ⁴ 277 ⁴ 347 ^{4,5} 480 ^{4,6}	Shipped included (blank) Surface mounting bracket BBW Surface-mounted back box (for conduit entry) ¹ ELCW	Shipped installed PE Photoelectric cell, button type ⁸ DMG 0-10v dimming wires pulled outside fixture (for use with an external control, ordered separately) PIR 180° motion/ambient light sensor, <15' mfg ht. ¹⁴ PIRH 180° motion/ambient light sensor, 15-30' mfg ht. ¹⁴ PIR1FCV Motion/ambient sensor, 8-15' mounting height, ambient sensor enabled at 1% ¹⁴ PIR1HFCV Motion/ambient sensor, 15-30' mounting height, ambient sensor enabled at 1% ¹⁴ ELCW Emergency battery backup (includes external component enclosure, CSA File 20 Noncompliant ¹⁴)	

Other Options	Finish (specify)			
Shipped installed SF Single fuse (120, 277 or 347V) ^{10,11} DF Double fuse (208, 240 or 480V) ^{10,11} HS House-side shield ¹⁷ SPD Separate surge protection ¹²	Shipped separately¹¹ BSM Blind element spikes WG Wire guard VG Vandal guard DDL Diffused drop lens	DOBXD Dark bronze DBXD Black DNAXD Natural aluminum DWBXD White DOBTD Textured dark bronze DBLXD Textured black DNATD Textured natural aluminum	DSBXD Sandstone DOBTD Textured dark bronze DBLXD Textured black DNATD Textured natural aluminum	DWBXD Textured white DSBXD Textured sandstone

Accessories
Ordered and shipped separately

DSXW1S House-side shield (see pr lighting)

DSXW1B U Blind element spikes

DSXW1W U Wire guard assembly

DSXW1C U Vandal guard assembly

NOTES
1. 20C, 1000 is not available with PIR, PIRH, PIR1FCV or PIR1HFCV.
2. MVOLT driver operates on any line voltage from 120-277V (50/60 Hz).
3. Single fuse (SF) requires 120, 277 or 347 voltage option. Double fuse (DF) requires 208, 240 or 480 voltage option.
4. Only available with 20C, 700mA or 1000mA. Not available with PIR or PIRH.
5. Back box ship installed on fixture. Cannot be field installed. Cannot be ordered as an accessory.
6. Photocontrol (PE) requires 120, 208, 240, 277 or 347 voltage option. Not available with motion/ambient light sensors (PIR or PIRH).
7. Reference Motion Sensor table on page 3.
8. Cold weather (CWC) rated. Not compatible with conduit entry applications. Not available with BBW mounting option. Not available with fusing. Not available with 347 or 480 voltage options. Emergency components located in back box housing. Emergency mode (ES) files located on product page at www.litco.com.
9. Not available with SPD.
10. Not available with ELCW.
11. Also available as a separate accessory; see Accessories information.
12. Not available with ELCW.

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DSXW1-LED
Rev. 8/15/19

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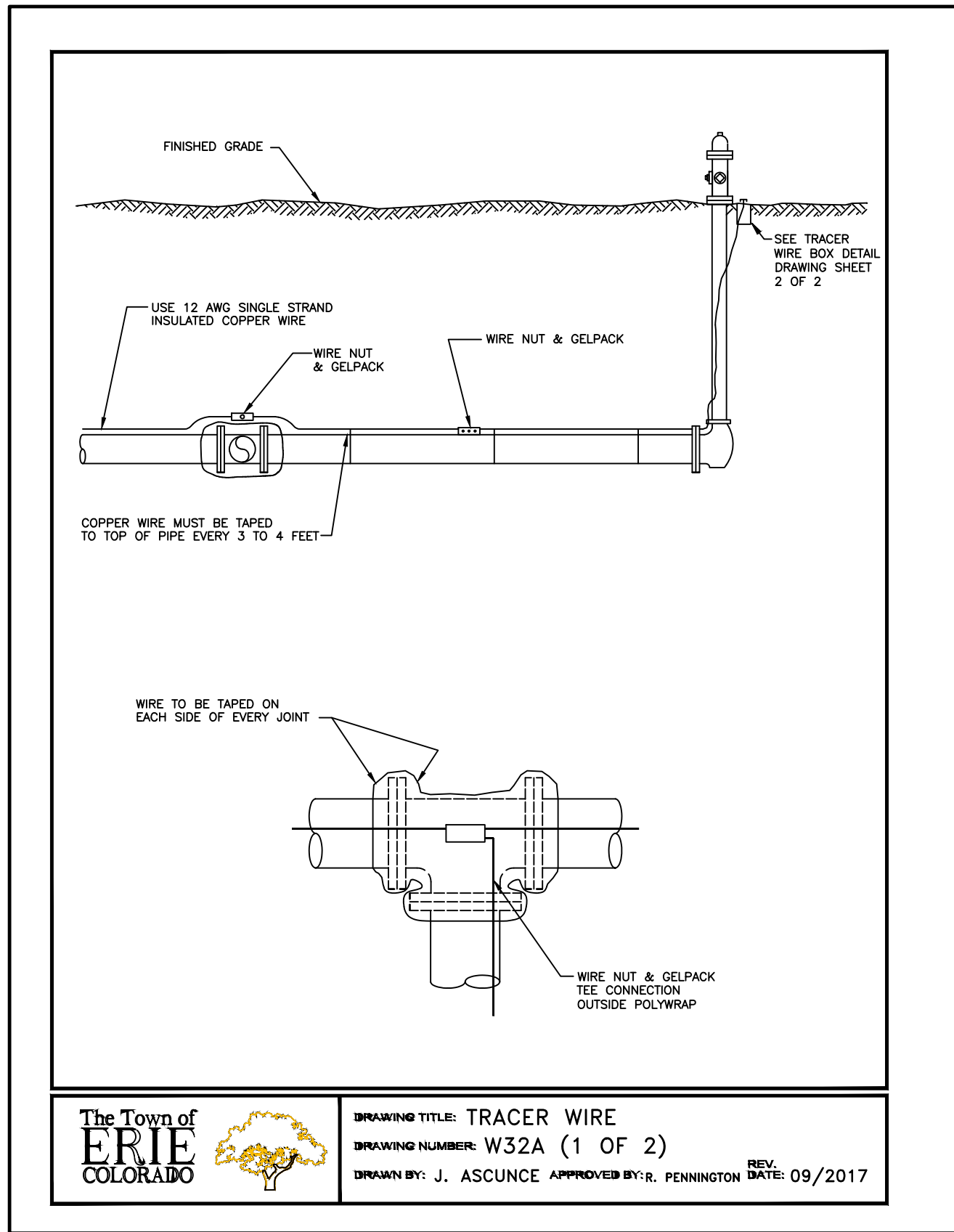
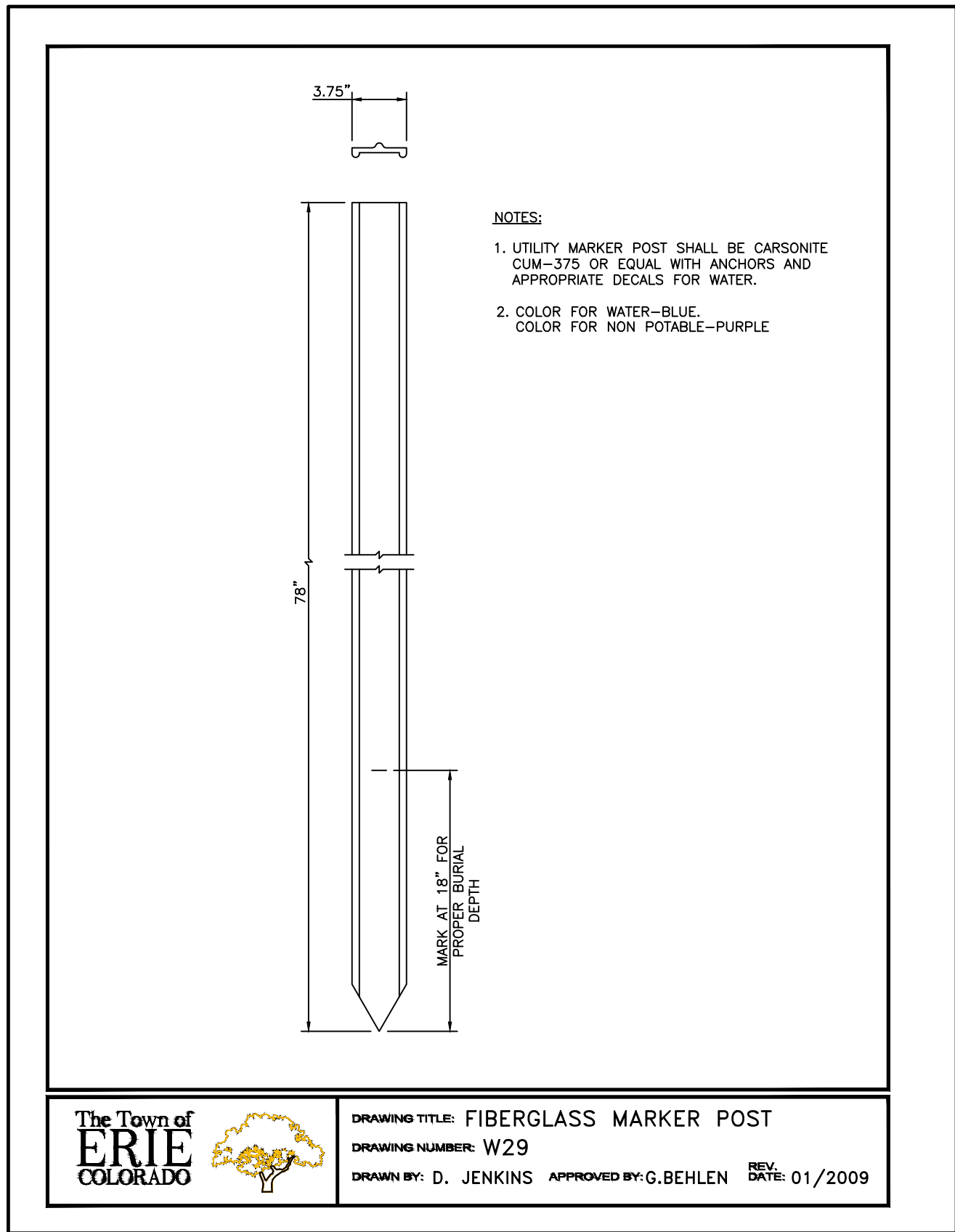
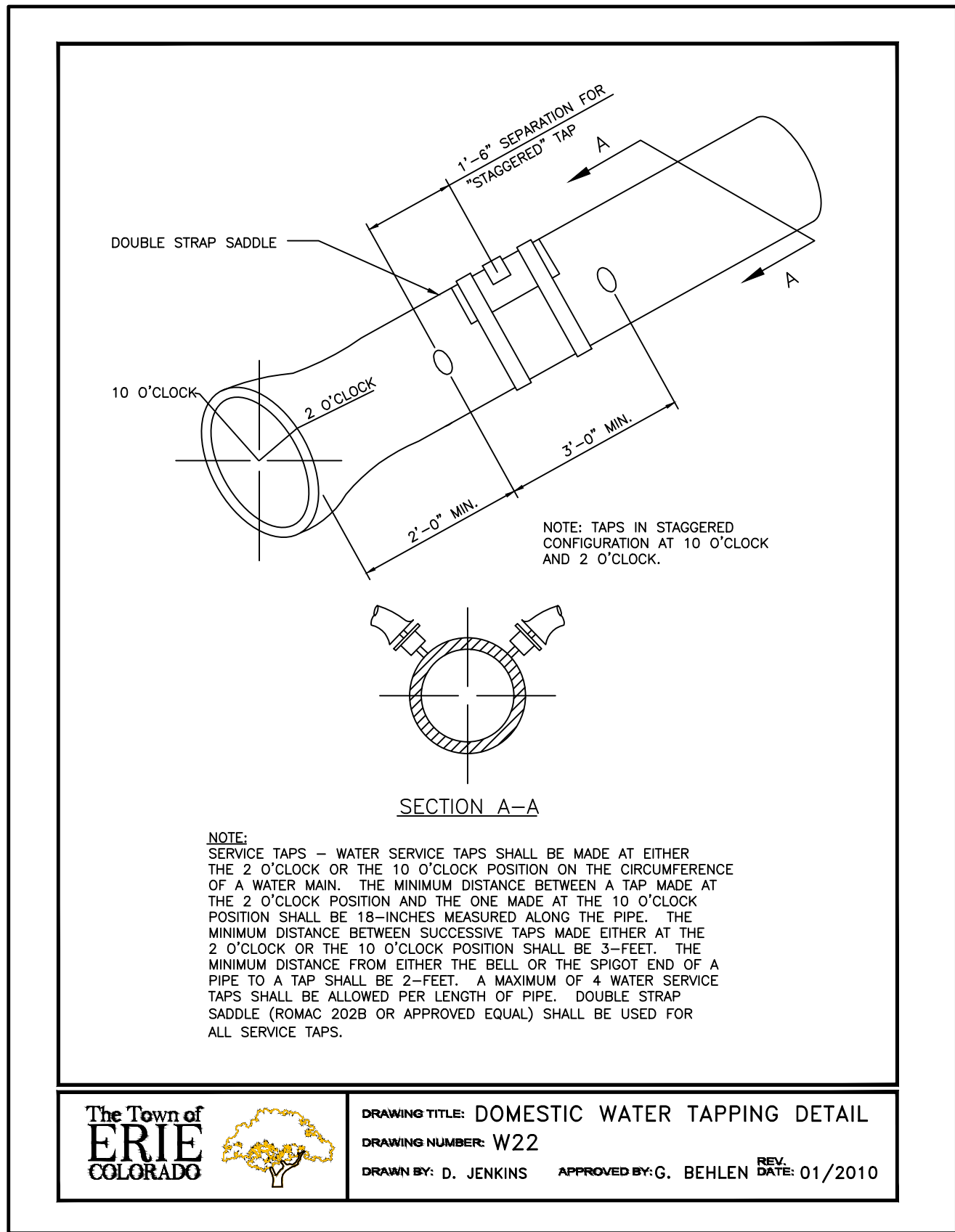
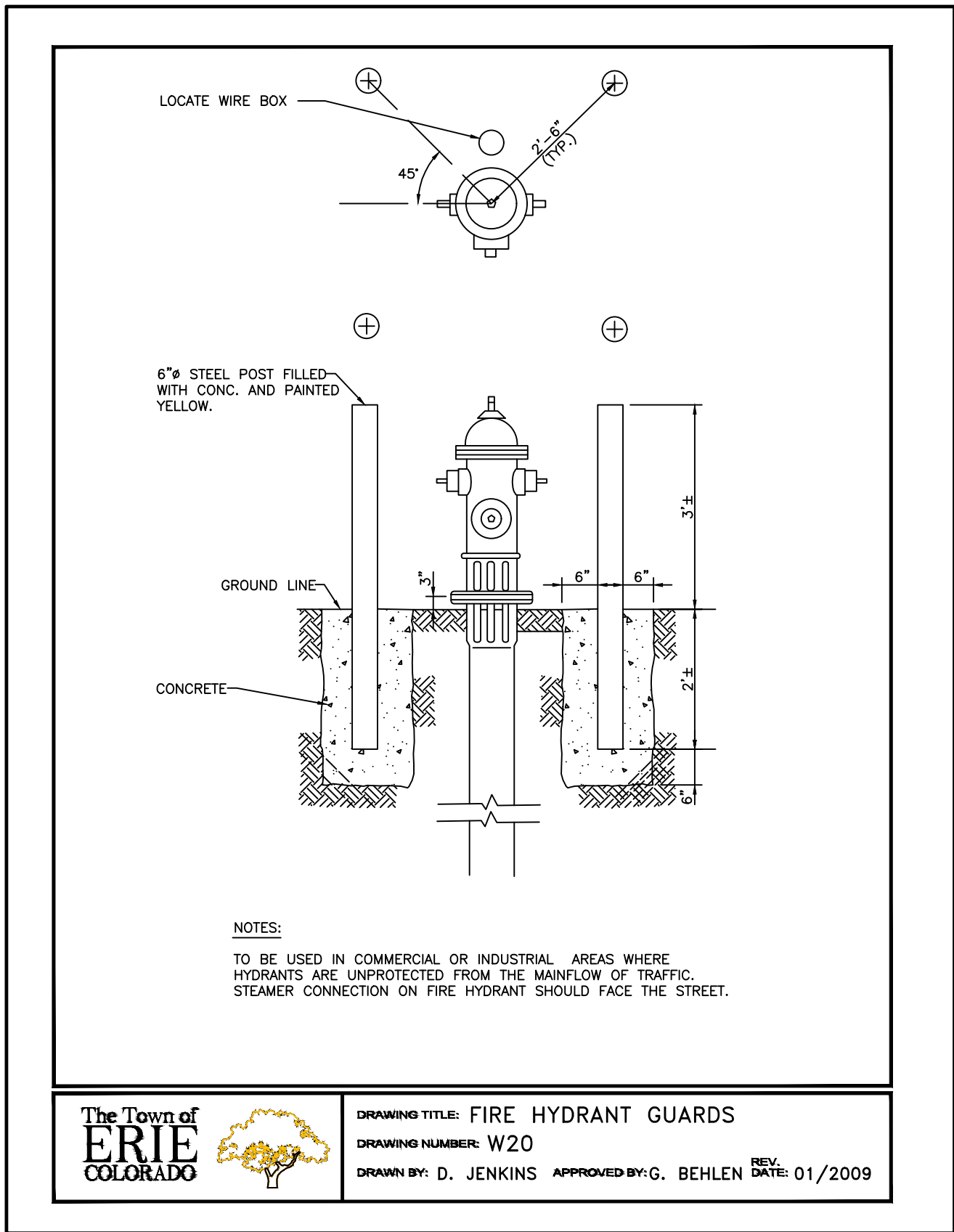
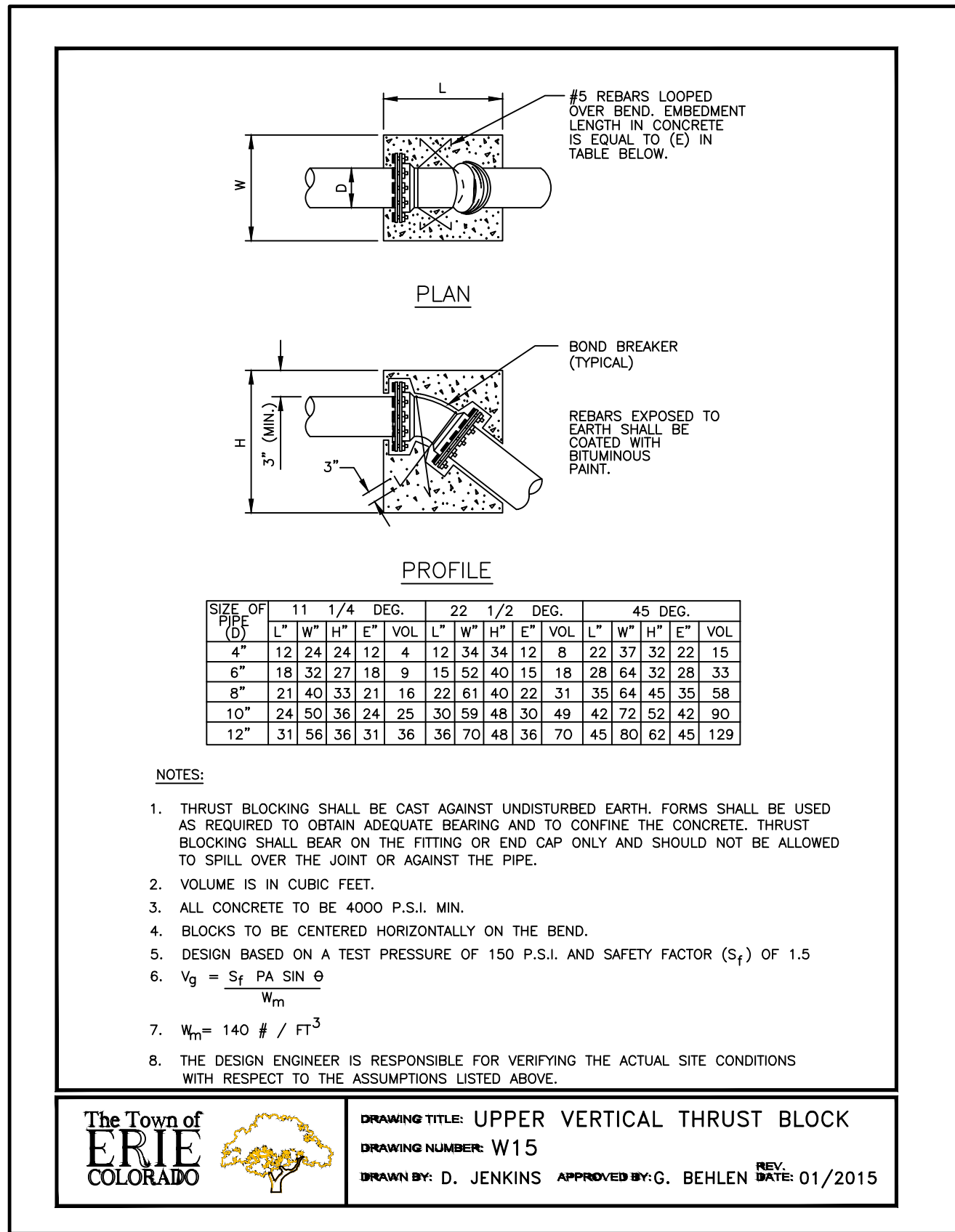
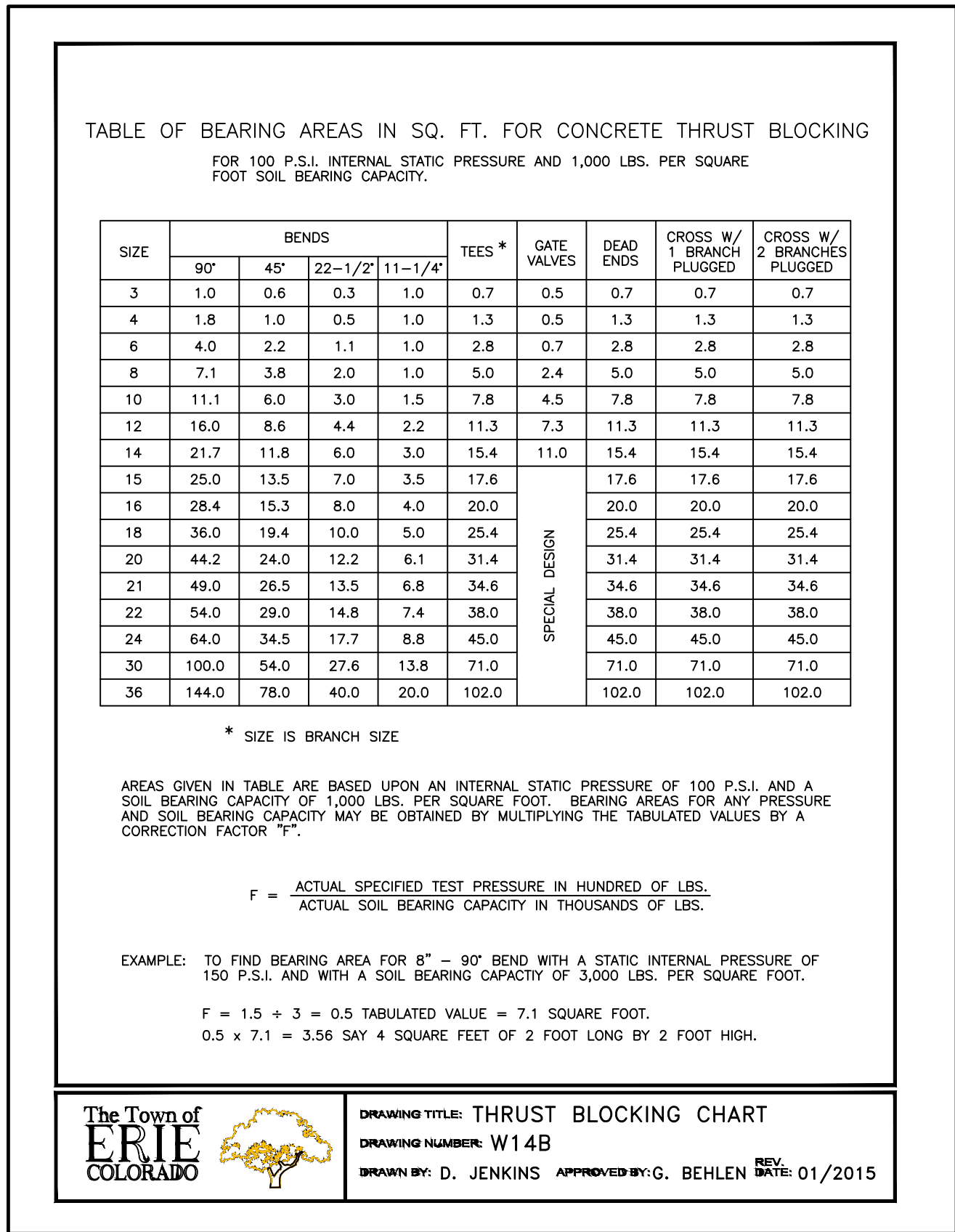
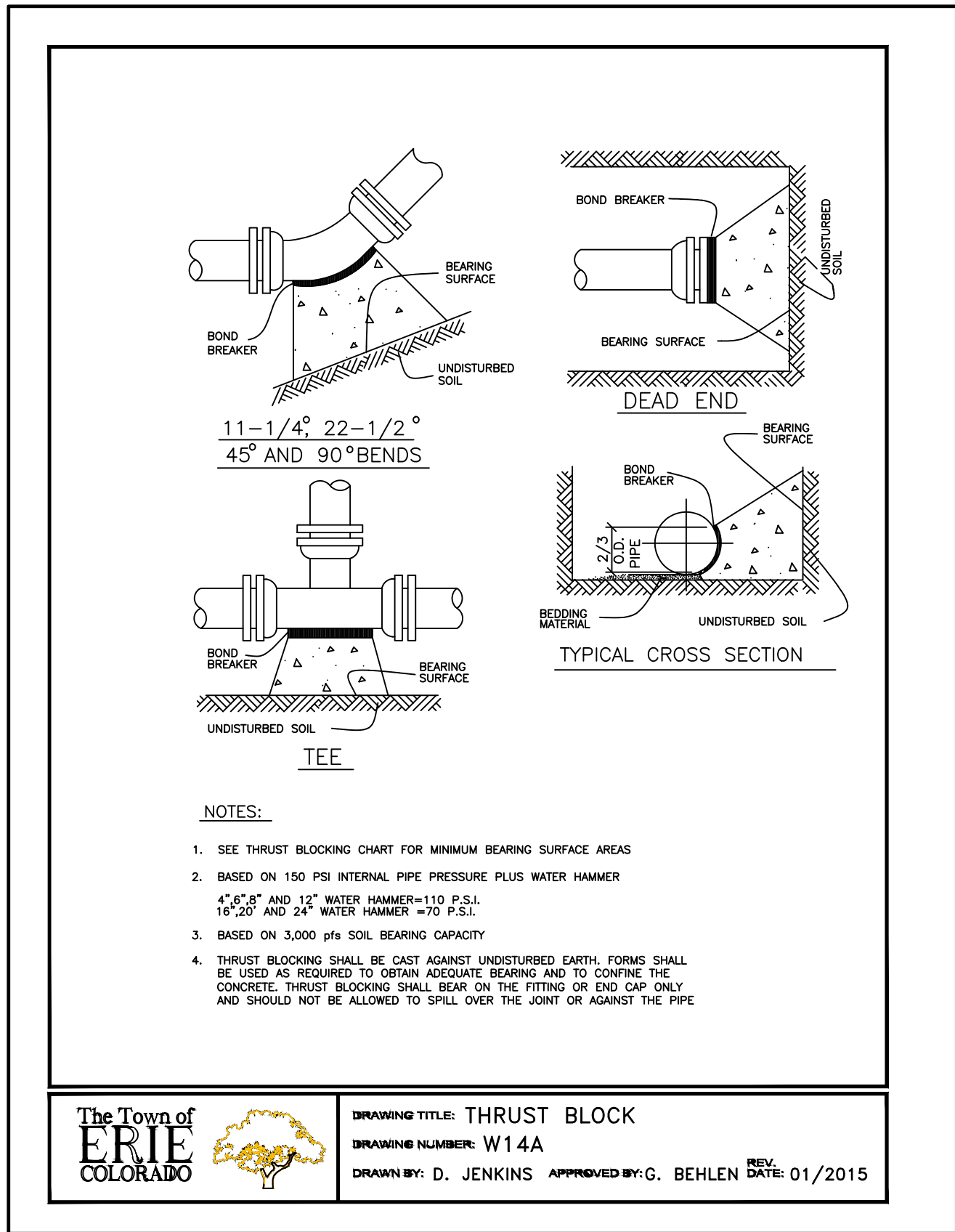
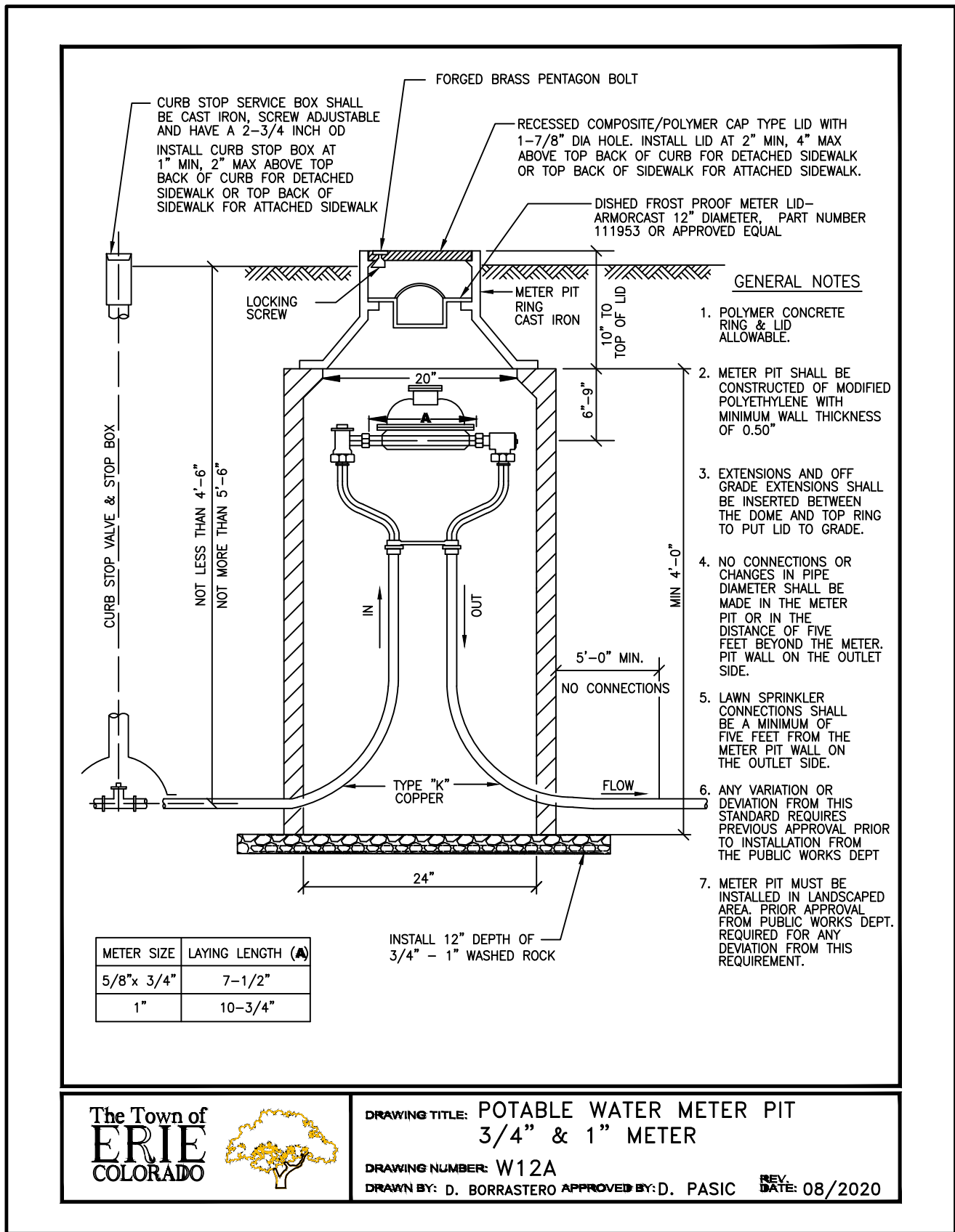
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Lighting Details

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3020-3100 Airport Drive
Erie, Colorado 80516

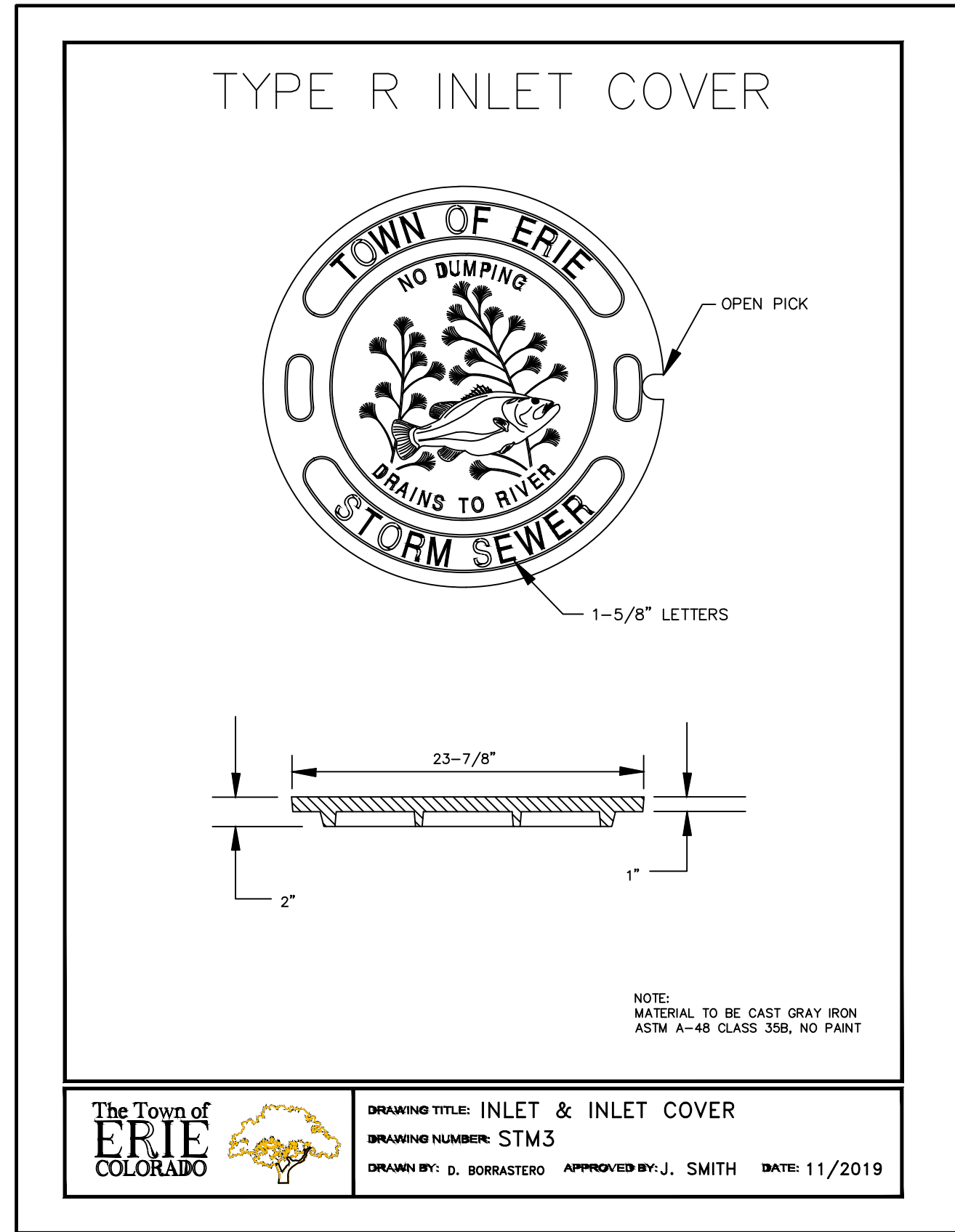
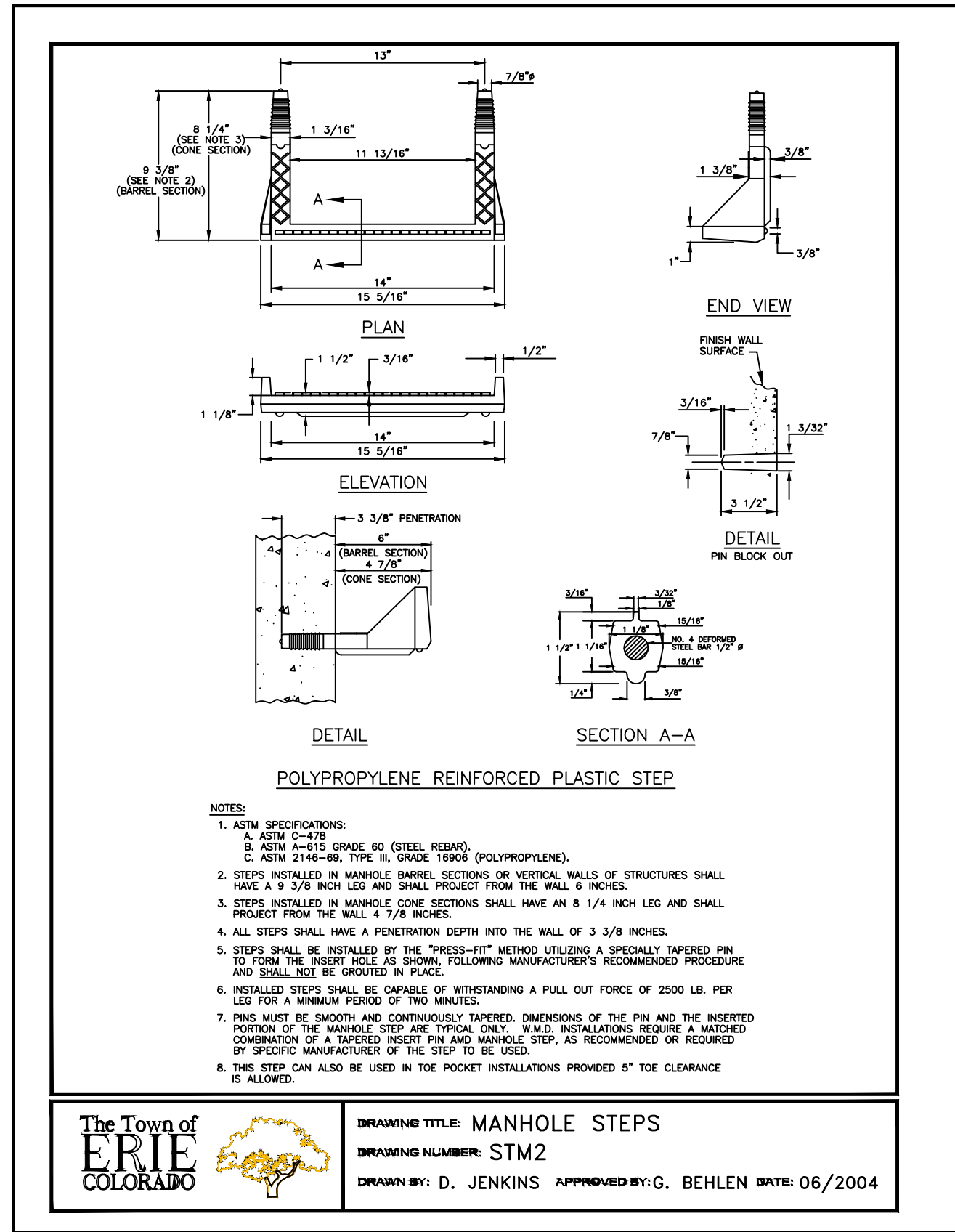
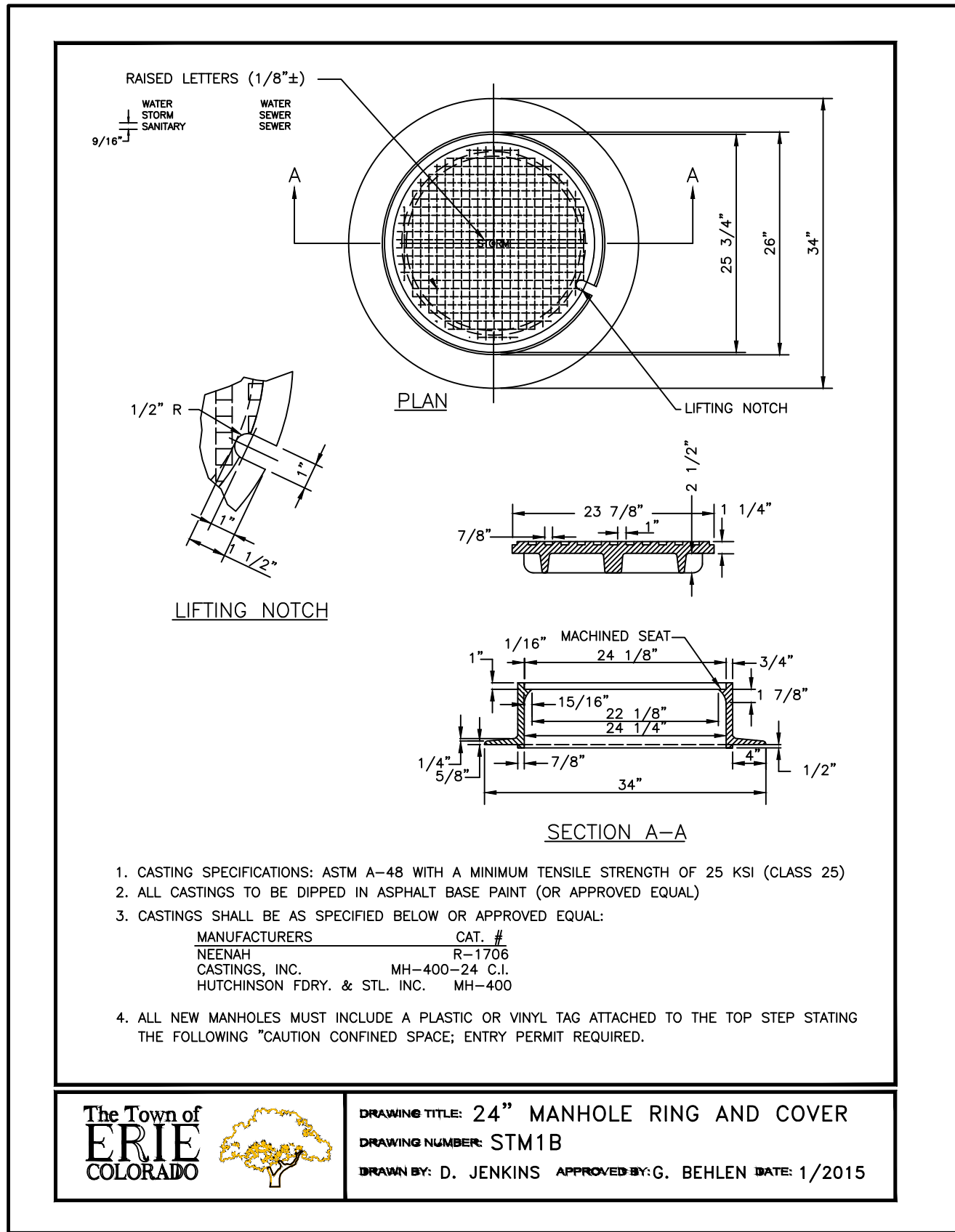
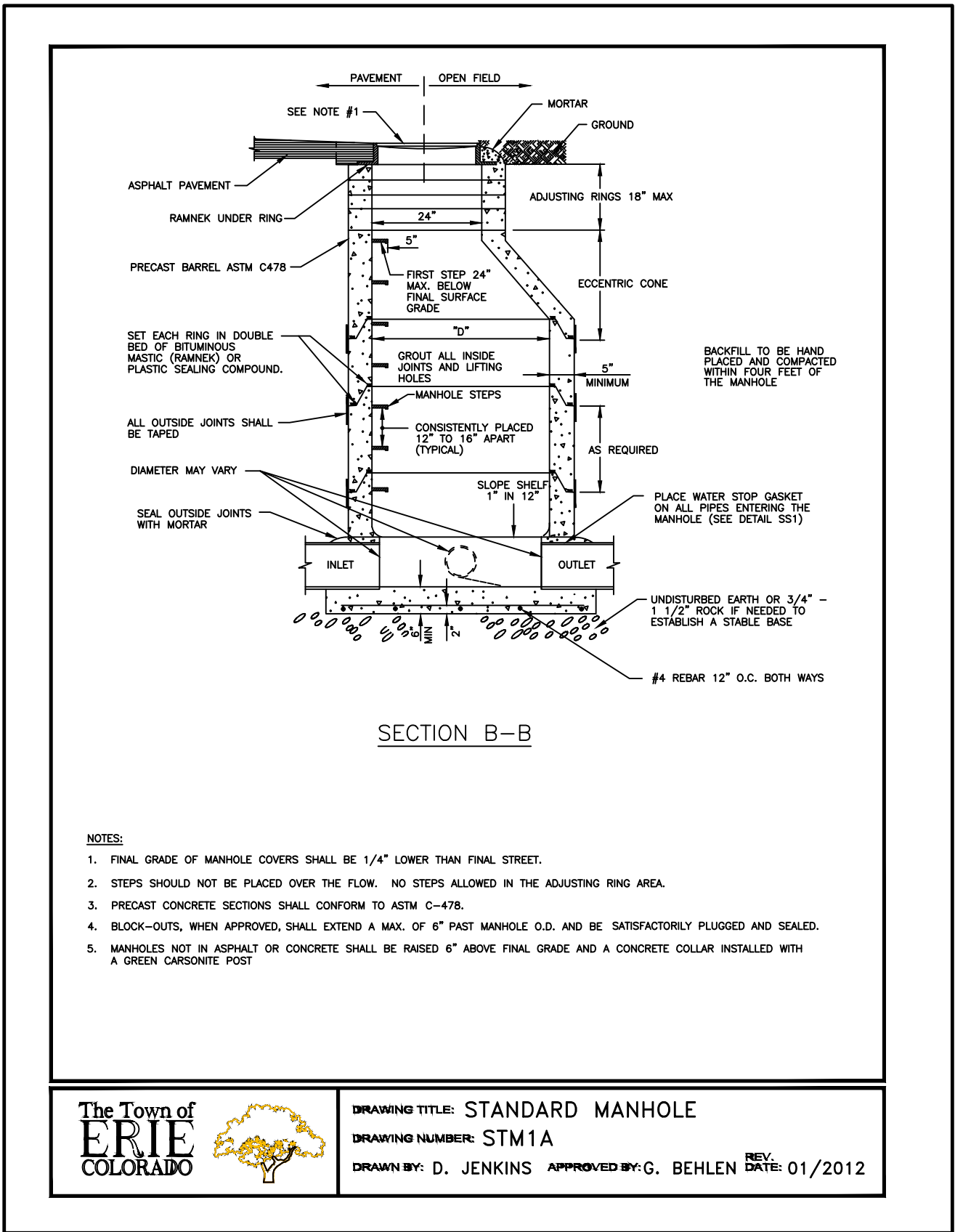
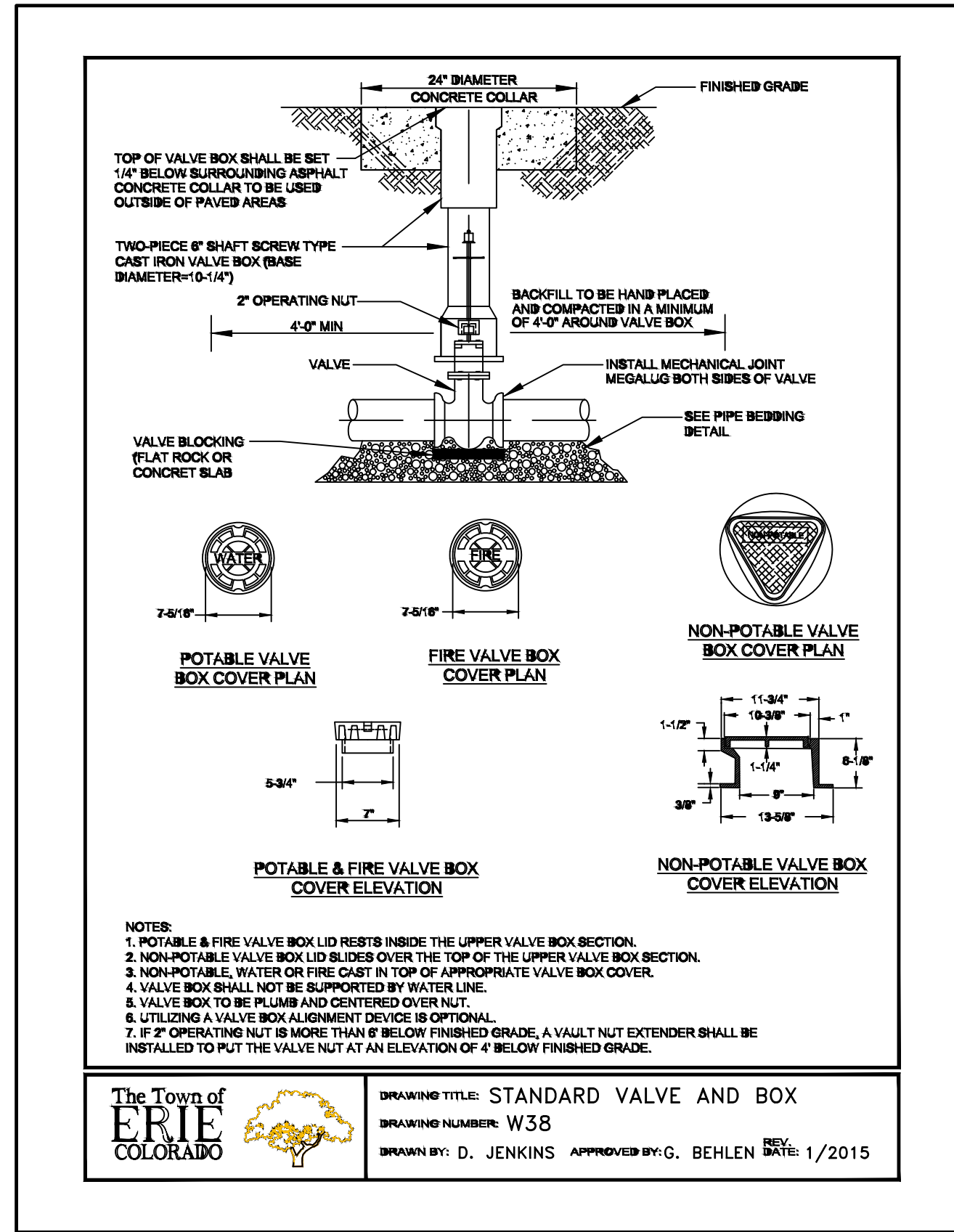
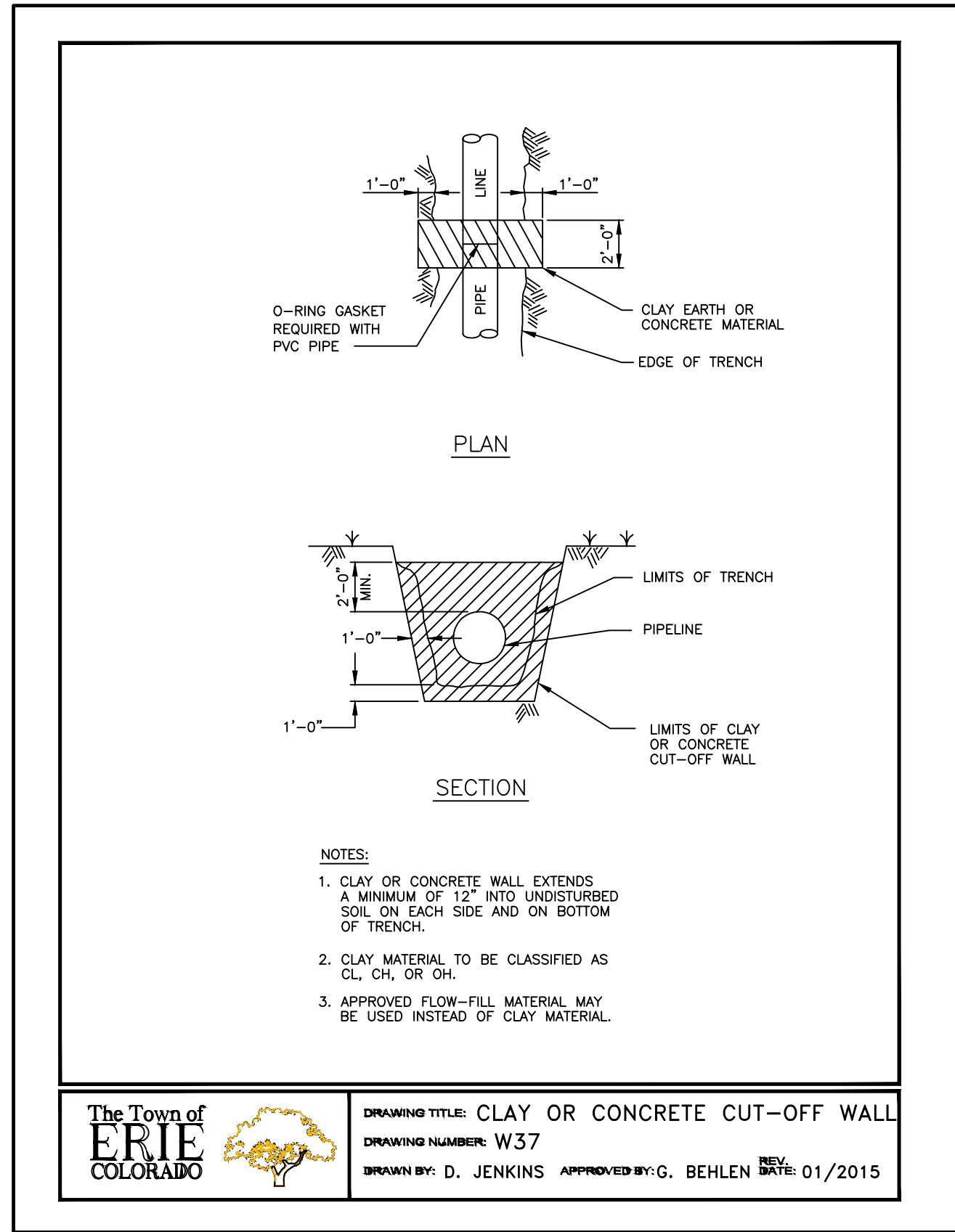
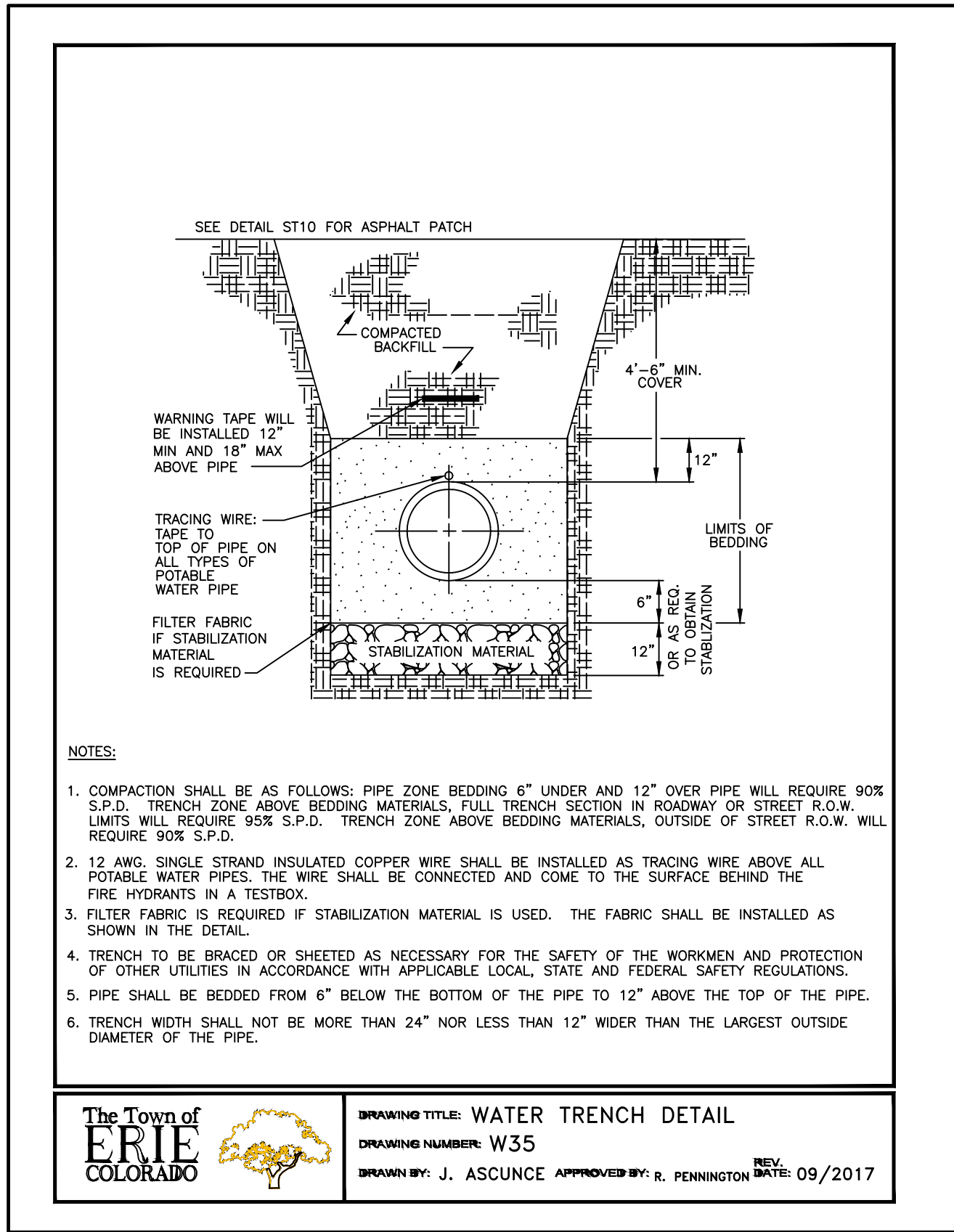
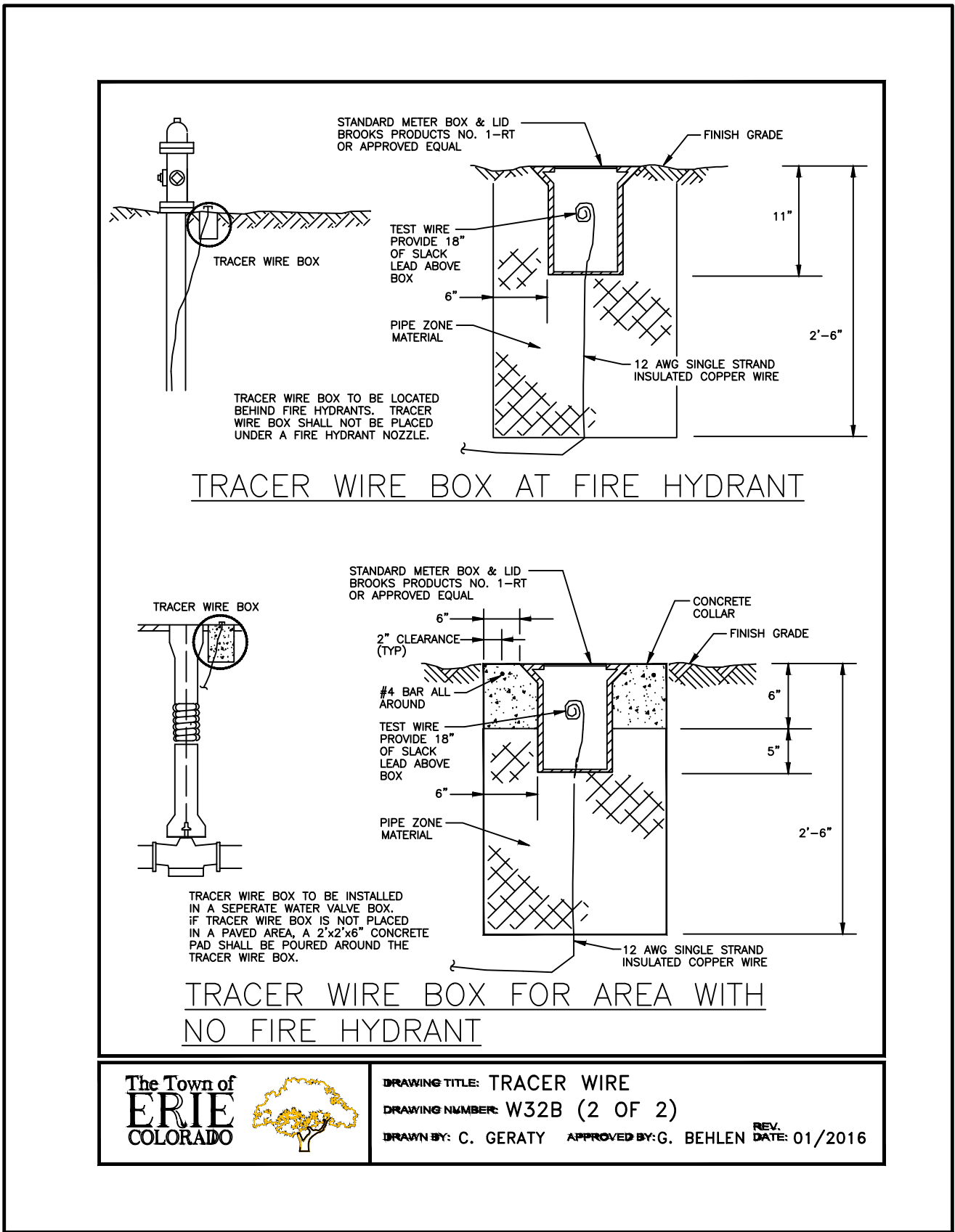
#	Date	Description
1	05/22/23	Initial Submittal
2	07/28/23	SIP Submittal
3	12/08/23	Town Comments
4	06/14/24	Town Comments

Project No: 23126A
By: JAS/DPA
File: 23126A-1

Civil Details

Sheet

C-702



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creativity for
the built environment

For
Submittal

Lot 1
Erie Air Park
Replat D
3020-3100 Airport Drive
Erie, Colorado 80516

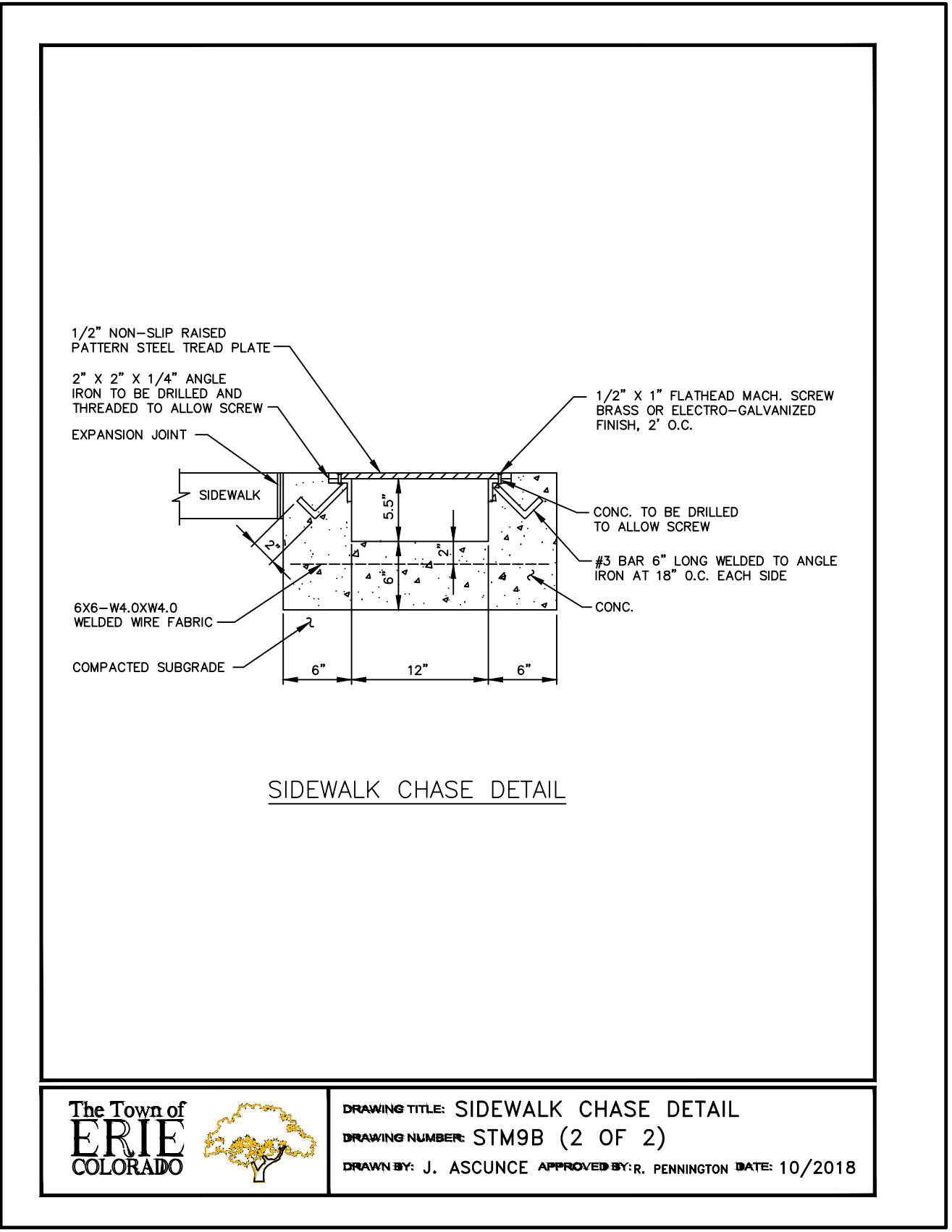
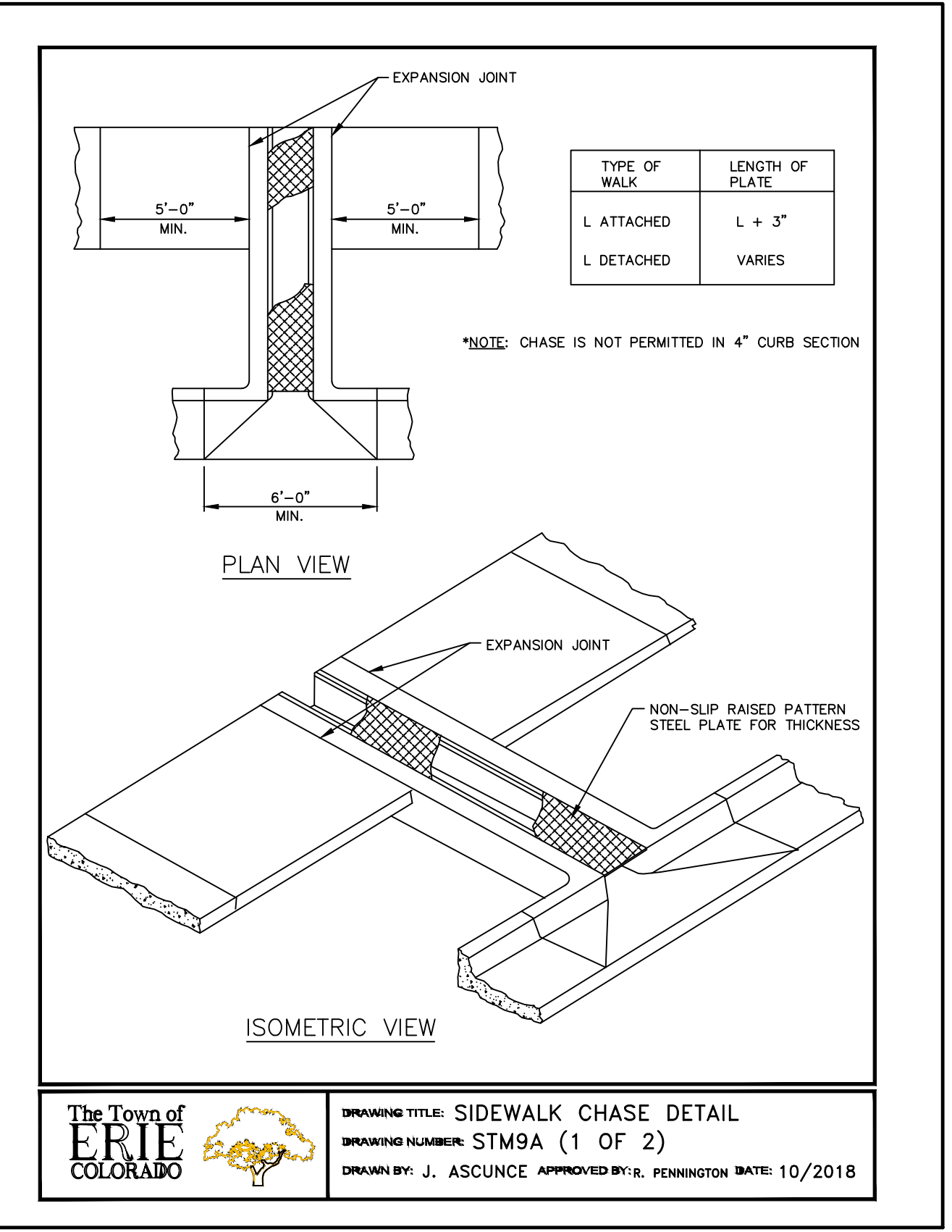
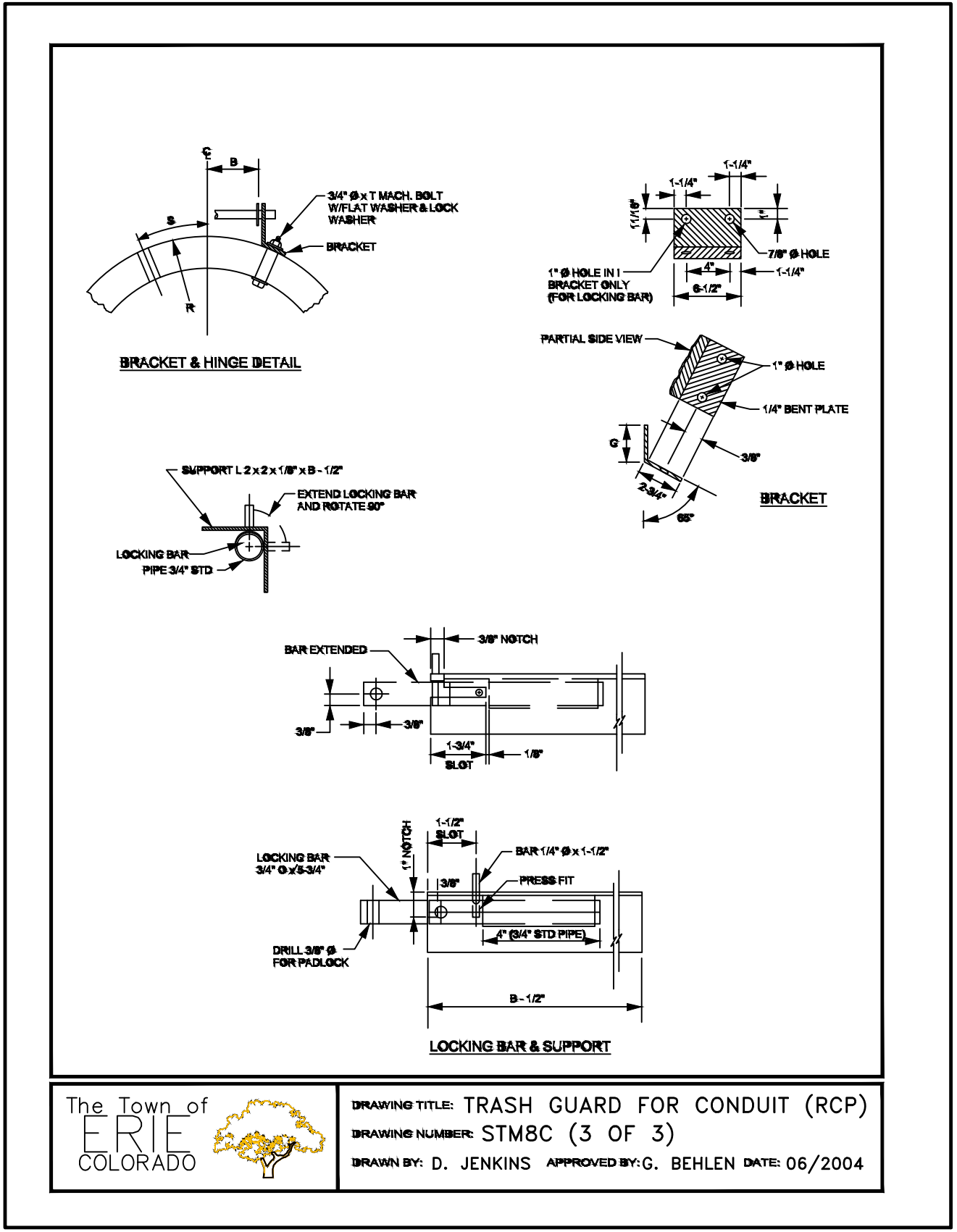
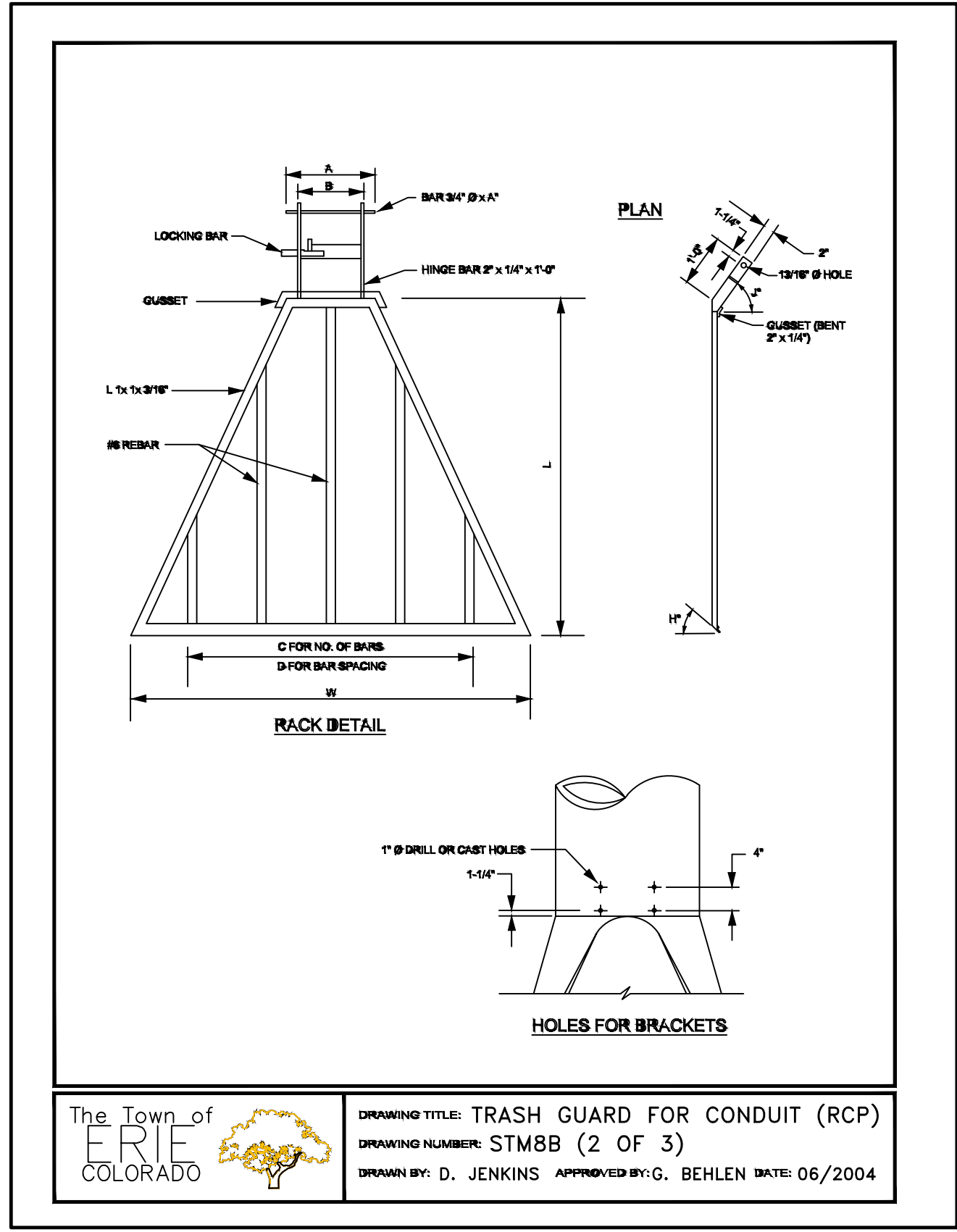
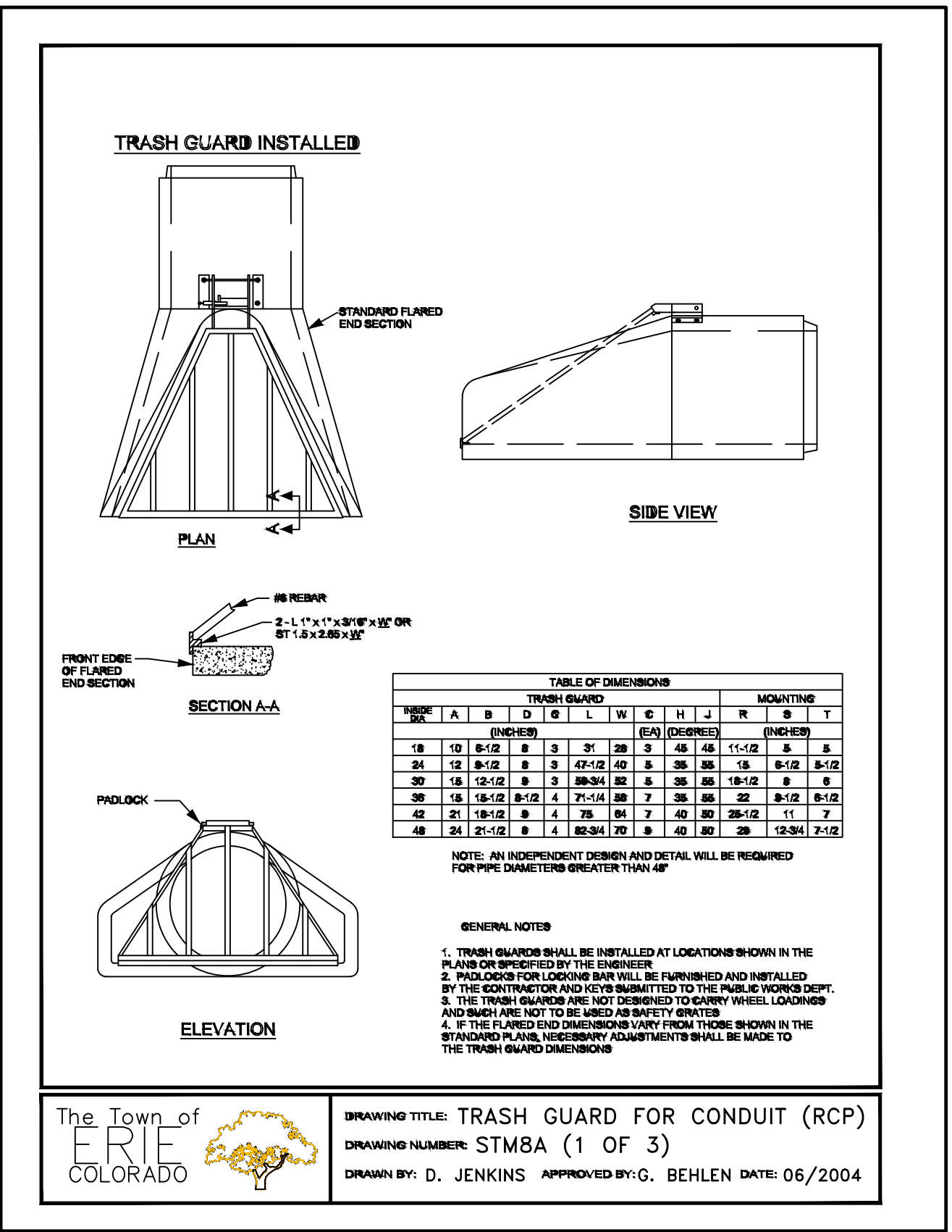
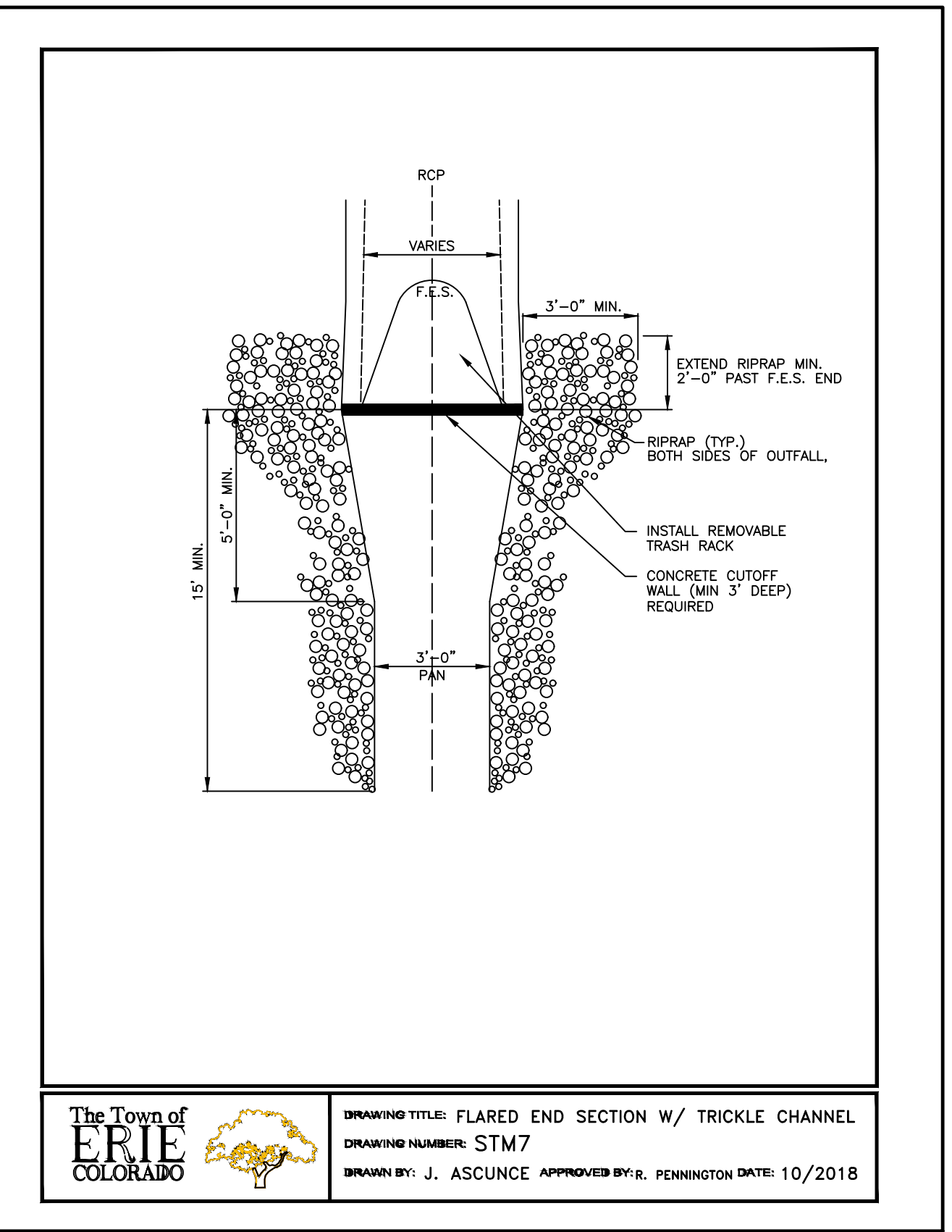
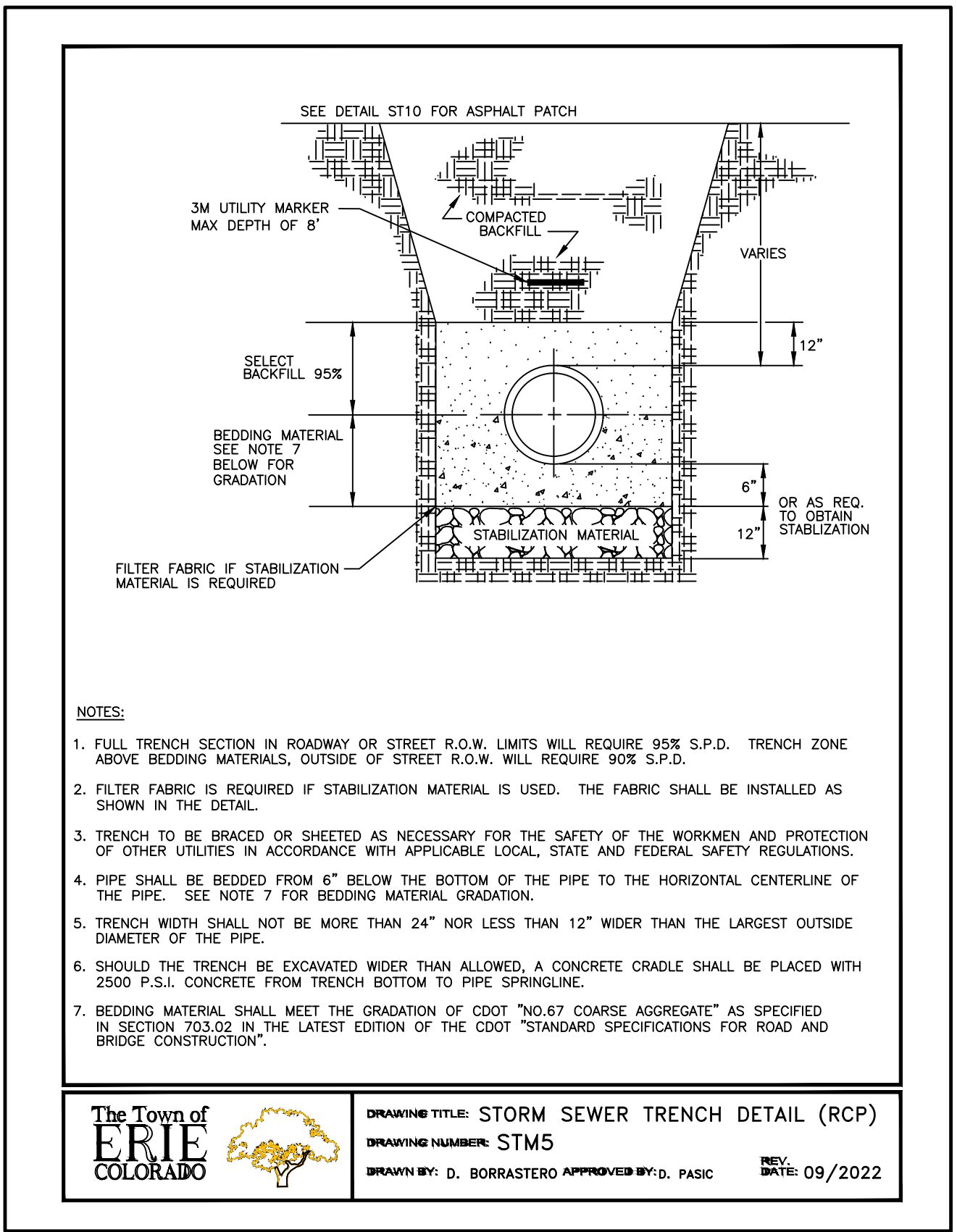
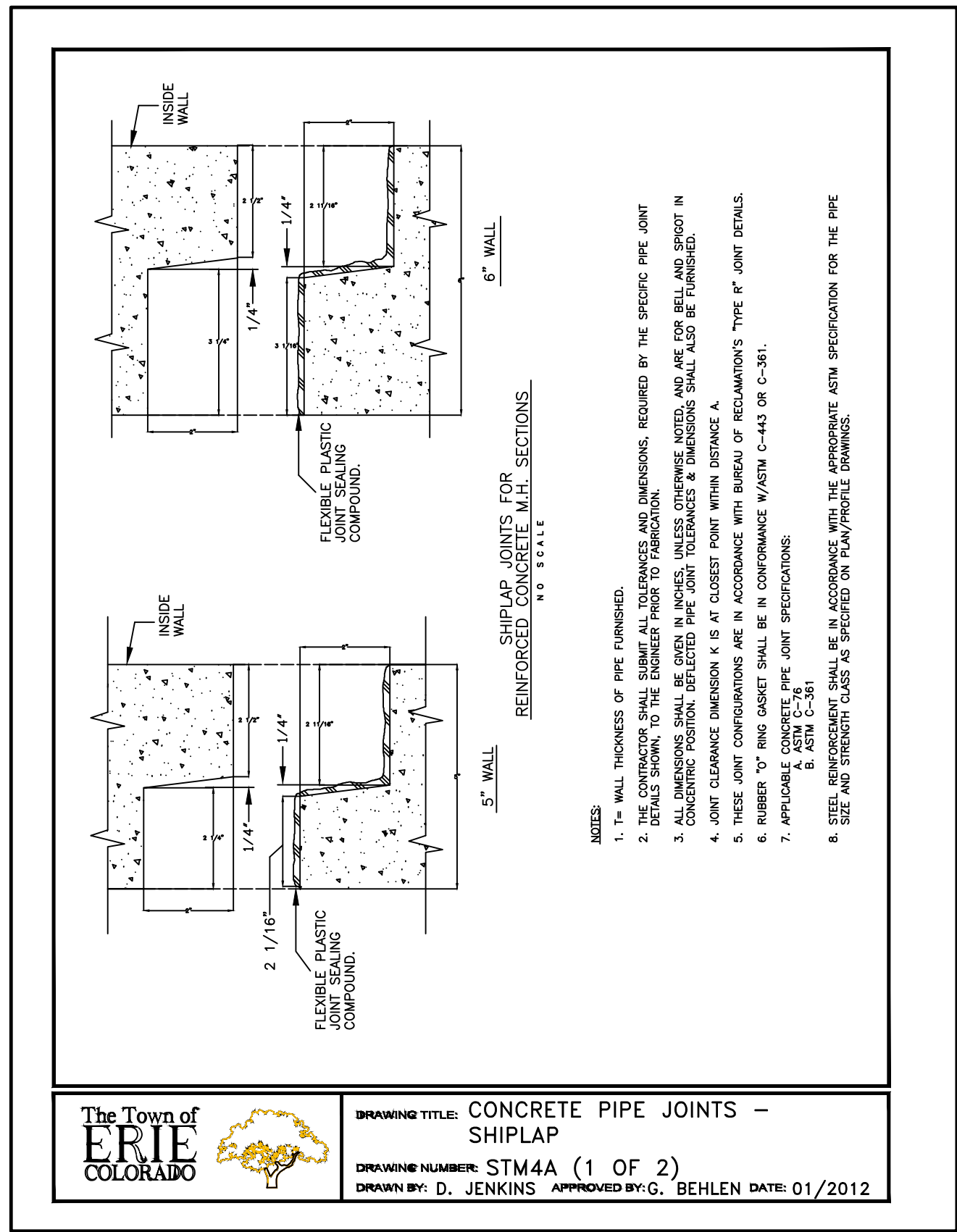
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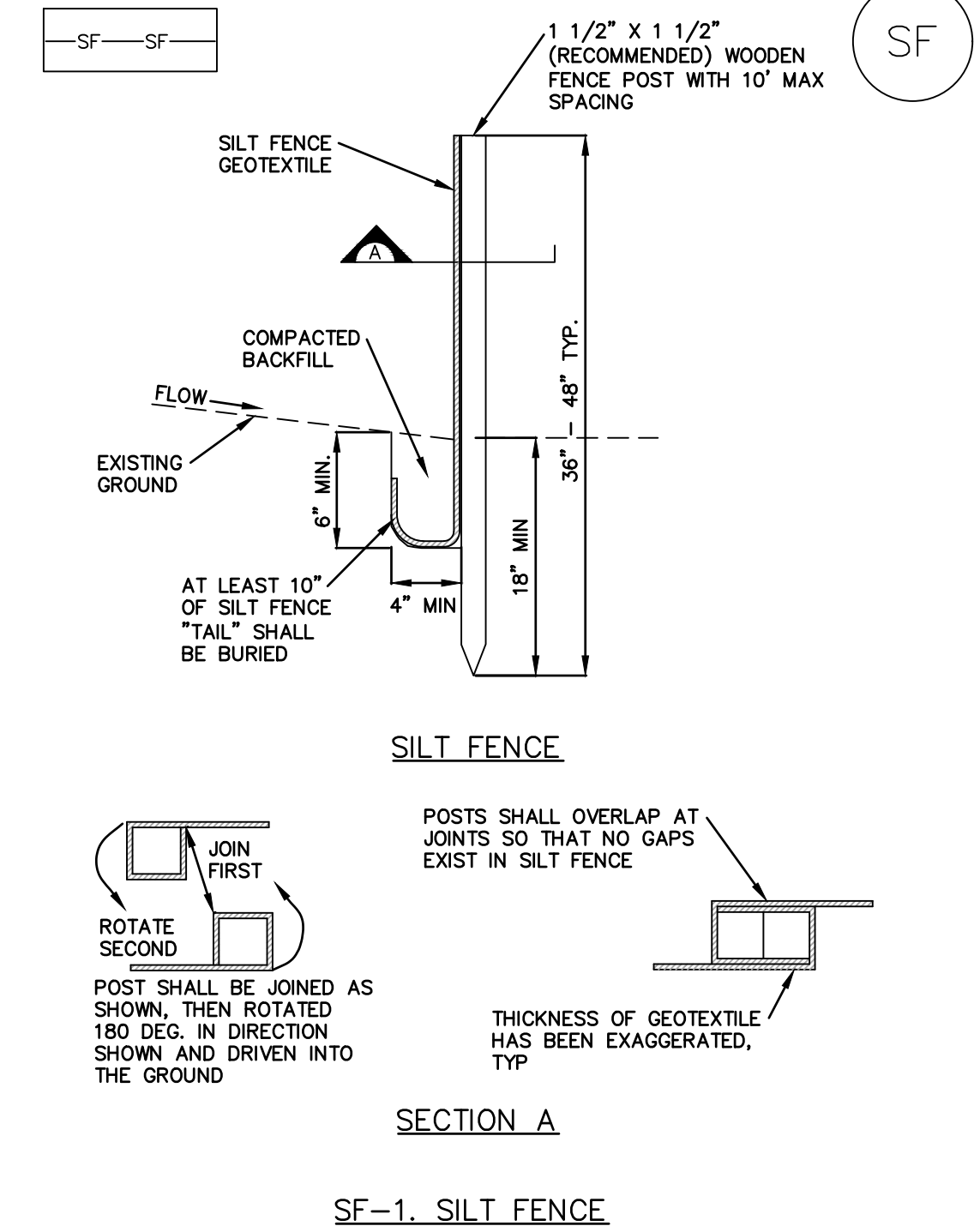
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4	06/14/24	Town Comments

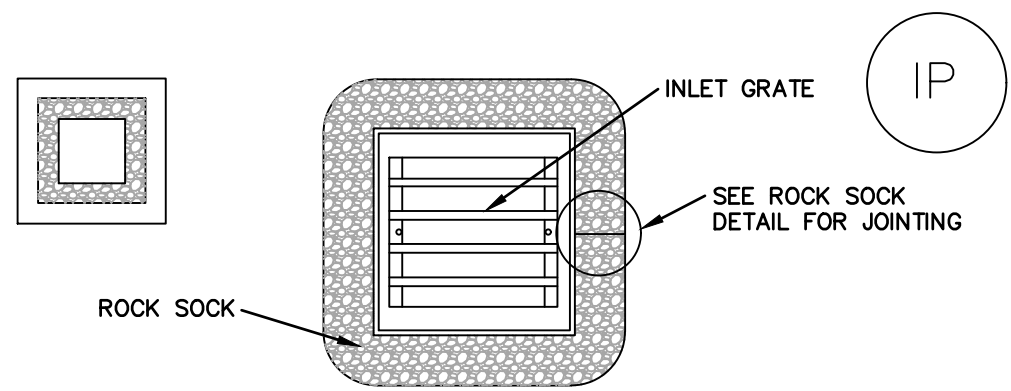
Project No: 23126A
By: JAS/DPA
File: 23126A-1



SF-1. SILT FENCE

1 SILT FENCE DETAIL

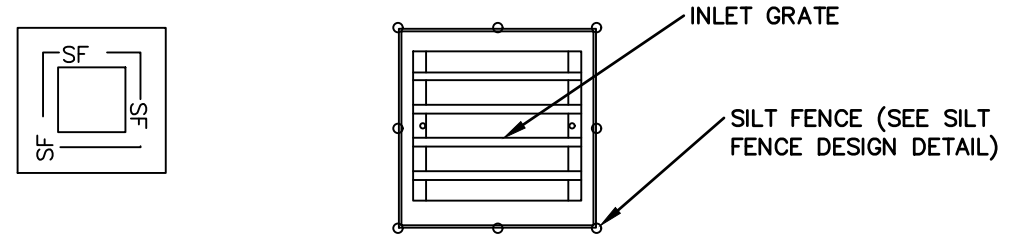
SCALE: N.T.S.



IP-3. ROCK SOCK SUMP/AREA INLET PROTECTION

ROCK SOCK SUMP/AREA INLET PROTECTION INSTALLATION NOTES

- SEE ROCK SOCK DESIGN DETAIL FOR INSTALLATION REQUIREMENTS.
- STRAW WATTLES/SEDIMENT CONTROL LOGS MAY BE USED IN PLACE OF ROCK SOCKS FOR INLETS IN PERVIOUS AREAS. INSTALL PER SEDIMENT CONTROL LOG DETAIL.



IP-4. SILT FENCE FOR SUMP INLET PROTECTION

SILT FENCE INLET PROTECTION INSTALLATION NOTES

- SEE SILT FENCE DESIGN DETAIL FOR INSTALLATION REQUIREMENTS.
- POSTS SHALL BE PLACED AT EACH CORNER OF THE INLET AND AROUND THE EDGES AT A MAXIMUM SPACING OF 3 FEET.
- STRAW WATTLES/SEDIMENT CONTROL LOGS MAY BE USED IN PLACE OF SILT FENCE FOR INLETS IN PERVIOUS AREAS. INSTALL PER SEDIMENT CONTROL LOG DETAIL.

3 INLET PROTECTION

SCALE: N.T.S.

SILT FENCE INSTALLATION NOTES

- SILT FENCE MUST BE PLACED AWAY FROM THE TOE OF THE SLOPE TO ALLOW FOR WATER PONDING. SILT FENCE AT THE TOE OF A SLOPE SHOULD BE INSTALLED IN A FLAT LOCATION AT LEAST SEVERAL FEET (2-5 FT) FROM THE TOE OF THE SLOPE TO ALLOW ROOM FOR PONDING AND DEPOSITION.
- A UNIFORM 6" X 4" ANCHOR TRENCH SHALL BE EXCAVATED USING TRENCHER OR SILT FENCE INSTALLATION DEVICE. NO ROAD GRADERS, BACKHOES, OR SIMILAR EQUIPMENT SHALL BE USED.
- COMPACT ANCHOR TRENCH BY HAND WITH A "JUMPING JACK OR BY WHEEL ROLLING. COMPACTION SHALL BE SUCH THAT SILT FENCE RESISTS BEING PULLED OUT OF ANCHOR TRENCH BY HAND.
- SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD BE NO NOTICEABLE SAG BETWEEN STAKES AFTER IT HAS BEEN ANCHORED TO THE STAKES.
- SILT FENCE FABRIC SHALL BE ANCHORED TO THE STAKES USING 1" HEAVY DUTY STAPLES OR NAILS WITH 1" HEADS. STAPLES AND NAILS SHOULD BE PLACED 3" ALONG THE FABRIC DOWN THE STAKE.
- AT THE END OF A RUN OF SILT FENCE ALONG A CONTOUR, THE SILT FENCE SHOULD BE TURNED PERPENDICULAR TO THE CONTOUR TO CREATE A "J-HOOK." THE "J-HOOK" EXTENDING PERPENDICULAR TO THE CONTOUR SHOULD BE OF SUFFICIENT LENGTH TO KEEP RUNOFF FROM FLOWING AROUND THE END OF THE SILT FENCE (TYPICALLY 10' - 20').
- SILT FENCE SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.

SILT FENCE MAINTENANCE NOTES

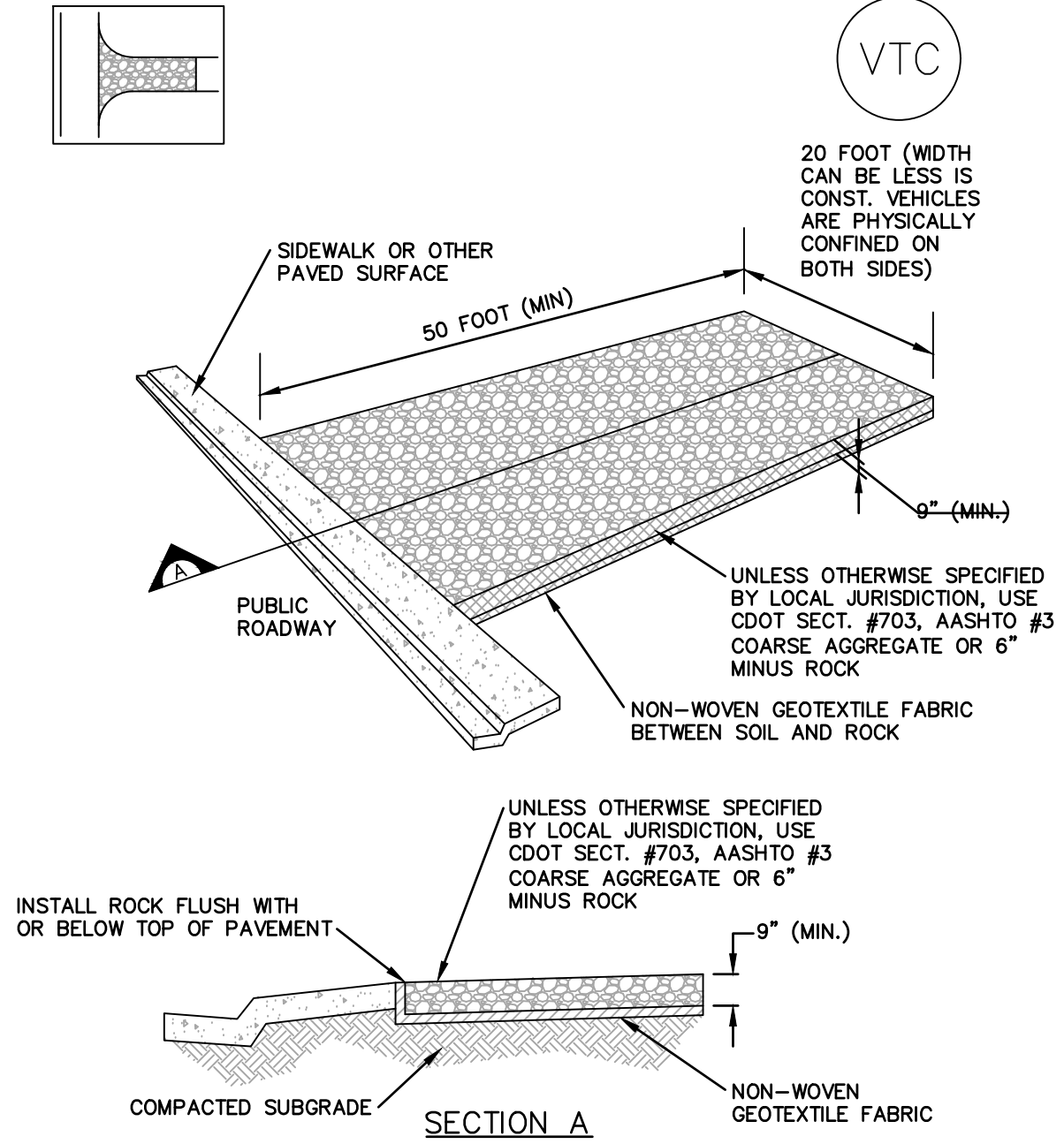
- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- SEDIMENT ACCUMULATED UPSTREAM OF THE SILT FENCE SHALL BE REMOVED AS NEEDED TO MAINTAIN THE FUNCTIONALITY OF THE BMP, TYPICALLY WHEN DEPTH OF ACCUMULATED SEDIMENTS IS APPROXIMATELY 6".
- REPAIR OR REPLACE SILT FENCE WHEN THERE ARE SIGNS OF WEAR, SUCH AS SAGGING, TEARING, OR COLLAPSE.
- SILT FENCE IS TO REMAIN IN PLACE UNTIL THE UPSTREAM DISTURBED AREA IS STABILIZED AND APPROVED BY THE LOCAL JURISDICTION, OR IS REPLACED BY AN EQUIVALENT PERIMETER SEDIMENT CONTROL BMP.
- WHEN SILT FENCE IS REMOVED, ALL DISTURBED AREAS SHALL BE COVERED WITH TOPSOIL, SEEDED AND MULCHED OR OTHERWISE STABILIZED AS APPROVED BY LOCAL JURISDICTION.

GENERAL INLET PROTECTION INSTALLATION NOTES

- SEE PLAN VIEW FOR:
 - LOCATION OF INLET PROTECTION.
 - TYPE OF INLET PROTECTION (IP-1, IP-2, IP-3, IP-4, IP-5, IP-6)
- INLET PROTECTION SHALL BE INSTALLED PROMPTLY AFTER INLET CONSTRUCTION OR PAVING IS COMPLETE (TYPICALLY WITHIN 48 HOURS). IF A RAINFALL/RUNOFF EVENT IS FORECAST, INSTALL INLET PROTECTION PRIOR TO ONSET OF EVENT.
- MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCO STANDARD DETAILS. CONSULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN DIFFERENCES ARE NOTED.

INLET PROTECTION MAINTENANCE NOTES

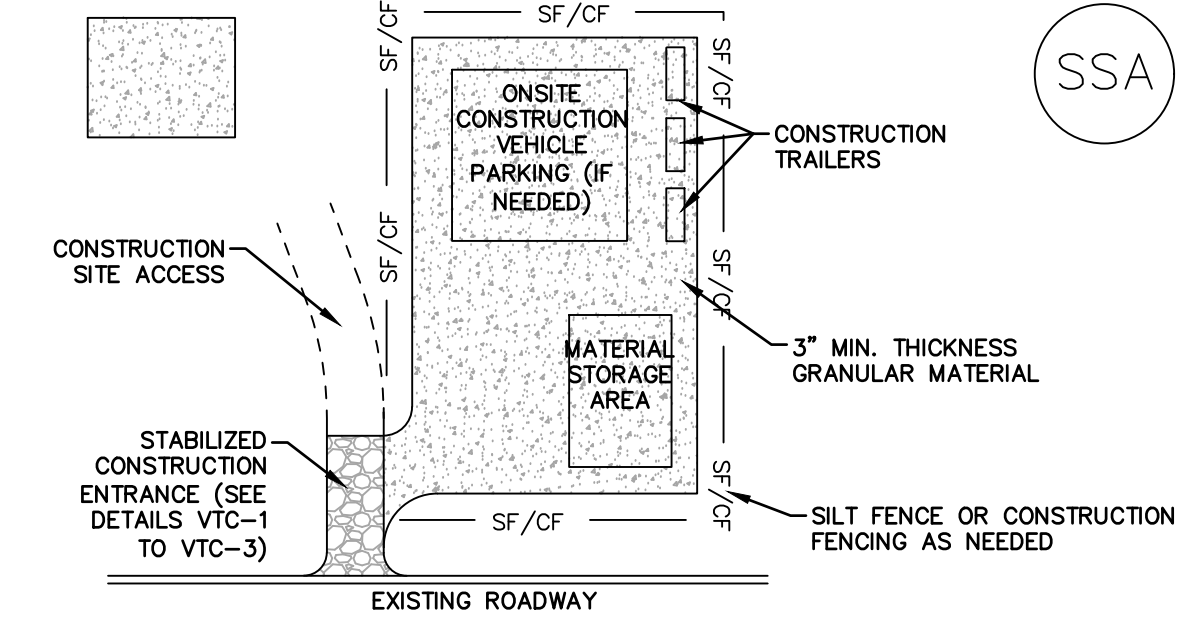
- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- SEDIMENT ACCUMULATED UPSTREAM OF INLET PROTECTION SHALL BE REMOVED AS NECESSARY TO MAINTAIN BMP EFFECTIVENESS, TYPICALLY WHEN STORAGE VOLUME REACHES 50% OF CAPACITY, A DEPTH OF 6" WHEN SILT FENCE IS USED, OR 1/4 OF THE HEIGHT FOR STRAW BALES.
- INLET PROTECTION IS TO REMAIN IN PLACE UNTIL THE UPSTREAM DISTURBED AREA IS PERMANENTLY STABILIZED, UNLESS THE LOCAL JURISDICTION APPROVES EARLIER REMOVAL OF INLET PROTECTION IN STREETS.
- WHEN INLET PROTECTION AT AREA INLETS IS REMOVED, THE DISTURBED AREA SHALL BE COVERED WITH TOP SOIL, SEEDED AND MULCHED, OR OTHERWISE STABILIZED IN A MANNER APPROVED BY THE LOCAL JURISDICTION.



VTC-1. AGGREGATE VEHICLE TRACKING CONTROL

2 VEHICLE TRACKING CONTROL DETAIL

SCALE: N.T.S.



SSA-1. STABILIZED STAGING AREA

STABILIZED STAGING AREA INSTALLATION NOTES

- SEE PLAN VIEW FOR:
 - LOCATION OF STAGING AREA(S).
 - CONTRACTOR MAY ADJUST LOCATION AND SIZE OF STAGING AREA WITH APPROVAL FROM THE LOCAL JURISDICTION.
- STABILIZED STAGING AREA SHOULD BE APPROPRIATE FOR THE NEEDS OF THE SITE. OVERSIZING RESULTS IN A LARGER AREA TO STABILIZE FOLLOWING CONSTRUCTION.
- STAGING AREA SHALL BE STABILIZED PRIOR TO OTHER OPERATIONS ON THE SITE.
- THE STABILIZED STAGING AREA SHALL CONSIST OF A MINIMUM 3" THICK GRANULAR MATERIAL.
- UNLESS OTHERWISE SPECIFIED BY LOCAL JURISDICTION, ROCK SHALL CONSIST OF DOT SECT. #703, AASHTO #3 COARSE AGGREGATE OR 6" (MINUS) ROCK.
- ADDITIONAL PERIMETER BMPs MAY BE REQUIRED INCLUDING BUT NOT LIMITED TO SILT FENCE AND CONSTRUCTION FENCING.

STABILIZED STAGING AREA MAINTENANCE NOTES

- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- ROCK SHALL BE REAPPLIED OR REGRADED AS NECESSARY IF RUTTING OCCURS OR UNDERLYING SUBGRADE BECOMES EXPOSED.
- STABILIZED STAGING AREA SHALL BE ENLARGED IF NECESSARY TO CONTAIN PARKING, STORAGE, AND UNLOADING/LOADING OPERATIONS.
- THE STABILIZED STAGING AREA SHALL BE REMOVED AT THE END OF CONSTRUCTION. THE GRANULAR MATERIAL SHALL BE REMOVED OR, IF APPROVED BY THE LOCAL JURISDICTION, USED ON SITE, AND THE AREA COVERED WITH TOPSOIL, SEEDED AND MULCHED OR OTHERWISE STABILIZED IN A MANNER APPROVED BY LOCAL JURISDICTION.

4 STABILIZED STAGING AREA

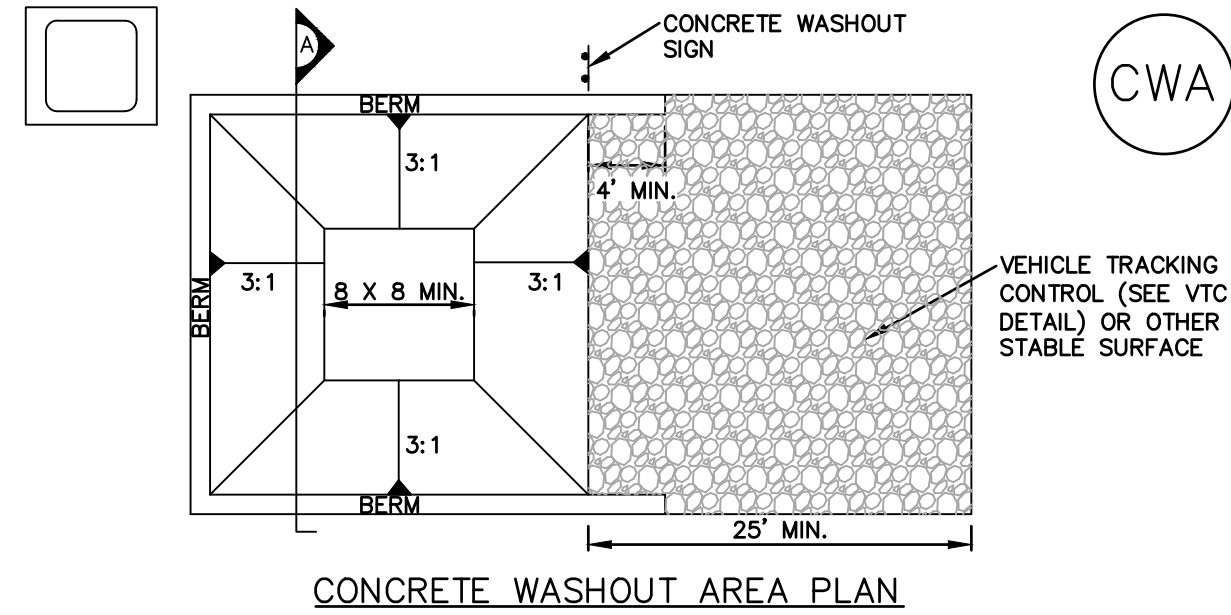
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STABILIZED CONSTRUCTION ENTRANCE/EXIT INSTALLATION NOTES

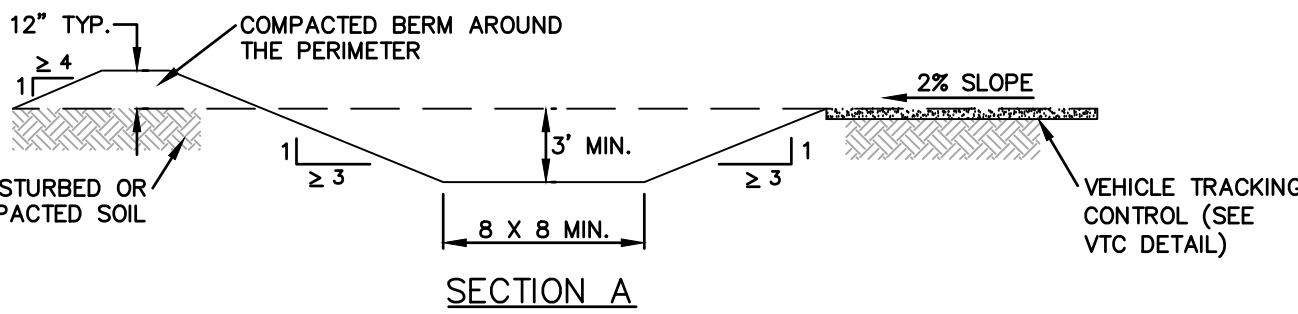
- SEE PLAN VIEW FOR:
 - LOCATION OF CONSTRUCTION ENTRANCE(S)/EXIT(S).
 - TYPE OF CONSTRUCTION ENTRANCE(S)/EXIT(S) (WITH/WITHOUT WHEEL WASH, CONSTRUCTION MAT OR TRM).
- CONSTRUCTION MAT OR TRM STABILIZED CONSTRUCTION ENTRANCES ARE ONLY TO BE USED ON SHORT DURATION PROJECTS (TYPICALLY RANGING FROM A WEEK TO A MONTH) WHERE THERE WILL BE LIMITED VEHICULAR ACCESS.
- A STABILIZED CONSTRUCTION ENTRANCE/EXIT SHALL BE LOCATED AT ALL ACCESS POINTS WHERE VEHICLES ACCESS THE CONSTRUCTION SITE FROM PAVED RIGHT-OF-WAYS.
- STABILIZED CONSTRUCTION ENTRANCE/EXIT SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.
- A NON-WOVEN GEOTEXTILE FABRIC SHALL BE PLACED UNDER THE STABILIZED CONSTRUCTION ENTRANCE/EXIT PRIOR TO THE PLACEMENT OF ROCK.
- UNLESS OTHERWISE SPECIFIED BY LOCAL JURISDICTION, ROCK SHALL CONSIST OF DOT SECT. #703, AASHTO #3 COARSE AGGREGATE OR 6" (MINUS) ROCK.

STABILIZED CONSTRUCTION ENTRANCE/EXIT MAINTENANCE NOTES

- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- ROCK SHALL BE REAPPLIED OR REGRADED AS NECESSARY TO THE STABILIZED ENTRANCE/EXIT TO MAINTAIN A CONSISTENT DEPTH.
- SEDIMENT TRACKED ONTO PAVED ROADS IS TO BE REMOVED THROUGHOUT THE DAY AND AT THE END OF THE DAY BY SHOVELING OR SWEEPING. SEDIMENT MAY NOT BE WASHED DOWN STORM SEWER DRAINS.



CWA-1. CONCRETE WASHOUT AREA



CWA-1. CONCRETE WASHOUT AREA

CWA INSTALLATION NOTES

- SEE PLAN VIEW FOR:
 - CWA INSTALLATION LOCATION.
- DO NOT LOCATE AN UNLINED CWA WITHIN 400' OF ANY NATURAL DRAINAGE PATHWAY OR WATERBODY. DO NOT LOCATE WITHIN 1,000' OF ANY WELLS OR DRINKING WATER SOURCES. IF SITE CONSTRAINTS MAKE THIS INFEASIBLE, OR IF HIGHLY PERMEABLE SOILS EXIST ON SITE, THE CWA MUST BE INSTALLED WITH AN IMPERMEABLE LINER (16 MIL MIN. THICKNESS) OR SURFACE STORAGE ALTERNATIVES USING PREFABRICATED CONCRETE WASHOUT DEVICES OR A LINED ABOVE GROUND STORAGE AREA SHOULD BE USED.
- THE CWA SHALL BE INSTALLED PRIOR TO CONCRETE PLACEMENT ON SITE.
- CWA SHALL INCLUDE A FLAT SUBSURFACE PIT THAT IS AT LEAST 8' BY 8' SLOPES LEADING OUT OF THE SUBSURFACE PIT SHALL BE 3:1 OR FLATTER. THE PIT SHALL BE AT LEAST 3' DEEP.
- BERM SURROUNDING SIDES AND BACK OF THE CWA SHALL HAVE MINIMUM HEIGHT OF 1'.
- VEHICLE TRACKING PAD SHALL BE SLOPED 2% TOWARDS THE CWA.
- SIGNS SHALL BE PLACED AT THE CONSTRUCTION ENTRANCE, AT THE CWA, AND ELSEWHERE AS NECESSARY TO CLEARLY INDICATE THE LOCATION OF THE CWA TO OPERATORS OF CONCRETE TRUCKS AND PUMP RIGS.
- USE EXCAVATED MATERIAL FOR PERIMETER BERM CONSTRUCTION.

CWA MAINTENANCE NOTES

- INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- THE CWA SHALL BE REPAIRED, CLEANED, OR ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR CONCRETE WASTE. CONCRETE MATERIALS, ACCUMULATED IN PIT, SHALL BE REMOVED ONCE THE MATERIALS HAVE REACHED A DEPTH OF 2'.
- CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE IN A WATER-TIGHT CONTAINER AND DISPOSED OF PROPERLY.
- THE CWA SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.
- WHEN THE CWA IS REMOVED, COVER THE DISTURBED AREA WITH TOP SOIL, SEED AND MULCH OR OTHERWISE STABILIZED IN A MANNER APPROVED BY THE LOCAL JURISDICTION.

5 CONCRETE WASHOUT AREA DETAIL

SCALE: N.T.S.

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For
Submittal

Lot 1
Erie Air Park
Replat D
3020-3100 Airport Drive
Erie, Colorado 80516

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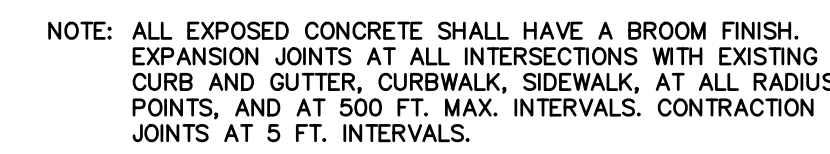
The diagram illustrates two types of construction joints: a Contraction Joint and an Expansion Joint.

CONTRACTION JOINT: This joint is formed by a groove in the concrete. A circular inset provides a detailed view of the groove, showing a depth of 1 1/2" and a width of 1/4" R (typ.). A note indicates to "USE 1/4\" MAX. TEMPLATE".

EXPANSION JOINT: This joint is formed by a pre-molded non-extruding and resilient type expansion joint material. A circular inset shows a cross-section of the material, which is 3/4" wide. A note specifies to use "3/4\" PRE-MOLDED NON-EXTRUDING AND RESILIENT TYPE EXPANSION JOINT MATERIAL TO MEET ASTM SPECS. D 1751 OR D 1752".

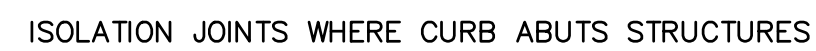
CONTRACTION JOINT

EXPANSION JOINT



CONCRETE SIDEWALK DETAIL

SCALE: N.T.S.



CURB ISOLATION JOINT

1 CURB AND GUTTER DETAIL

SCALE: N.T.S.



SEAL AT CONSTRUCTION JOINTS



TYPICAL CONCRETE PAVEMENT JOINTS

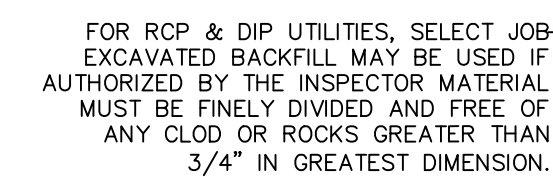
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SCALE: N.T.S.



N.T.S.



D	C	
	MIN.	MAX.
0 - 18"	6"	12"
20 - 28"	9"	12"
30" & LARGER	12"	18"

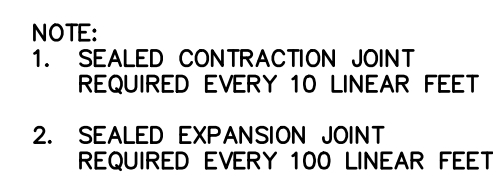
LEGEND

Bc = OUTSIDE DIAMETER
H = BACKFILL COVER ABOVE
TOP OF PIPE
D = INSIDE DIAMETER
d = DEPTH OF BEDDING
MATERIAL BELOW PIPE

D	IN SOIL d (MIN.)	IN ROCK d (MIN.)
27" & SMALLER	3"	6"
30" TO 60"	4"	9"
66" & LARGER	6"	12"

PIPE BEDDING DETAIL

N.T.S.



2' DRAINAGE PAN
N.T.S.

For Submittal

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3020-3100 Airport Drive
Erie, Colorado 80516

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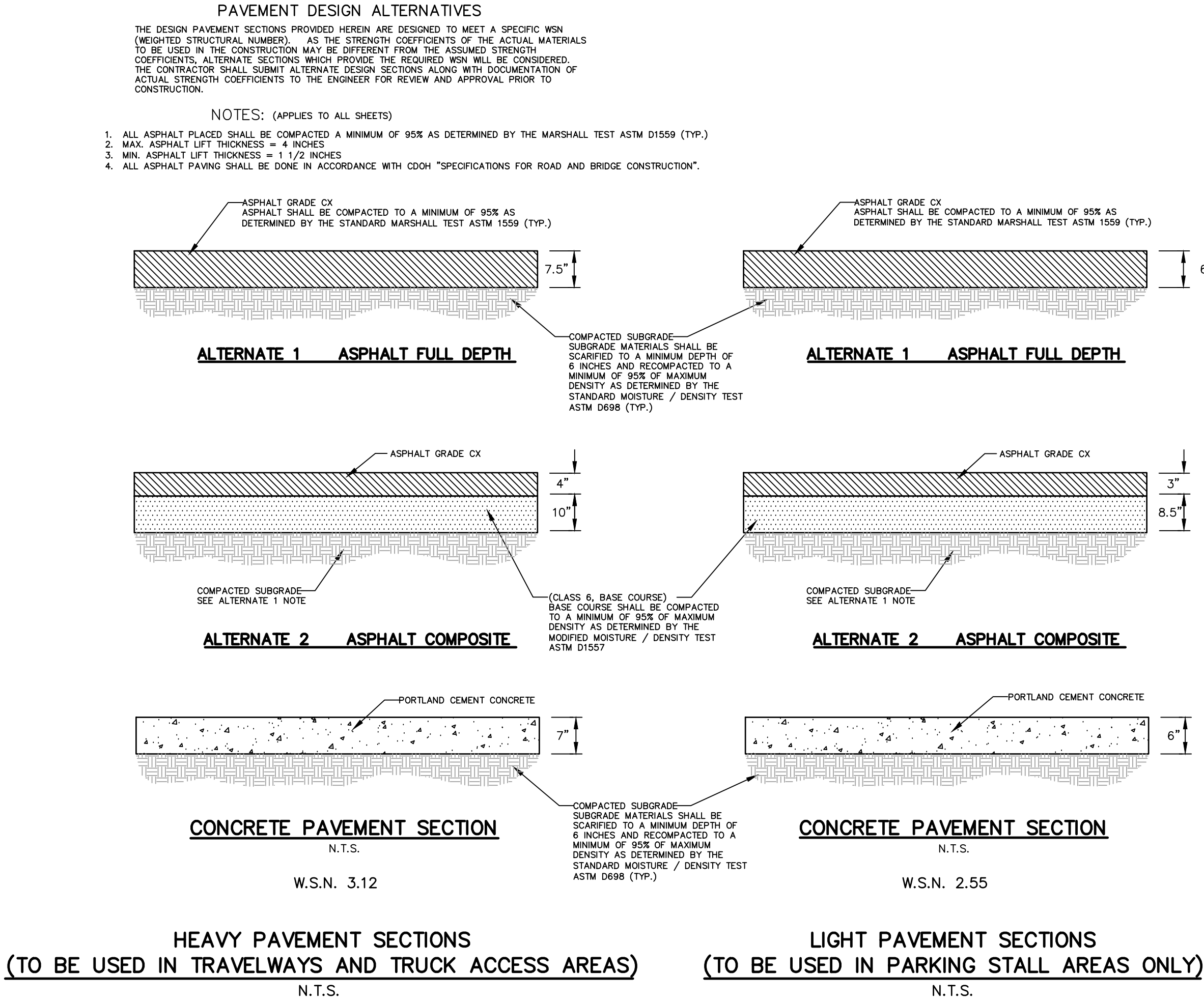
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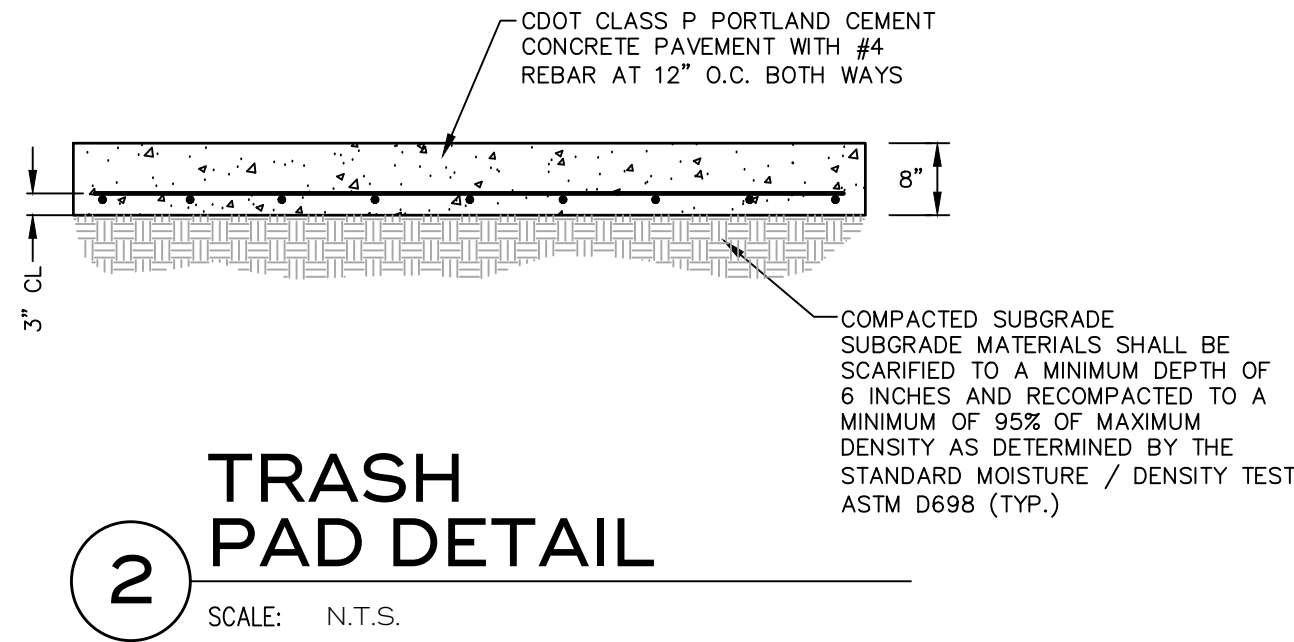
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1 **PAVEMENT SECTIONS**

SCALE: N.T.S.

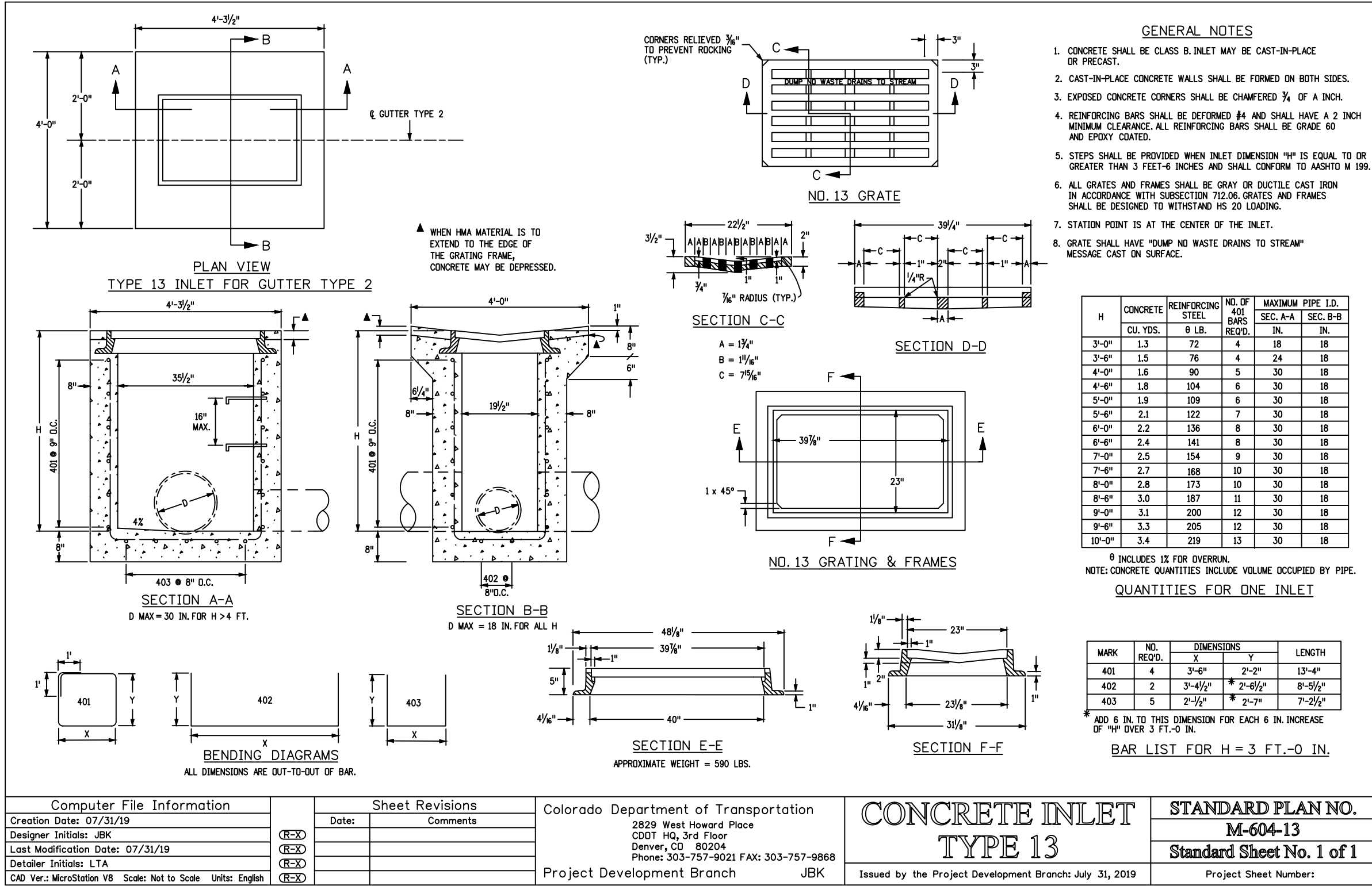
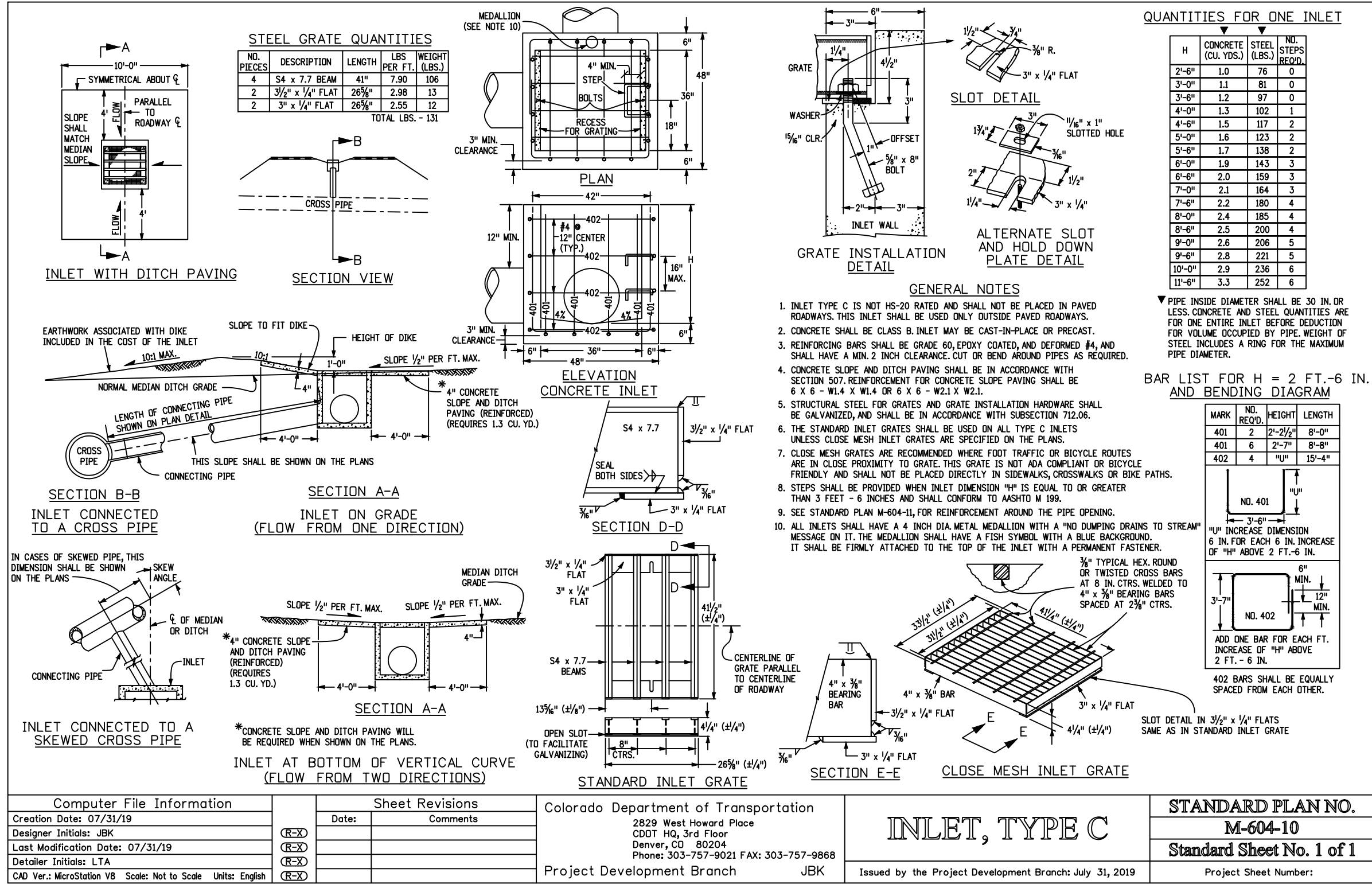


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For
Submittal

Lot 1
Erie Air Park
Replat D
3020-3100 Airport Drive
Erie, Colorado 80516

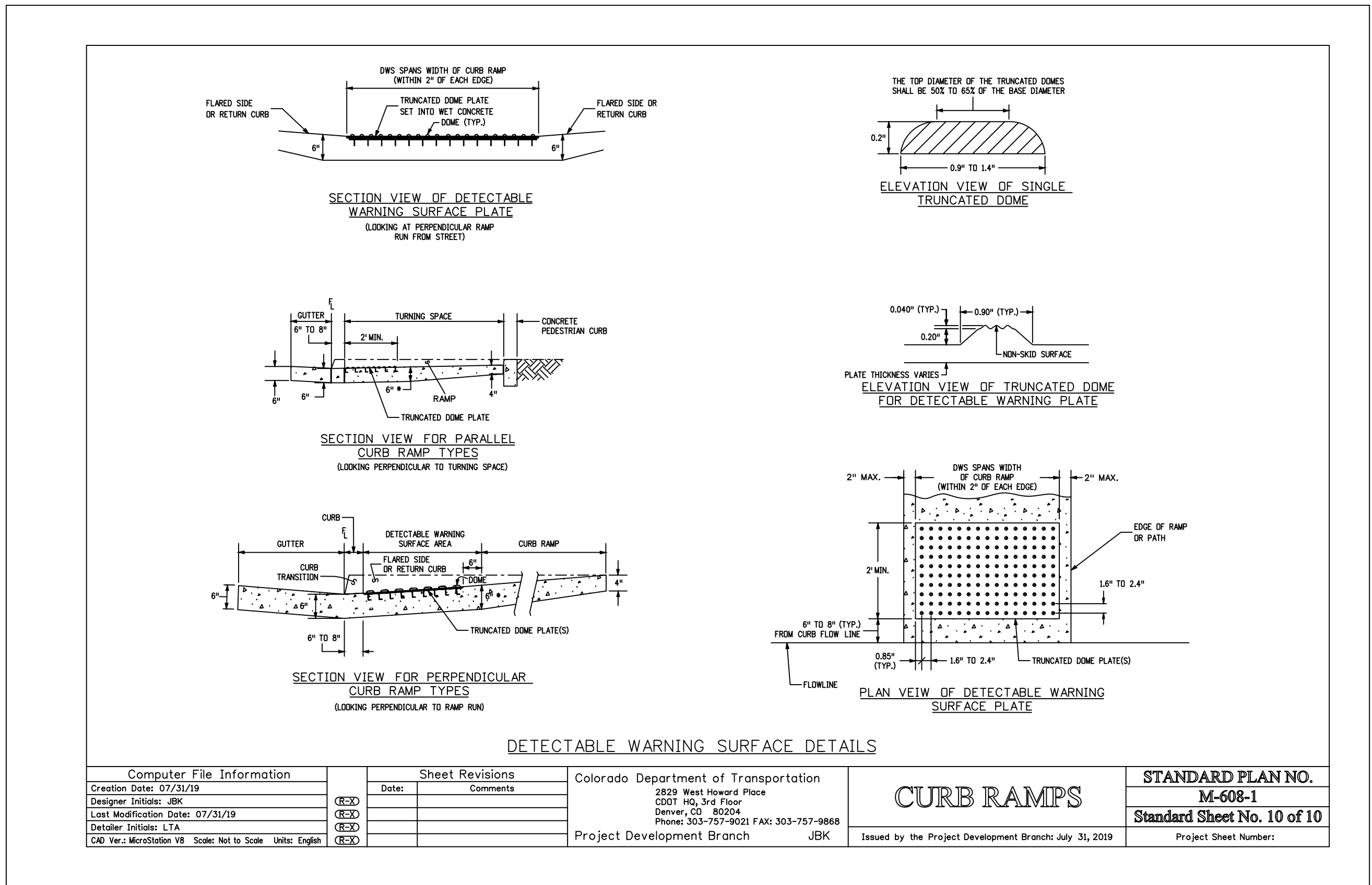
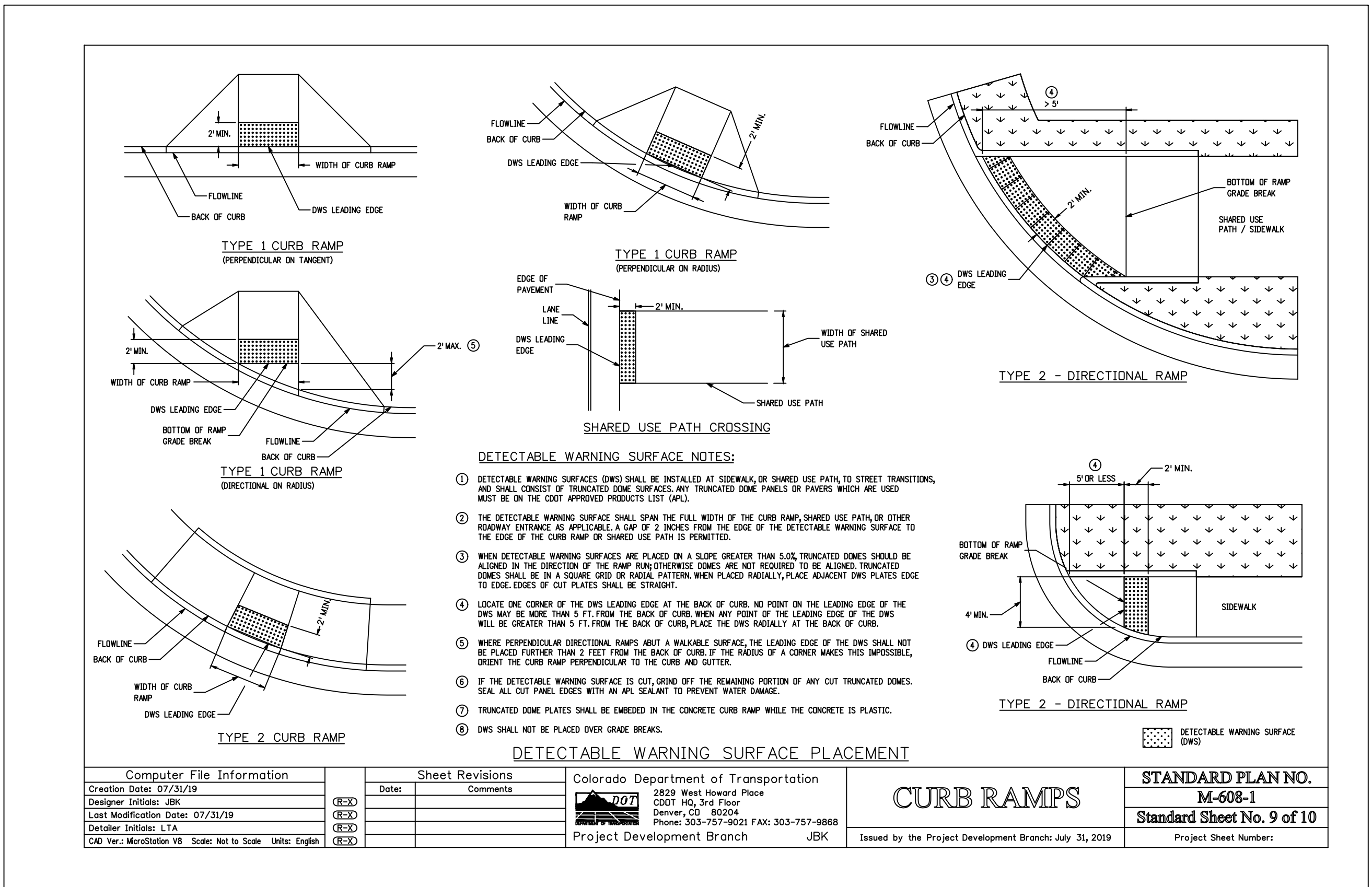
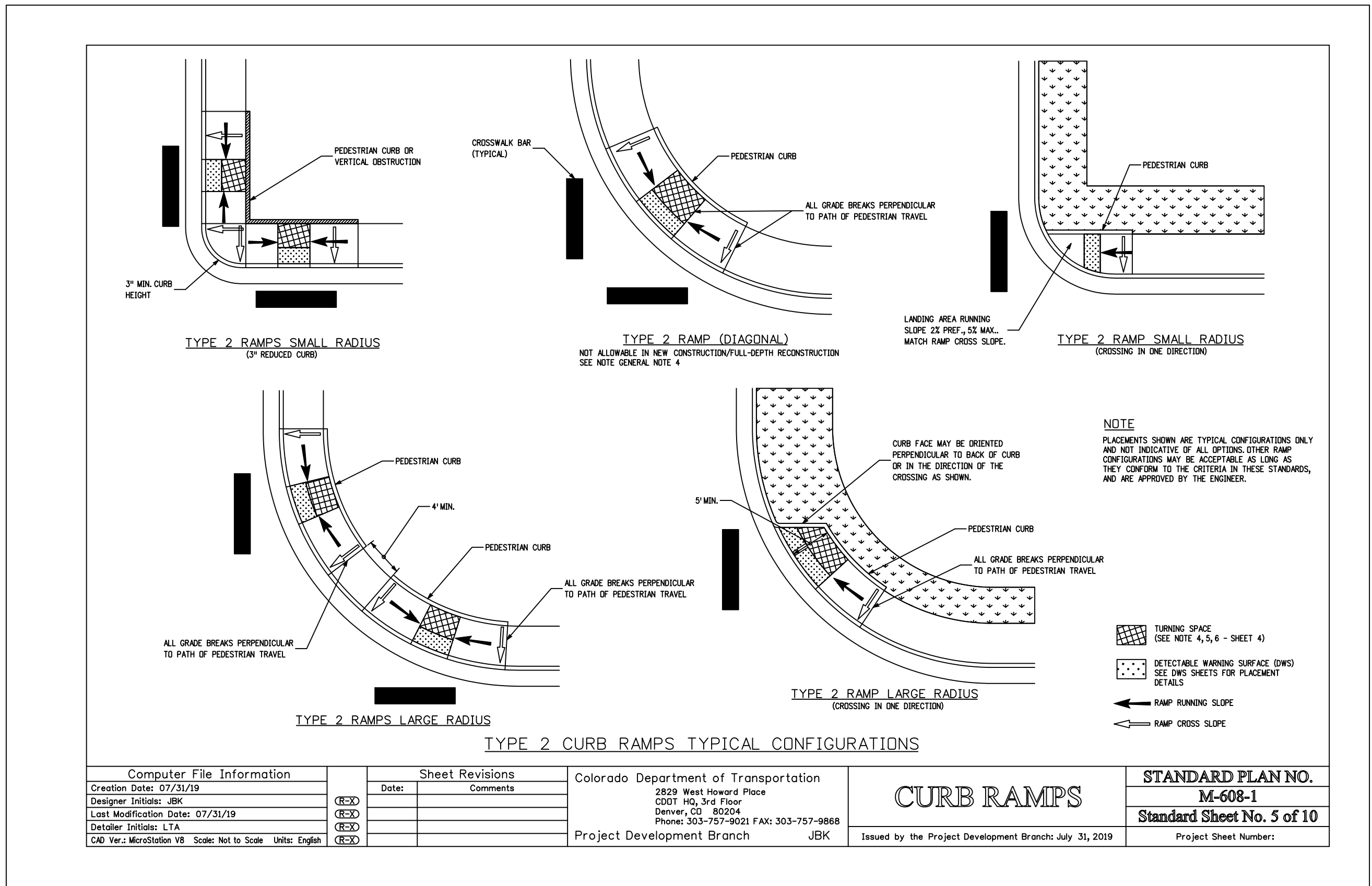
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3	12/08/23	Town Comments
4	06/14/24	Town Comments

Project No: 23126A
By: JAS/DPA
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Sheet

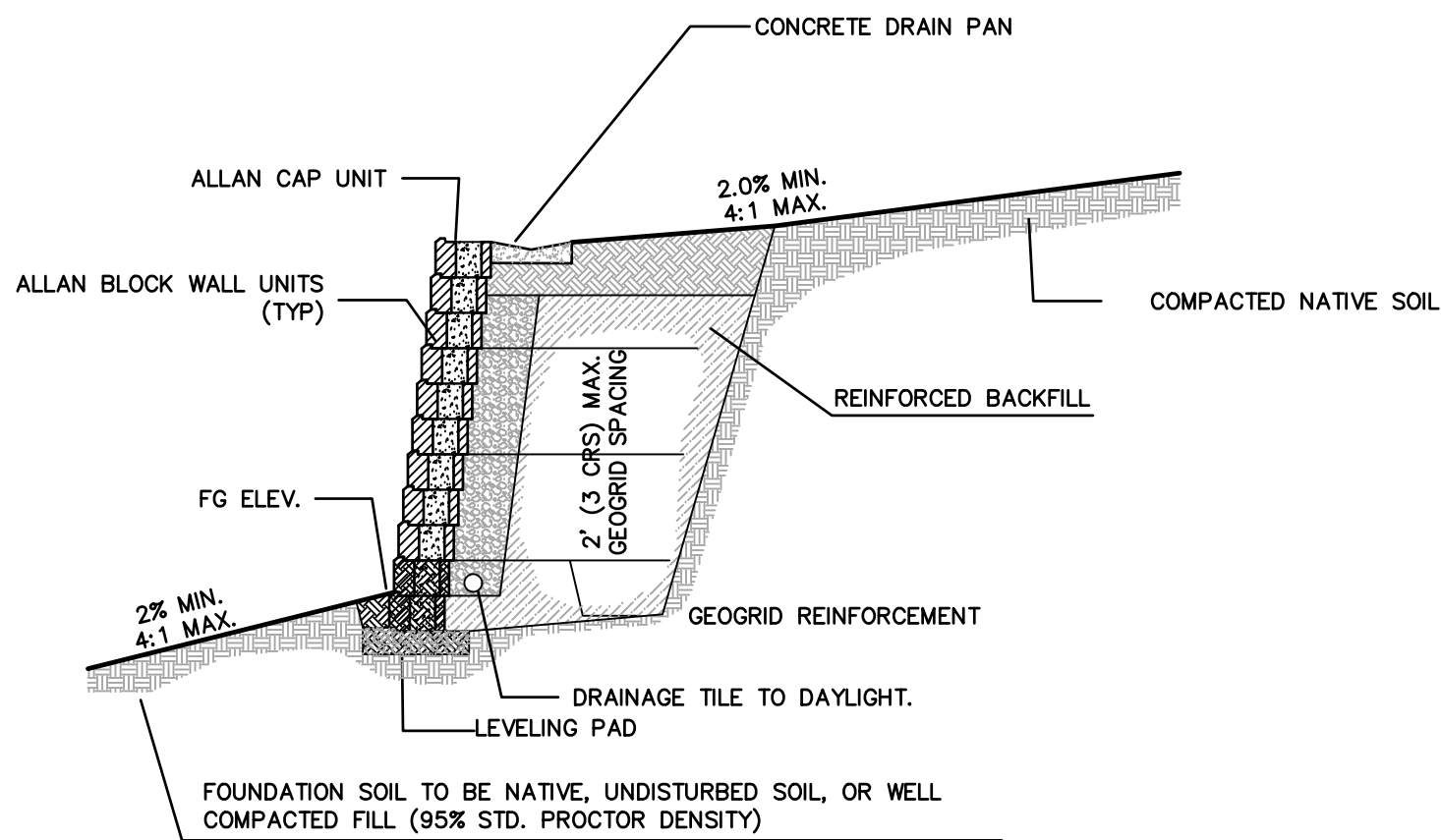
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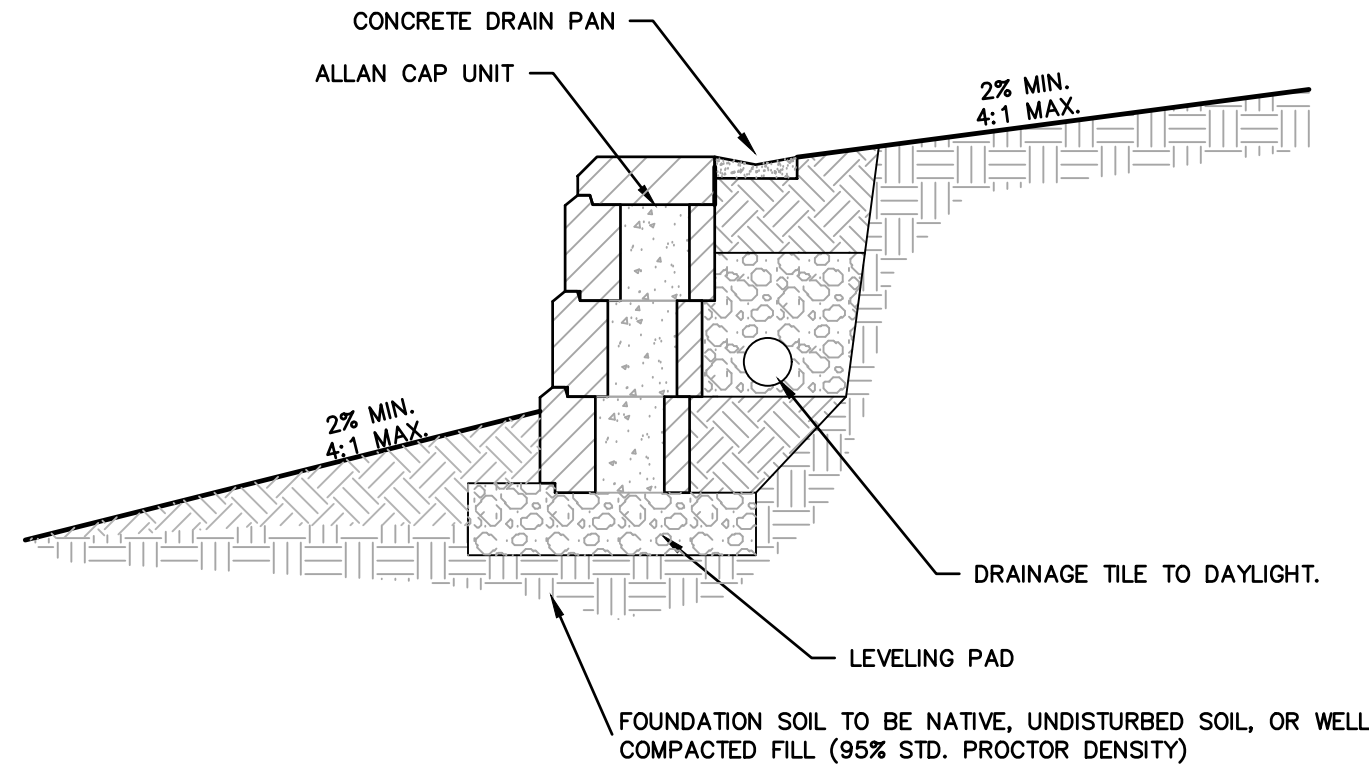
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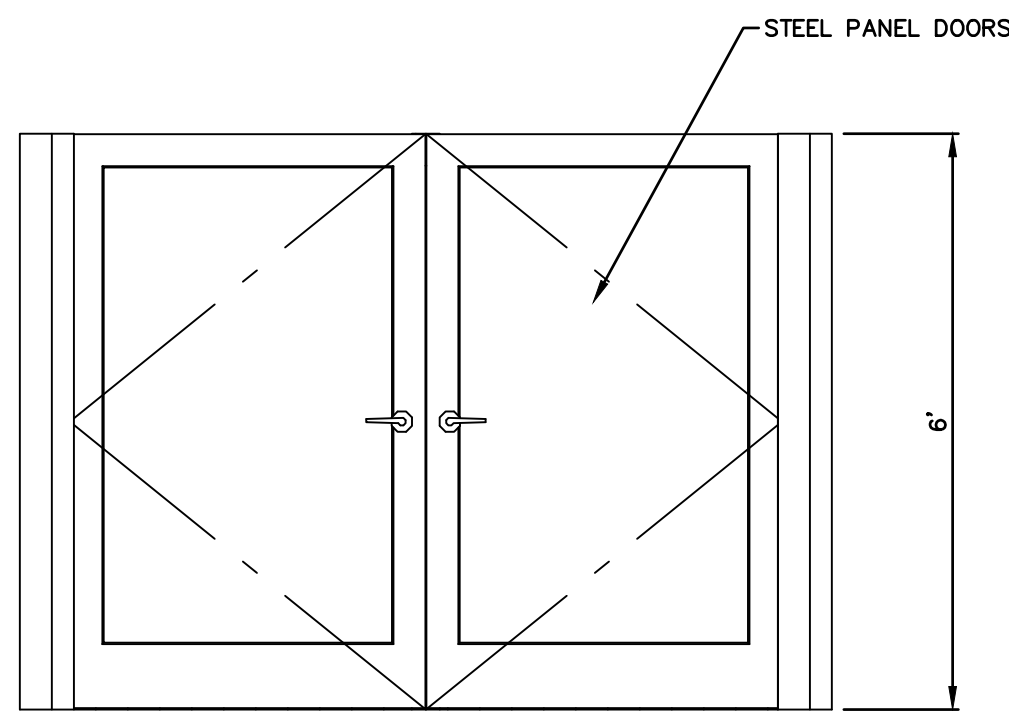
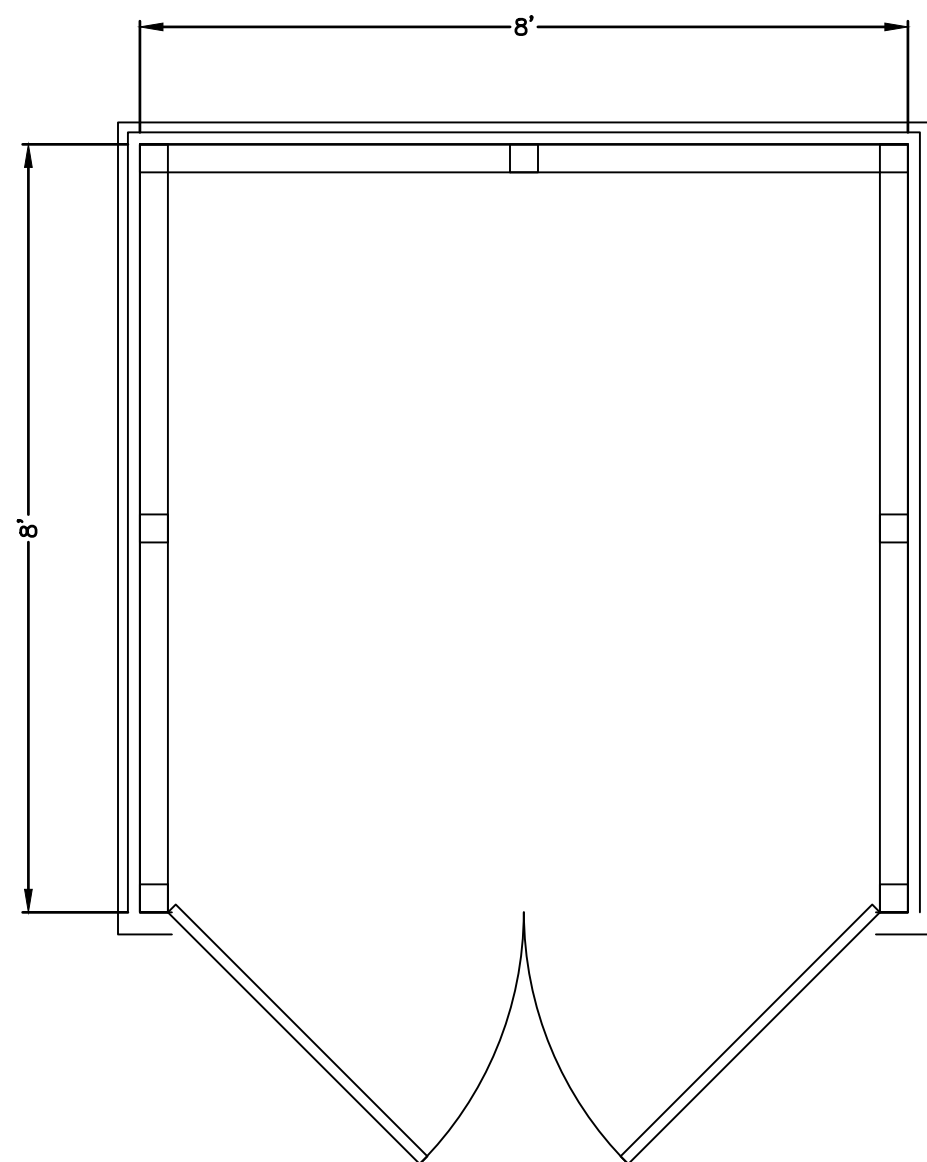


*ALL WALL UNITS SHALL BE ALLAN CLASSIC (ALLAN BLOCK) UNITS AS PROVIDED BY BASALITE CONCRETE PRODUCTS OR APPROVED EQUAL. COLOR SHALL BE TAN BLEND.

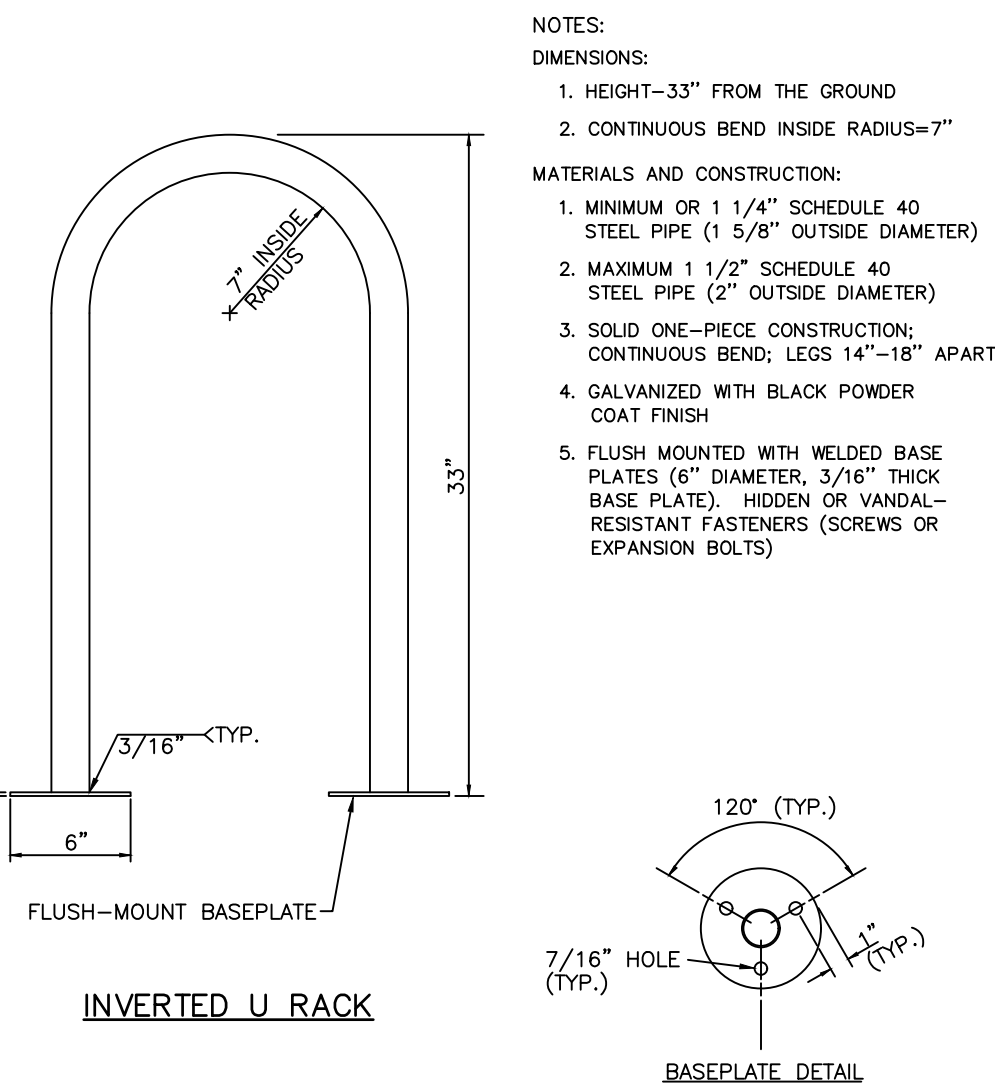
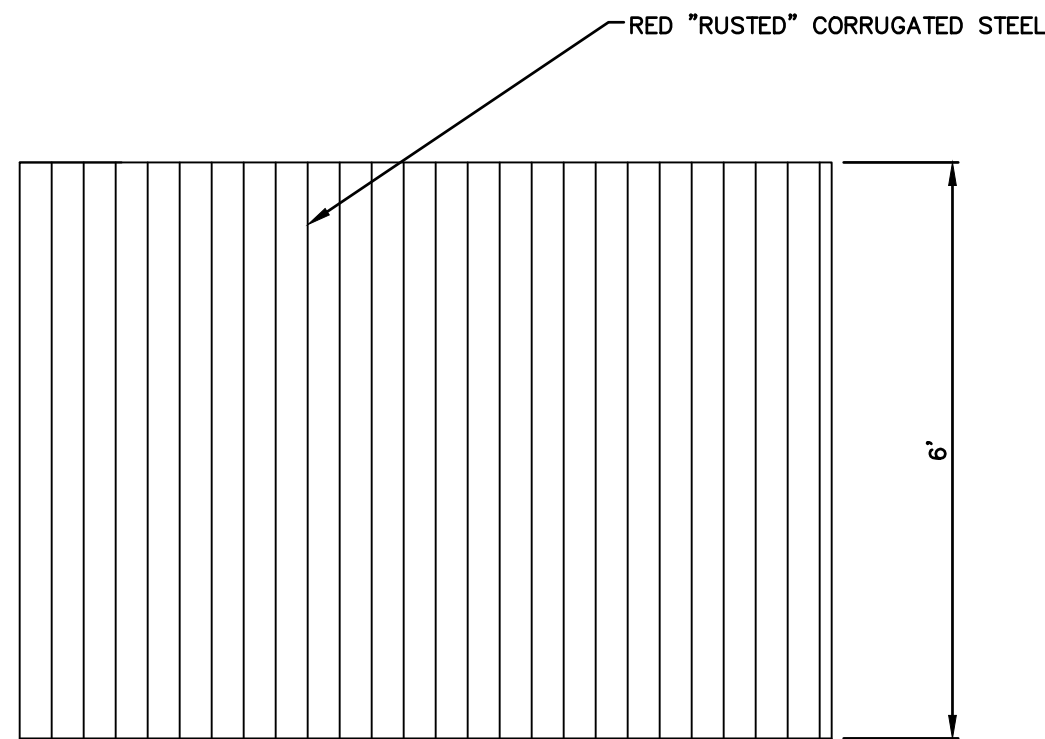


1 RETAINING WALL DETAIL
SCALE: N.T.S.

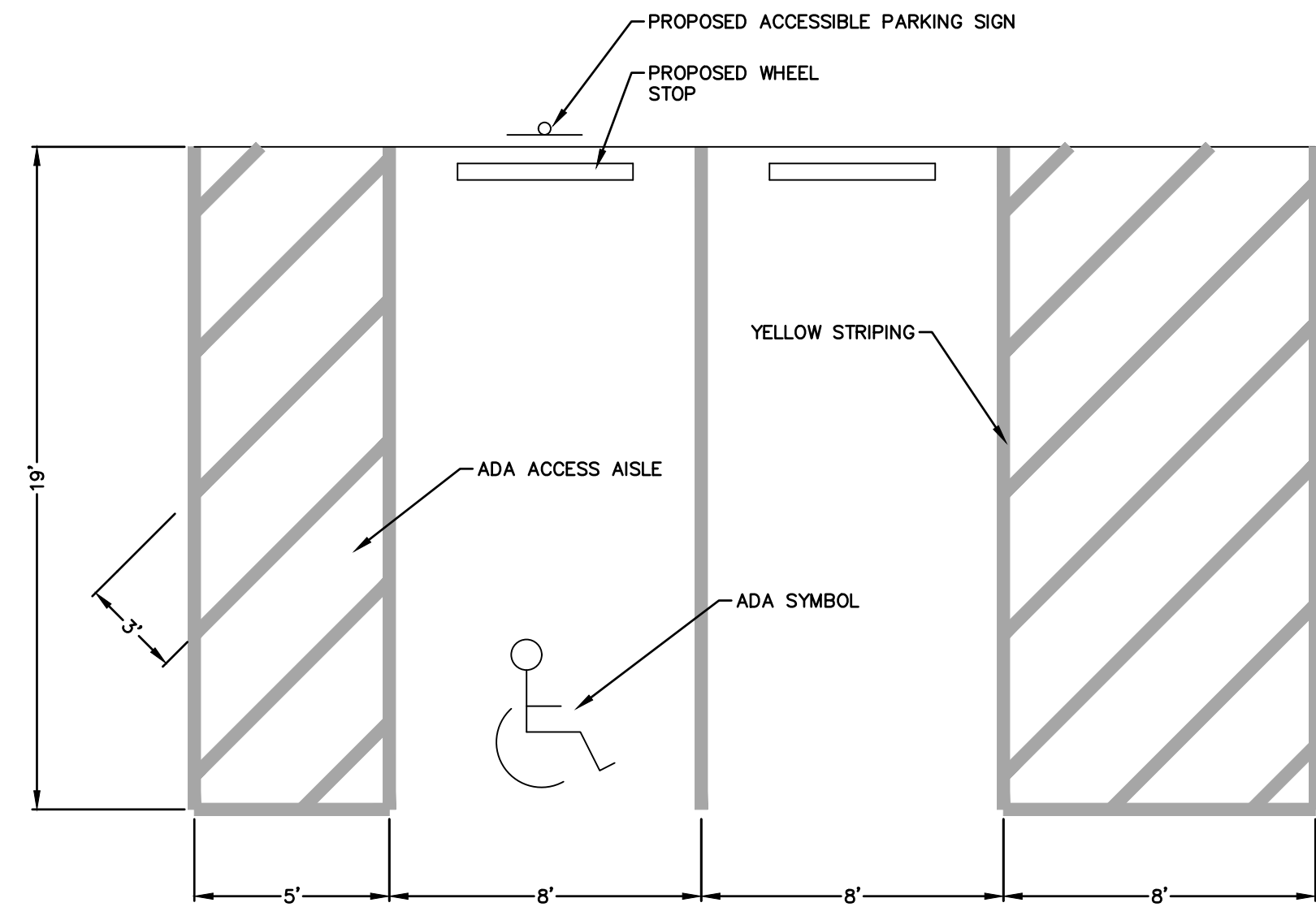
2 RETAINING WALL DETAIL
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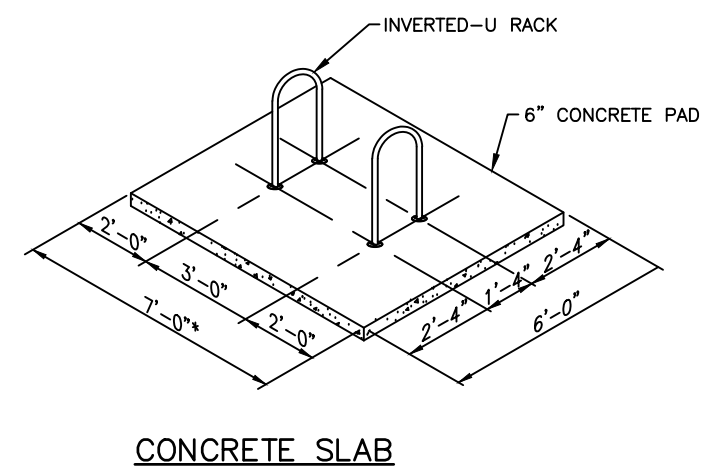
4 TRASH ENCLOSURE DETAIL
SCALE: N.T.S.



5 BIKE RACK DETAIL
SCALE: N.T.S.

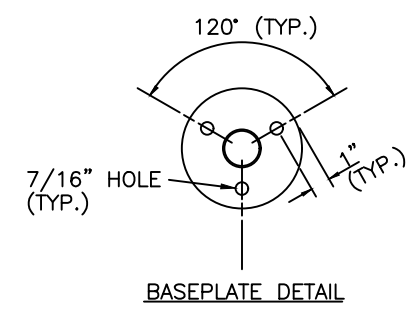
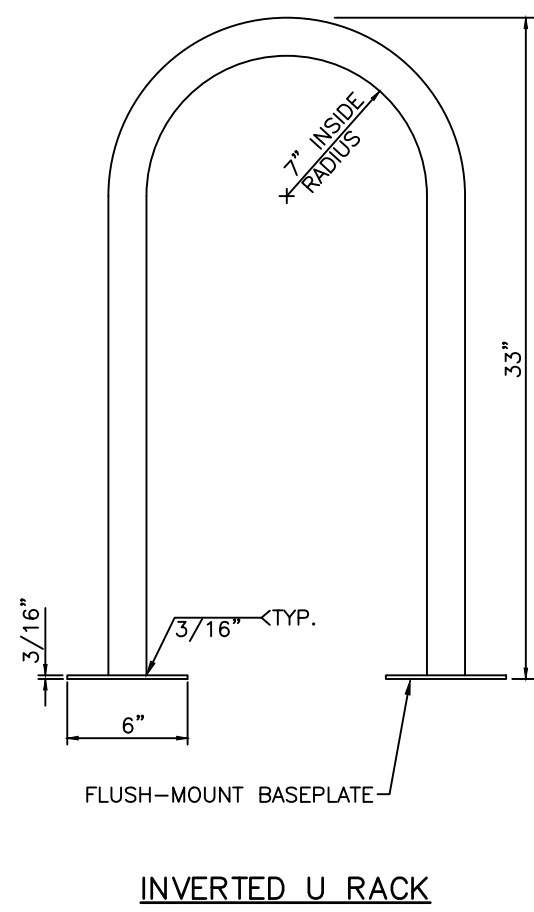


3 ACCESSIBLE PARKING DETAIL
SCALE: N.T.S.



CONCRETE SLAB

NOTES:
DIMENSIONS:
1. HEIGHT-33" FROM THE GROUND
2. CONTINUOUS BEND INSIDE RADIUS=7"
MATERIALS AND CONSTRUCTION:
1. MINIMUM OR 1 1/4" SCHEDULE 40 STEEL PIPE (1 5/8" OUTSIDE DIAMETER)
2. MAXIMUM 1 1/2" SCHEDULE 40 STEEL PIPE (2" OUTSIDE DIAMETER)
3. SOLID ONE-PIECE CONSTRUCTION; CONTINUOUS BEND; LEGS 14"-18" APART
4. GALVANIZED WITH BLACK POWDER COAT FINISH
5. FLUSH MOUNTED WITH WELDED BASE PLATES (6" DIAMETER, 3/16" THICK BASE PLATE). HIDDEN OR VANDAL-RESISTANT FASTENERS (SCREWS OR EXPANSION BOLTS)



BASEPLATE DETAIL

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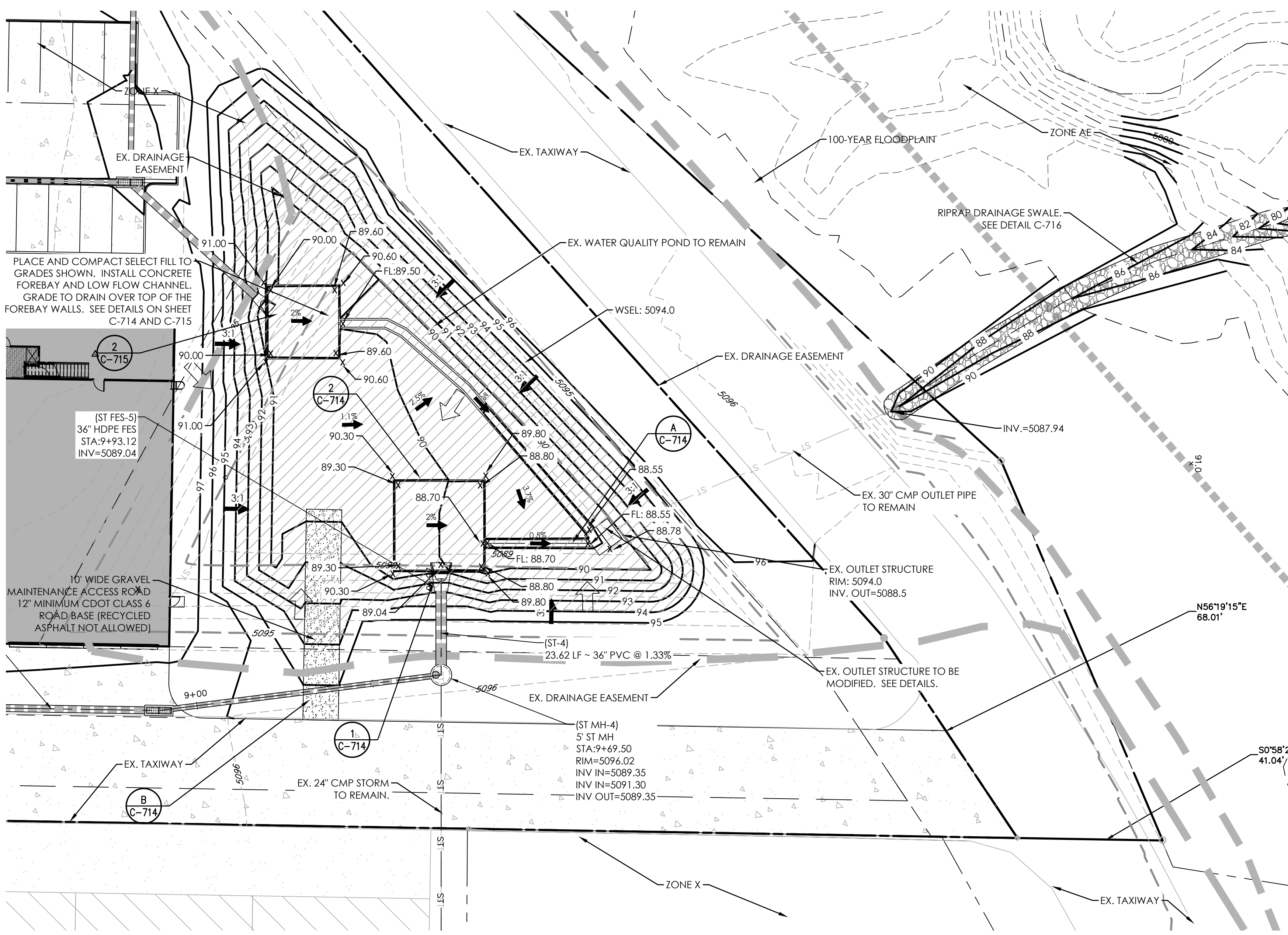
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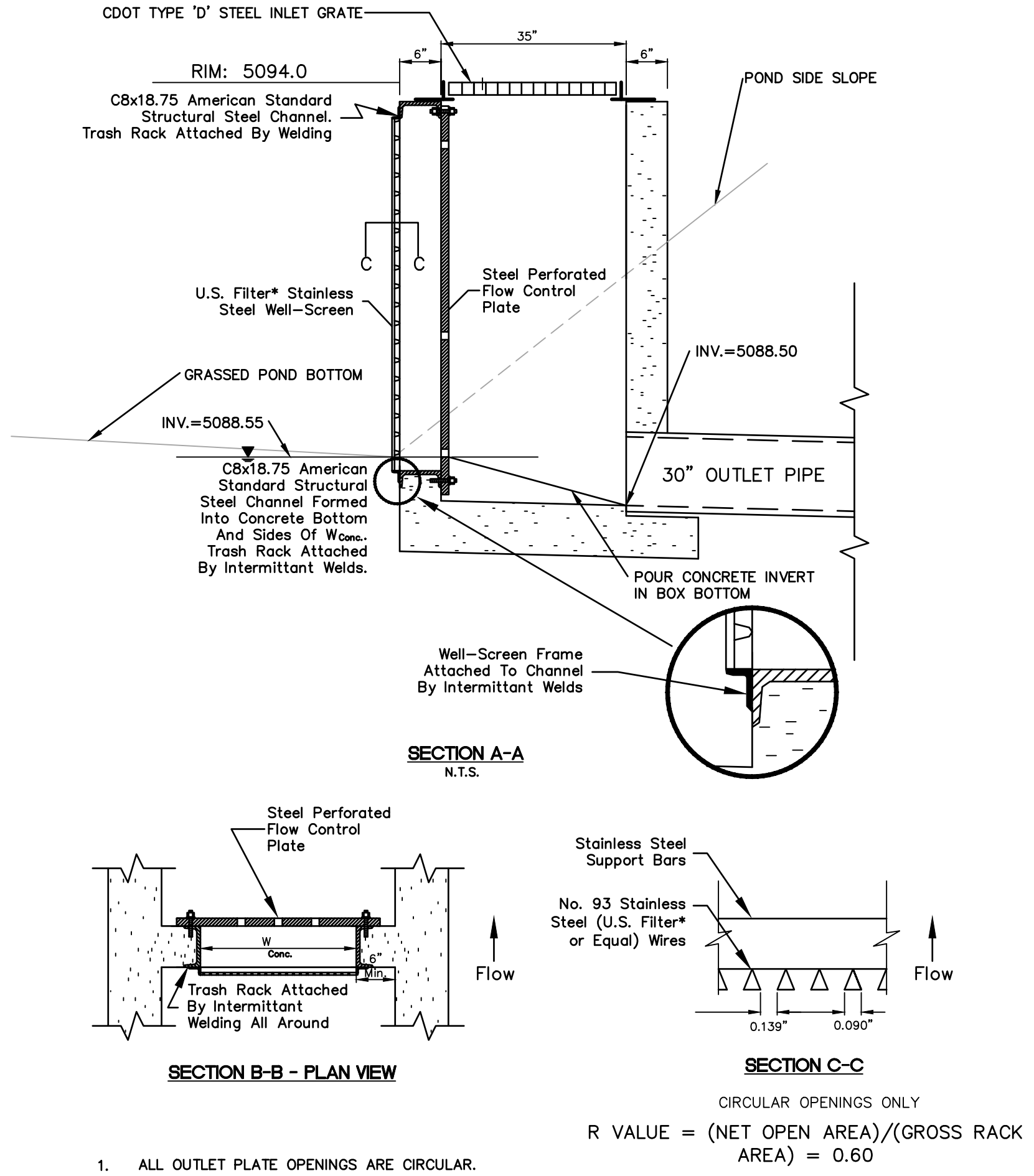
Civil Details

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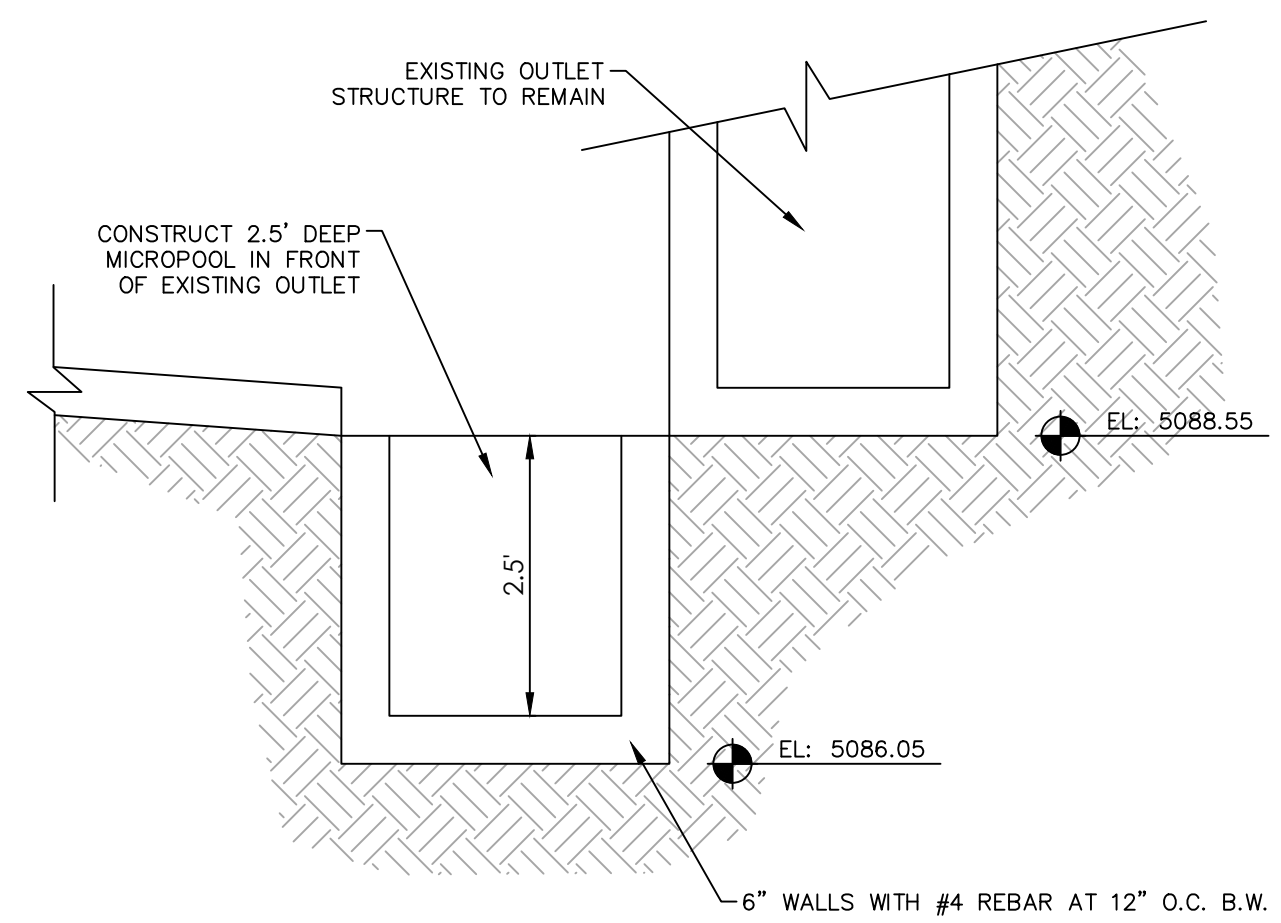
C-713



1 POND DETAIL
SCALE: 1" = 20'
0 20 40 Feet

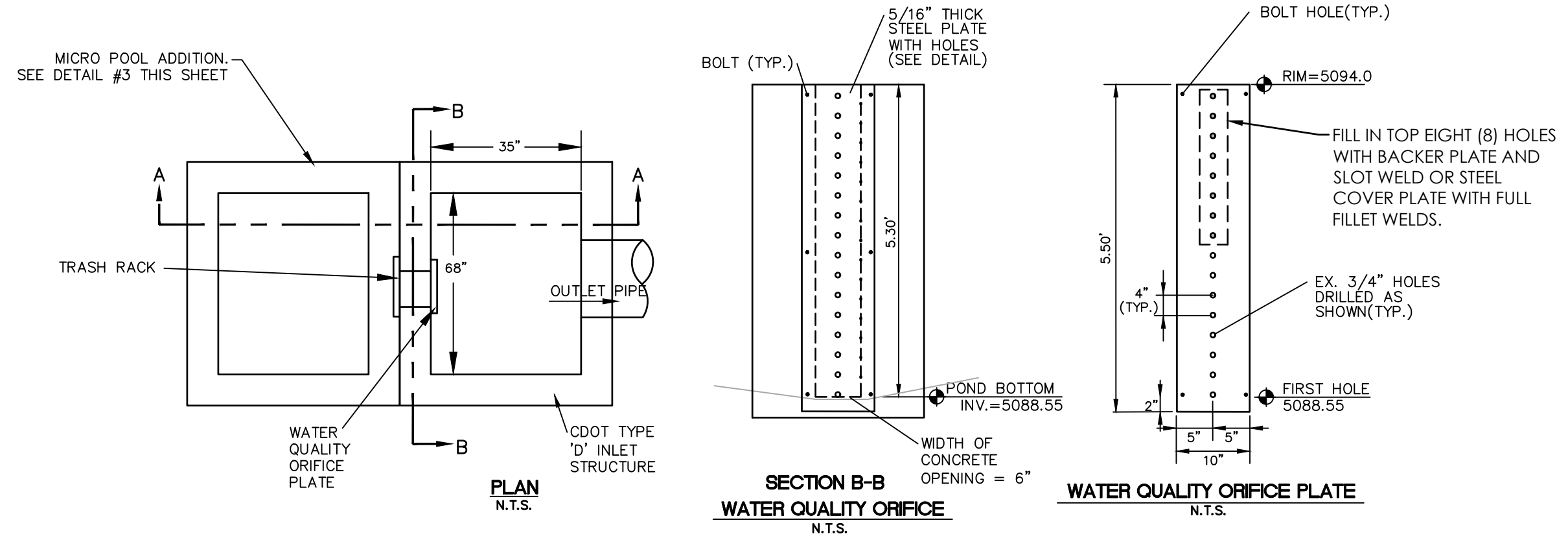


1 EXISTING TRASH RACK DETAIL
SCALE: N.T.S.



3 MICROPOOL DETAIL
SCALE: N.T.S.

WATER QUALITY POND SUMMARY				
STAGE	VOLUME REQUIRED (CF)	VOLUME PROVIDED (CF)	WATER SURFACE ELEVATION (FT)	RELEASE RATE (CFS)
WQCV	23,392	36,475	5094.0	N/A
EURV	N/A	N/A	N/A	N/A
100-YR	N/A	N/A	N/A	N/A



2 EXISTING POND OUTLET DETAIL
SCALE: N.T.S.

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INSPECTION AND MAINTENANCE REQUIREMENTS AT SPECIFIC EDB FEATURES

	SEDIMENT REMOVAL	MOWING/ WEED CONTROL	TRASH/DEBRIS REMOVAL	EROSION	OVERGROWN VEGETATION REMOVAL	STANDING WATER (MOSQUITO/ALGAE CONTROL)	STRUCTURE REPAIR
INFLOW POINTS	X		X				X
FOREBAY	X		X				X
LOW-FLOW	X		X	X	X		X
BOTTOM STAGE	X	X	X	X	X	X	
MICROPOOL	X		X		X	X	X
OUTLET WORKS	X		X				X
SPILLWAY			X	X	X		X
UPPER STAGE			X	X			
EMBANKMENT		X		X	X		

SUMMARY OF ROUTINE MAINTENANCE ACTIVITIES FOR AN EDB

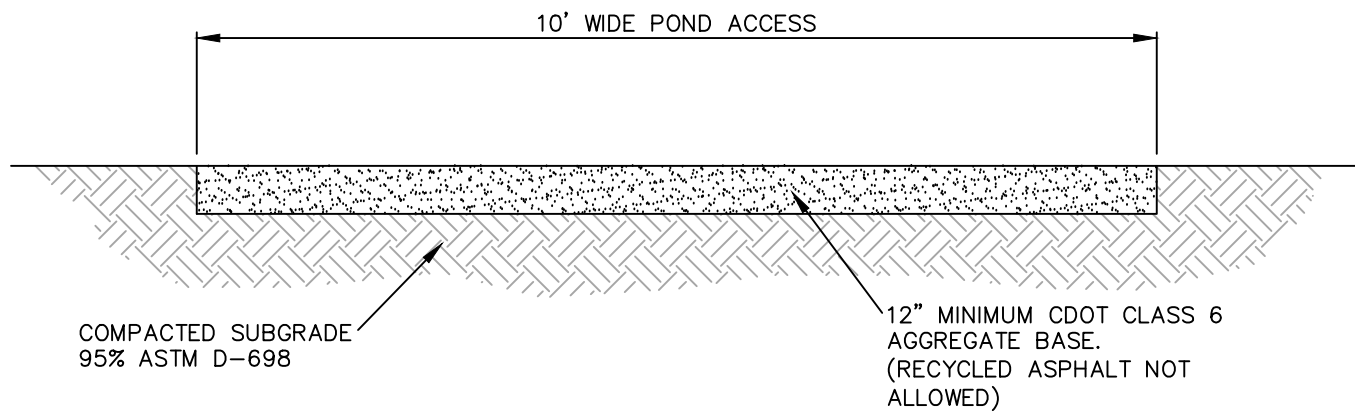
MAINTENANCE ACTIVITY	MINIMUM FREQUENCY	INDICATION ACTION IS NEEDED	MAINTENANCE ACTION
MOWING	TWICE ANNUALLY	EXCESSIVE GRASS HEIGHT/AESTHETICS	MOW GRASS TO A HEIGHT OF 4" TO 6"
TRASH/DEBRIS REMOVAL	TWICE ANNUALLY	TRASH AND DEBRIS IN POND	REMOVE AND DISPOSE OF TRASH AND DEBRIS
OUTLET WORKS CLEANING	AS NEEDED; AFTER SIGNIFICANT RAIN EVENTS; TWICE ANNUALLY	CLOGGED OUTLET STRUCTURE; PONDING WATER	REMOVE AND DISPOSE OF DEBRIS/TRASH/SEDIMENT TO ALLOW OUTLET STRUCTURE TO FUNCTION PROPERLY
WEED CONTROL	MINIMUM TWICE ANNUALLY	NOXIOUS WEEDS; UNWANTED VEGETATION	TREAT WITH HERBICIDE OR HAND PULL; CONSULT WITH COUNTY WEED SPECIALIST
MOSQUITO TREATMENT	AS NEEDED	STANDING WATER; MOSQUITO HABITAT	TREAT WITH EPA APPROVED CHEMICALS
ALGAE TREATMENT	AS NEEDED	STANDING WATER; ALGAE GROWTH/GREEN COLOR	TREAT WITH EPA APPROVED CHEMICALS

SUMMARY OF MINOR ROUTINE MAINTENANCE ACTIVITIES FOR AN EDB
(THESE ACTIVITIES REQUIRE PRIOR CONSULTATION WITH THE TOWN)

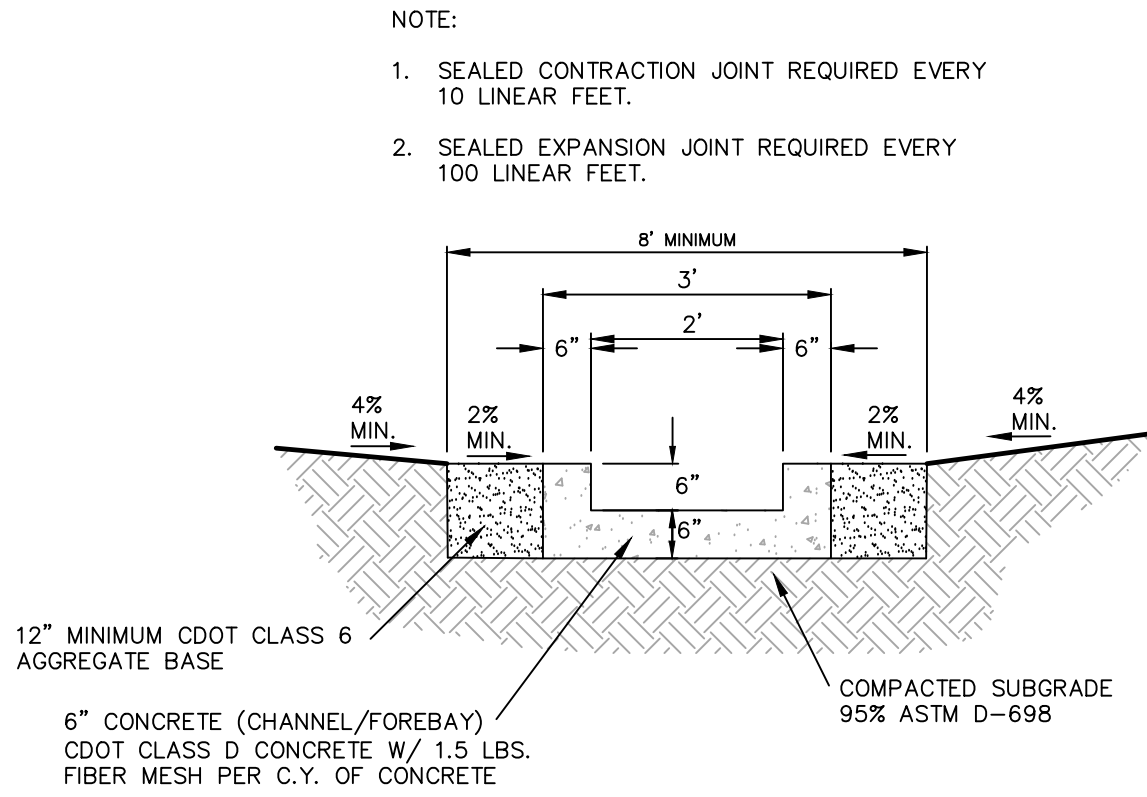
MAINTENANCE ACTIVITY	MINIMUM FREQUENCY	INDICATION ACTION IS NEEDED	MAINTENANCE ACTION
SEDIMENT REMOVAL	AS NEEDED; TYPICALLY EVERY 1 TO 2 YEARS	MUST REMOVE SEDIMENT WHEN IT REACHES 3/4 OF FOREBAY/MICROPOOL VOLUME	REMOVE AND DISPOSE OF SEDIMENT
EROSION REPAIR	AS NEEDED; BASED UPON INSPECTION	RILL/GULLY EROSION ON SIDE SLOPES; TRICKLE CHANNEL, AND OTHER AREAS	REPAIR ERODED AREAS AND REVEGETATE; ADDRESS SOURCE OF EROSION.
VEGETATION REMOVAL/TREE THINNING	AS NEEDED; BASED UPON INSPECTION	LARGE TREES/WOOD VEGETATION IN LOWER CHAMBER OF POND	REMOVE VEGETATION; RESTORE GRADE AND SURFACE
DRAIN CLEANING/JET VAC	AS NEEDED; BASED UPON INSPECTION	SEDIMENT BUILD-UP/NON DRAINING SYSTEM	CLEAN DRAINS; JET VAC IF NEEDED

SUMMARY OF MAJOR ROUTINE MAINTENANCE ACTIVITIES FOR AN EDB
(THESE ACTIVITIES REQUIRE PRIOR CONSULTATION WITH THE TOWN)

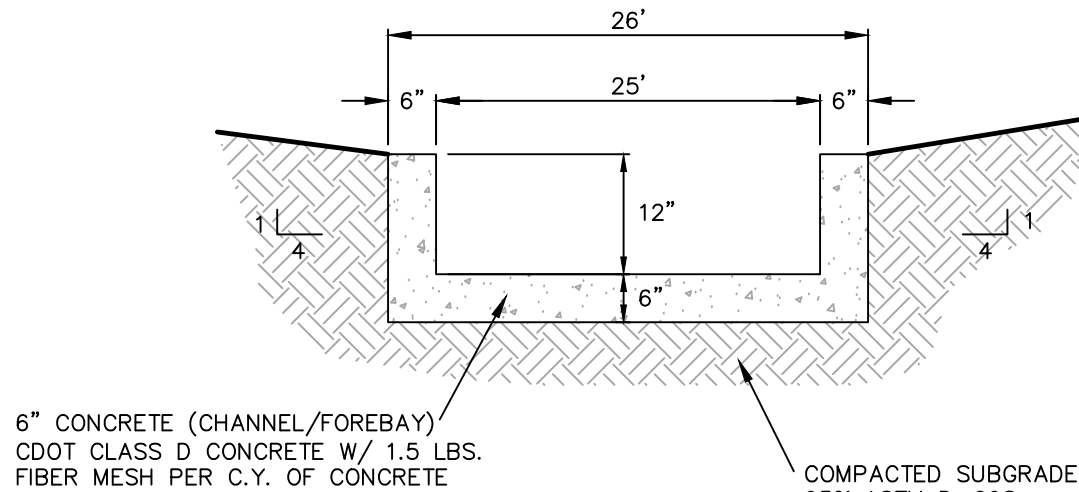
MAINTENANCE ACTIVITY	MINIMUM FREQUENCY	INDICATION ACTION IS NEEDED	MAINTENANCE ACTION
MAJOR SEDIMENT REMOVAL	AS NEEDED; BASED UPON INSPECTION	LARGE QUANTITIES OF SEDIMENT; REDUCED POND CAPACITY	REMOVE AND DISPOSE OF SEDIMENT; REPAIR VEGETATION AS NEEDED
MAJOR EROSION REPAIR	AS NEEDED; BASED UPON INSPECTION	SEVERE EROSION INCLUDING GULLING, EXCESSIVE SOIL DISPLACEMENT AREAS OF SETTLEMENT, HOLES	REPAIR EROSION; FIND CAUSE OF PROBLEM AND ADDRESS TO AVOID FUTURE EROSION
STRUCTURAL REPAIR	AS NEEDED; BASED UPON INSPECTION	DETERIORATION AND/OR DAMAGE TO STRUCTURAL COMPONENTS (I.E. BROKEN CONCRETE, DAMAGED PIPES, DAMAGED OUTLET WORKS)	STRUCTURAL REPAIR TO RESTORE THE STRUCTURE TO ITS ORIGINAL DESIGN



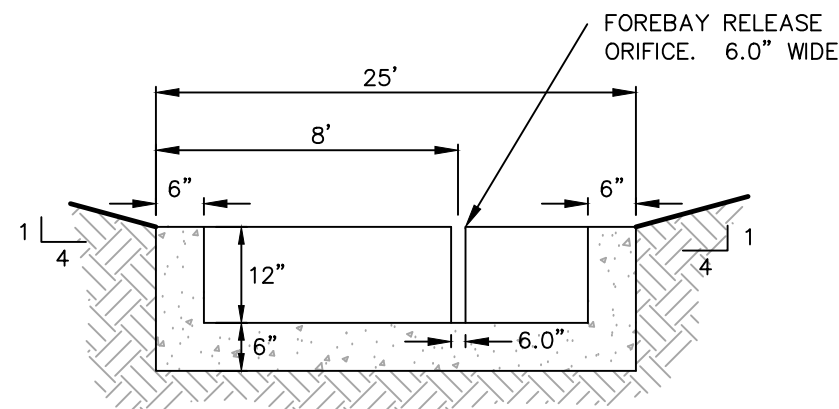
F ACCESS ROAD SECTION
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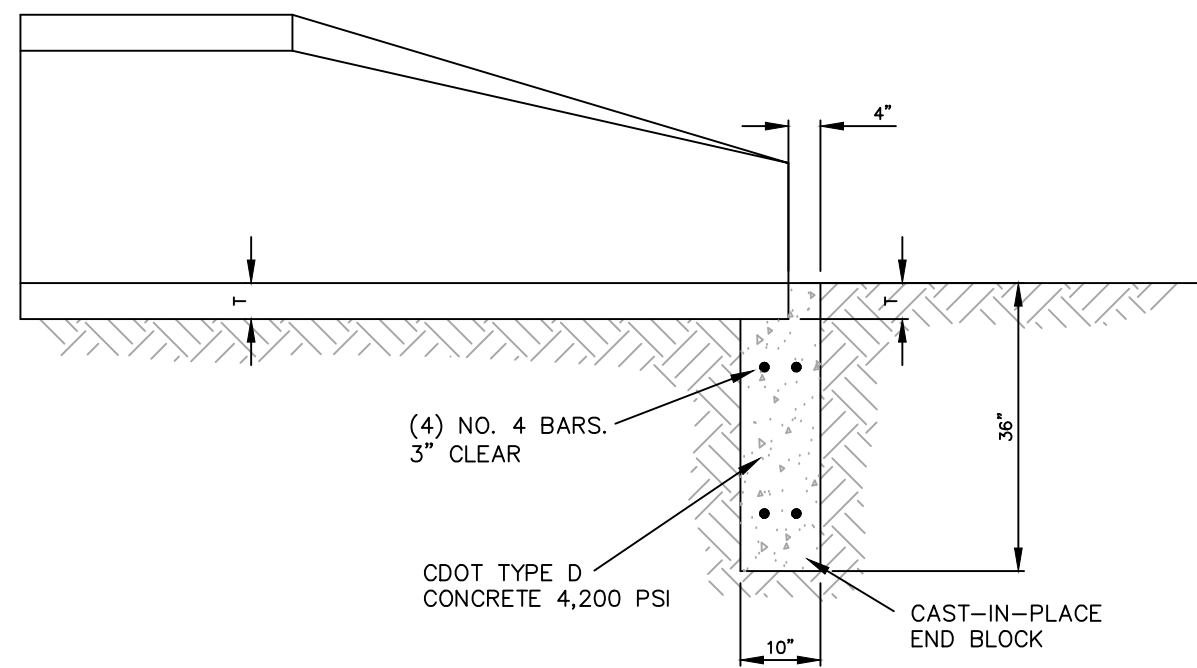
A CHANNEL SECTION FLOW CHANNEL
SCALE: N.T.S.



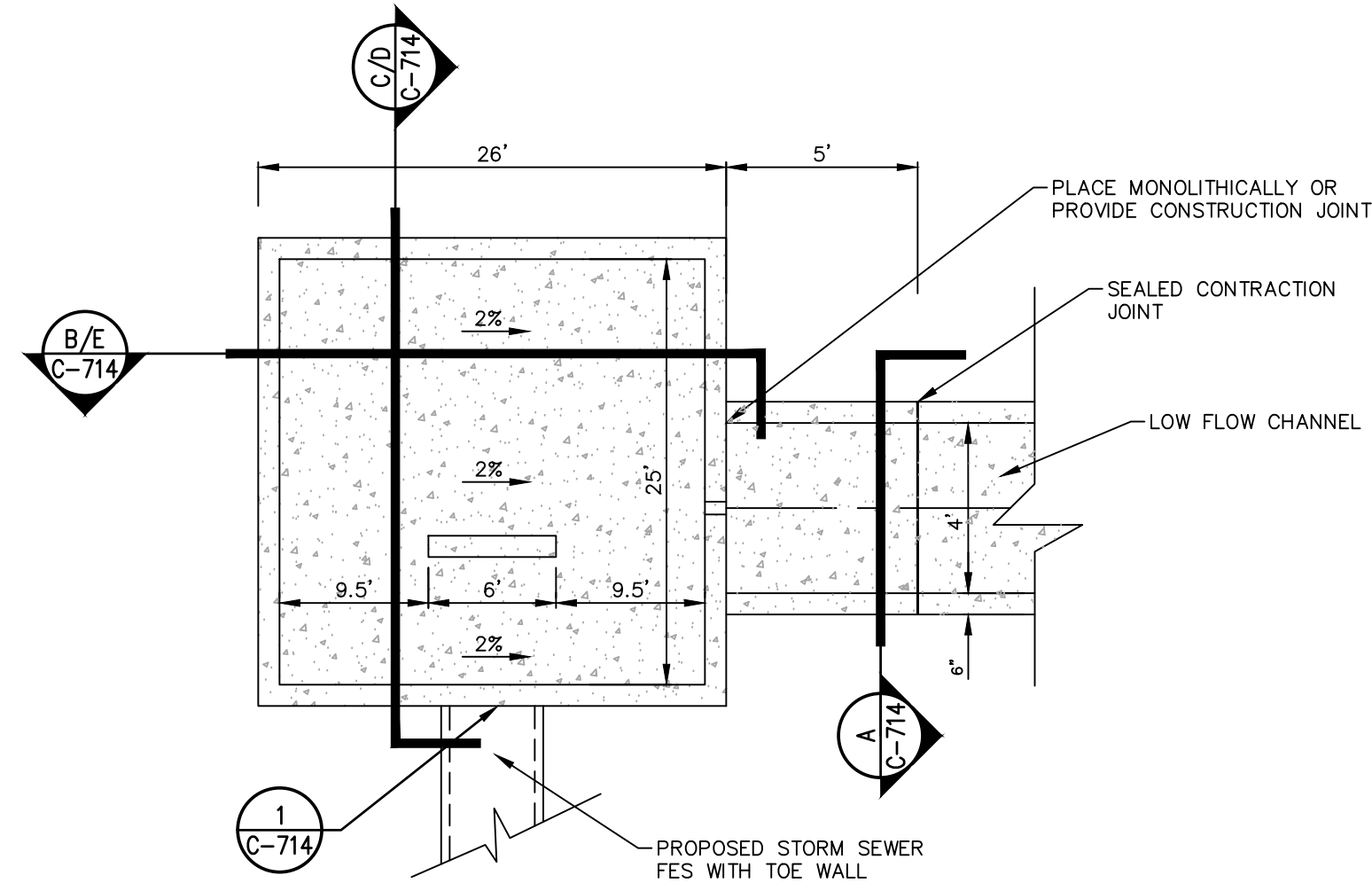
B FOREBAY SECTION
SCALE: N.T.S.



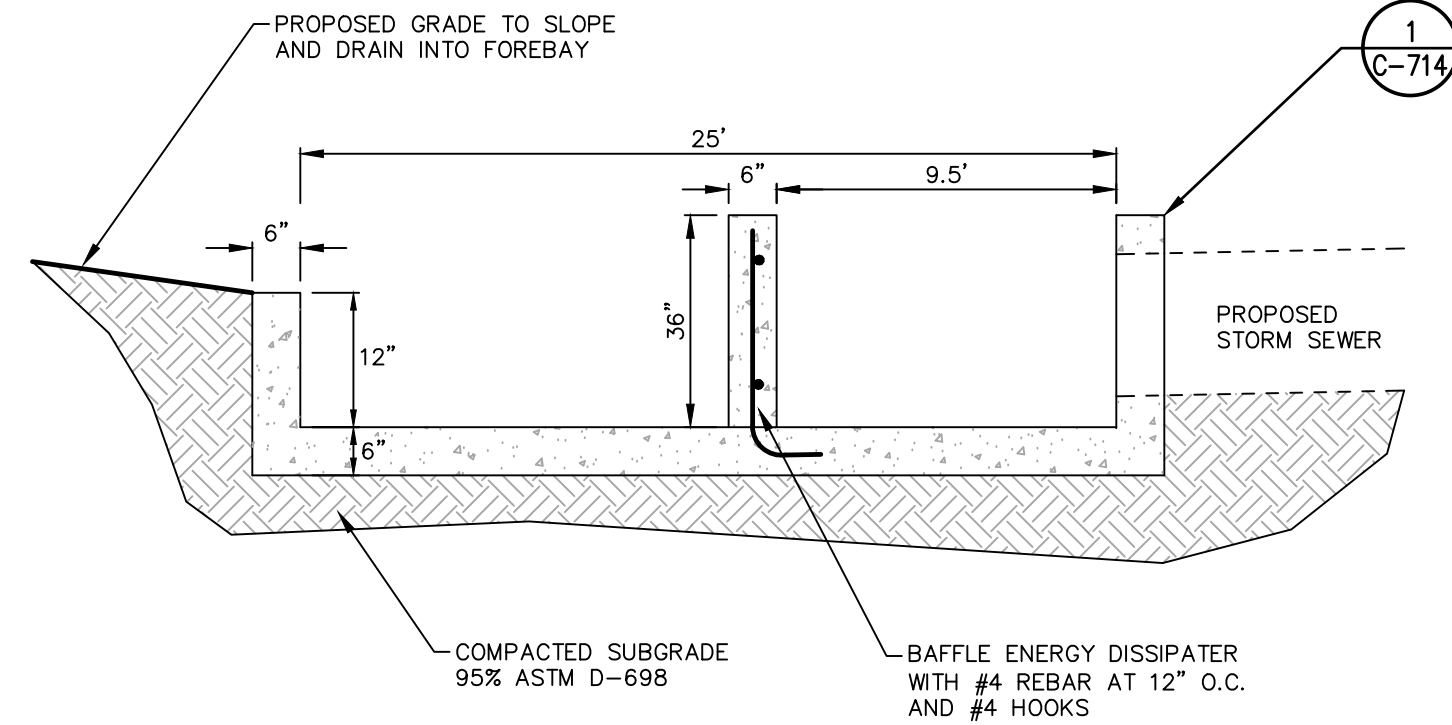
C FOREBAY ORIFICE SECTION
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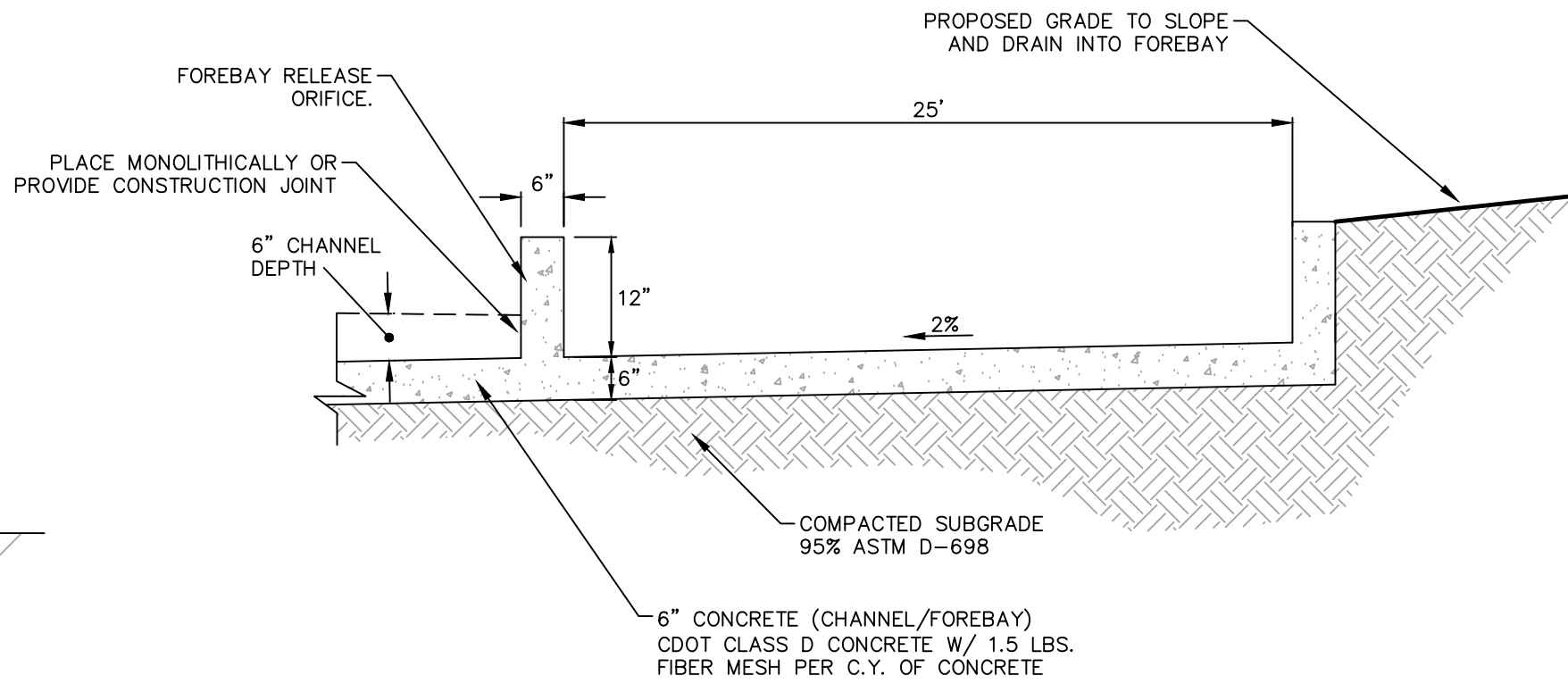
1 F.E.S. TOE WALL DETAIL
SCALE: N.T.S.



2 FOREBAY TRANSITION DETAIL
SCALE: N.T.S.



D FOREBAY SECTION
SCALE: N.T.S.



E FOREBAY SECTION
SCALE: N.T.S.

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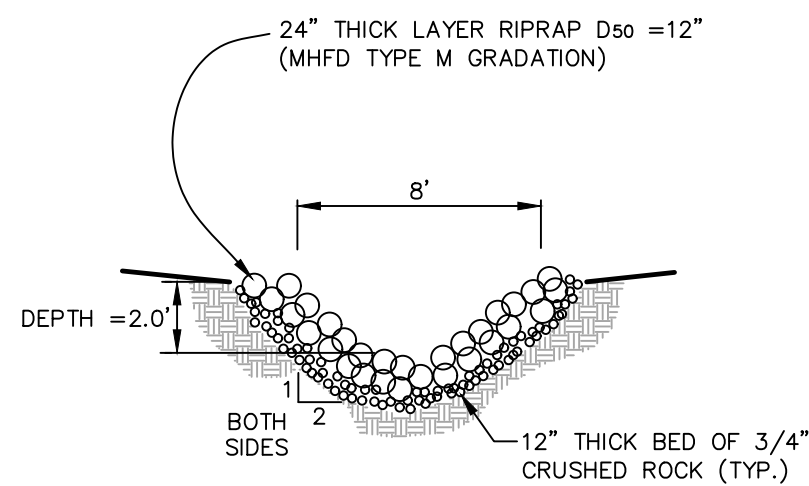
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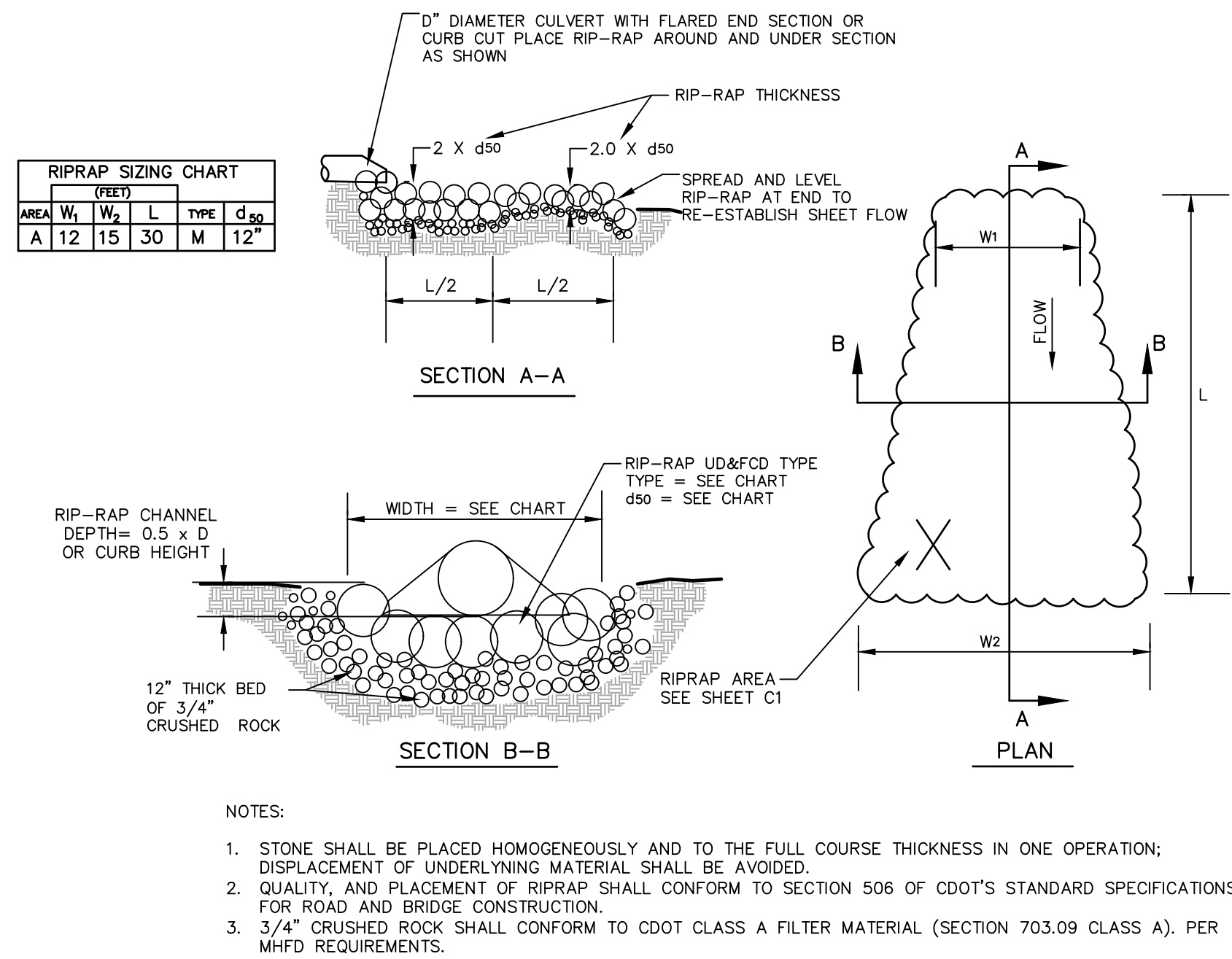
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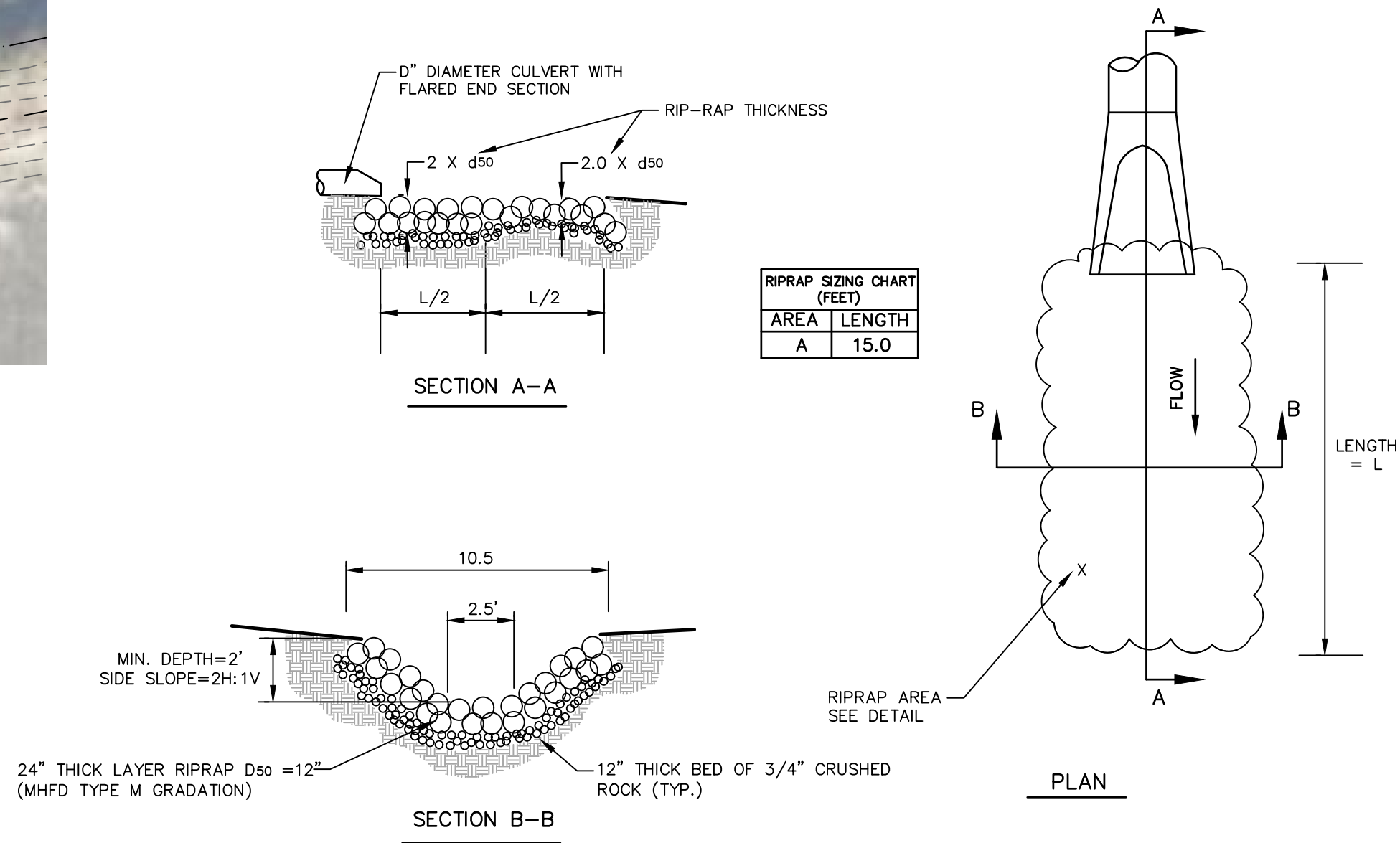
1 **OUTFALL DETAIL**
SCALE: 1" = 20'



2 **RIPRAP CHANNEL DETAIL**
SCALE: N.T.S.



3 **RIPRAP RUNDOWN DETAIL**
SCALE: N.T.S.



4 **RIPRAP RUNDOWN DETAIL**
SCALE: N.T.S.